

**STAFF REPORT FOR THE HISTORIC RESOURCES COMMISSION MEETING OF
MAY 8, 2014**

FILE NO: HRC-14-014

AGENDA ITEM: F-1

STAFF AUTHOR: Susan Dorr Pansky, Planning Manager

REQUEST: Approval of a request from the Nevada State Railroad Museum (Property Owner: State of Nevada, Division of State Lands) to install a new freestanding LED monument sign, landscaping and other minor improvements on property zoned Public (P), located at 2180 South Carson Street, APNs 003-302-02 and 003-302-04.

APPLICANT: Nevada State Railroad Museum

OWNER: State of Nevada, Division of State Lands

LOCATION: 2180 South Carson Street

APNs: 003-302-02 and 003-302-04

RECOMMENDED MOTION: "I move to approve HRC-14-014, a request from the Nevada State Railroad Museum (Property Owner: State of Nevada, Division of State Lands) to install a new freestanding LED monument sign, landscaping and other minor improvements on property zoned Public, located at 2180 South Carson Street, APNs 003-302-02 and 003-302-04, based on the Standards and Guidelines for Rehabilitation, Carson City Historic District Guidelines and with Historic Resources Commission Policies."



RECOMMENDED CONDITIONS OF APPROVAL:

1. All development shall be substantially in accordance with the attached site development plan.
2. All on and off-site improvements shall conform to City standards and requirements.
3. The use for which this permit is approved shall commence within 12 months of the date of final approval. An extension of time must be requested in writing to the Planning Division 30 days prior to the one year expiration date. Should this request not be initiated within one year and no extension granted, the approval shall become null and void.
4. The applicant must sign and return the Notice of Decision within 10 days of receipt of notification. If the Notice of Decision is not signed and returned within 10 days, then the item will be rescheduled for the next Historic Resources Commission meeting for further consideration.
5. The monument sign structure shall not exceed 11 feet high by 13 feet eight inches wide by two feet six inches deep as shown on the elevations provided in the application. The height shall be measured from the adjacent finish grant to the highest point of the sign's structure. The combined square footage of the LED display sign and the fixed signage lettering shall not exceed 43 square feet (19 square feet – fixed lettering, 24 square feet – LED display face).
6. All new fencing and gates shall be two-rail design made of wood with horizontal rails as shown on the site plan and to match the existing fencing.
7. Approval is based on upon the project complying with the Standards and Guidelines for Rehabilitation, Carson City Historic District Guidelines, the Historic Resources Commission Policies; and that the plans as submitted are in general conformance with the Secretary of the Interior's Standards, as determined by the State Historic Preservation Office.
8. Approval of a Special Use Permit through the Carson City Planning Commission is required for the freestanding LED monument sign. Specific requirements for the LED sign operation consistent with other LED signs in Carson City (i.e. hold and transition times, brightness, animation, etc.) will be incorporated into the Special Use Permit conditions.

LEGAL REQUIREMENTS: CCMC 18.06.015 (Procedure for Proposed Project)

MASTER PLAN DESIGNATION: Public/Quasi-Public

ZONING: Public

PREVIOUS REVIEWS:

None

HISTORY:

The subject property is known as the Nevada State Railroad Museum (Museum) and is comprised of four separate parcels totaling approximately 14.3 acres. Two of those parcels, which are located on the eastern portion of the Museum site, contain all of the improvements proposed in this application. Although all of the proposed improvements are located on two of the four parcels, for purposes of this application staff will refer to the Museum property in its entirety.

The Nevada State Railroad Museum was constructed in the 1980s to preserve Nevada's railroad heritage and share it with the general public. The Museum's collection is comprised of 65 locomotives and cars, including the McKeen Motor Car #22, which was designated as a National Landmark in 2013. The Museum property is also the site of two historic structures, both of which are on the parcels subject to improvements. These structures include the Wabuska Railroad Station and the Louis Nelson House.

The Wabuska Railroad Station was built in Wabuska, Nevada in 1906 by the Southern Pacific Railroad as a freight and passenger facility to serve the Mason Valley region of Lyon County, where it remained in active service until 1979. It was donated in 1982 and moved to the Museum site on July 22, 1983. It is currently used as the train station associated with tourist train rides at the Museum. The Wabuska Railroad Station is listed on the National Register of Historic Places.

The Louis Nelson House was constructed in 1920s according to the application (the Assessor's database lists the construction year as 1935). This building is the former home of Louis Nelson and was originally located at 211 West 8th Street in Carson City. It was donated and moved to the Museum grounds in 1985.

DISCUSSION:

The applicant is proposing to remove the existing freestanding Museum sign and replace it with a new freestanding monument sign including LED display at approximately the same location as the current sign stands today. The proposed sign structure is 13 feet eight inches wide, 11 feet high and two feet six inches deep. The actual signage area is 43 square feet (19 square feet for fixed lettering, 24 square feet for LED display as shown on the elevations provided). The proposed sign structure has architectural features similar to the Wabuska Railroad Station, with eaves and a gable that imitate a roof.

By comparison, the existing sign is approximately 10 feet wide by 12 feet high including the height of the supporting base poles and ornamental railroad crossing feature. The actual sign face is four feet high by 10 feet wide, or 40 square feet. Staff notes that the massing of the proposed freestanding monument sign is substantially larger than the original sign, however, understands from the application that the larger overall structure of the sign is necessary to be adequately seen by vehicles passing the Museum.

The entire sign body is proposed to be constructed of aluminum as indicated on the elevations provided, and will be painted brown and yellow like both the Wabuska Railroad Station and the Louis Nelson House. The sign will contain a three foot high by eight foot wide LED display screen to allow for changing digital messages. It will also contain an exterior light at the tip of the gable that will face downward and externally illuminate the fixed "Nevada State Railroad Museum" lettering on the sign.

Additionally, the applicant proposes to perform landscaping and site improvements including the following:

- Remove decorative boulders in front of Museum and relocate to rear of Museum;
- Remove one elm tree at north end of drainage ditch;
- Remove willows at north end of drainage ditch;
- Have certified arborist prune existing cottonwood tree;
- Fill voids where water accumulates east of the railroad tracks;
- Add two inch layer of one and a half inch fractured brown/block river rock and thoroughly compact;
- Install four foot border of three inch by 10 inch fractured river rock along edge of the level area adjacent to the west side of the drainage ditch;
- Spread pre-emergent granules in all rocked areas to prevent weed seed germination;
- Construct new fencing to improve site entry definition. Fencing will be made of wood with two horizontal rails to match existing fencing;
- Construct new fencing and gate to limit access to museum staff and railroad engineers. Fencing will be made of wood with two horizontal rails to match existing fencing;
- Add one and a half inch layer of "Nevada Gold" fractured rock, or equivalent, to the landscape border on the south side of the Access Road between the two-rail fencing and the Access Road;
- Remove soil along the edge of the roadway so that the top of the rock mulch is flush with the top of the roadway;
- Remove and dispose of dirt and cinders to a depth of three inches;
- Add two and a half inch deep layer of one and a half inch "Nevada Gold" fractured rock;
- Construct a moveable platform to allow for tourists/photographers to safely cross the rails to photograph the engine on special occasions.

Because the proposed project will be receiving federal funds, a review from the State Historic Preservation Office (SHPO) was required. Modifications were made to a previous version of the freestanding monument sign proposal based on comments received from SHPO, and the sign was subsequently deemed acceptable as outlined in SHPO's letter dated March 12, 2014. This letter is included as a part of the application. The landscaping and fencing improvements had been previously reviewed and accepted as also outlined in the letter.

When considering exterior modifications on historic properties, the following Carson City Historic District Design Guidelines are applicable to the proposed project:

Freestanding Monument Sign

5.23 – Signs. Signs in the Historic District are governed by both Division 5 (Historic District) and Division 4 (Signs) of the Development Standards. Signage in the Historic District is generally concerned with the conversion of original residential buildings to light office use or with new buildings which have been constructed as commercial buildings in a style compatible with the basic residential nature of the district.

All signs must have an appearance, color, size, texture and design which conforms to the sign codes and to the historic character of the district. Signs should also closely match

stylistically with the architectural style of the building. Additionally, the location and/or method of attachment of the sign will be considered. The HRC will review all sign applications within the context of the building and the location in the district.

Carson City Municipal Code, Section 18.06.135 (Historic District, Signs) also outlines specific regulations which are to be followed within the Historic District.

- All signs on historic places or in Historic Districts must conform with Division 4 of the Development Standards, additionally, must have an appearance, color, size, position, method of attachment, texture of materials and design in keeping with the character of the place or district.
- Signage shall be further limited as follows:
 - No off-site signs are permitted;
 - Business signs shall be limited to a single sign for each street frontage;
 - No sign may extend above the top of the nearest façade, eaves or firewall of building or structure;
 - No sign which flashes, blinks, revolves or is otherwise in motion or which is connected to audio equipment, shall be permitted; and no visible bulbs, neon tubing, luminous paints or backlight shall be permitted as part of any sign.
 - Buildings and signs may be illuminated by remote light sources, provided that these light sources are shielded to protect adjacent properties.

The standards outlined above are typical for properties that are located in the Historic District and on recognized historic properties outside of the boundaries of the district, either by a National Register listing or historic place designation by the HRC. The overall subject property itself is not a historic property, however, it has the designation because of the three historic elements that currently reside on the site, two of which are in the immediate vicinity of the proposed improvements.

The subject property is also located in a Public zoning district; therefore, specific development standards for the proposed freestanding monument sign will be set through the approval of a Special Use Permit. A Special Use Permit application has been submitted for the sign and will be heard by the Planning Commission on June 25, 2014.

Staff notes that although this property is zoned Public and has a historical designation, it is in the general vicinity of both Retail Commercial and General Commercial zoning districts and is along a major thoroughfare. LED signs are allowed in the nearby zoning districts and freestanding signage can be a maximum of 300 square feet, depending upon the size of the main building and the amount of signage allocated to any building signage versus freestanding signage. The proposed freestanding sign is well below this maximum at 43 square feet including both fixed signage and LED display. It is also important to note that the freestanding sign is essentially the only sign that advertises the Museum on adjacent streets, rather than a combination of freestanding signage and building signage typical of most commercial establishments.

From a Historic District perspective, staff is in agreement with SHPO that the overall design of the freestanding monument sign itself is in keeping with the Secretary of Interior's Design Standards and Guidelines. While it imitates historic buildings on the property, it does not exactly duplicate them nor does it attach to the structures in any way, which could potentially degrade their historical integrity.

As for the LED digital display component of the freestanding monument sign, staff would not typically be in favor of this technology within the Historic District because it is not historically appropriate among buildings and landscapes from the late 19th to early 20th century. That being said, since the subject property is located outside of the main area that is referred to and regulated as the Historic District, but is included in its requirements because of historic elements on a small portion of the overall site, staff is able to support the LED display. This display is for advertising purposes of the Museum complex as a whole. It is relatively small in size with relation to the total complex and is located on Carson Street, which is a busy major arterial in the City. Staff does not feel that placing an LED sign in this location would be detrimental to the historic structures on the Museum site.

Fencing and Gates

5.24.1 – Guidelines for New Fences. The appropriate design for a fence will be determined by its intended function and its location. No fence shall be constructed which adversely affects the primary view(s) of any building. A fence design should enhance the overall visual presentation of a building. A fence should also contribute to the character and defining features of any building in a positive manner. (Standard Number: 9)

The fencing and gates proposed by the applicant are a continuation of the existing fencing on the site and are consistent with post and rail fencing common with historic ranch properties. The fencing will not adversely affect the primary views of the historic structures on the property or any other buildings on the property and will enhance the visual presentation of the overall Museum complex.

Landscaping

5.25.2 – Guidelines for New Construction (Landscaping). New construction in the district should include landscape elements which reflect the scale, rhythm, texture, material, color, style and visual qualities of the historic landscape present (Standard Number: 9, 10).

The landscaping proposed by the applicant is consistent with existing landscaping and compatible with the historic structures that are located on the site. Staff would like to note that the proposed removal of the elm tree at the north end of the drainage ditch will be addressed with the Special Use Permit.

Portable Platform

Because the proposed portable platform is a moveable, temporary object that will only be placed on the tracks during special events to allow safe passage across the tracks, staff does not consider this to be a structure that warrants review by the Historic Resources Commission.

PUBLIC COMMENTS:

Public notices were mailed to the adjacent property owners of the subject parcel in accordance with the provisions of NRS and CCMC 18.02.045 on April 25, 2014. No comments have been received in favor or opposed to the proposed project as of the date this staff report was completed. Any comments that are received after this report's completion will be submitted prior to or at the Historic Resources Commission meeting,

depending on their submittal date to the Planning Division.

BUILDING DIVISION COMMENTS:

The Carson City Building Division has no jurisdiction over state-owned structures.

ENGINEERING DIVISION COMMENTS:

Development Engineering has no preference or objection to the request.

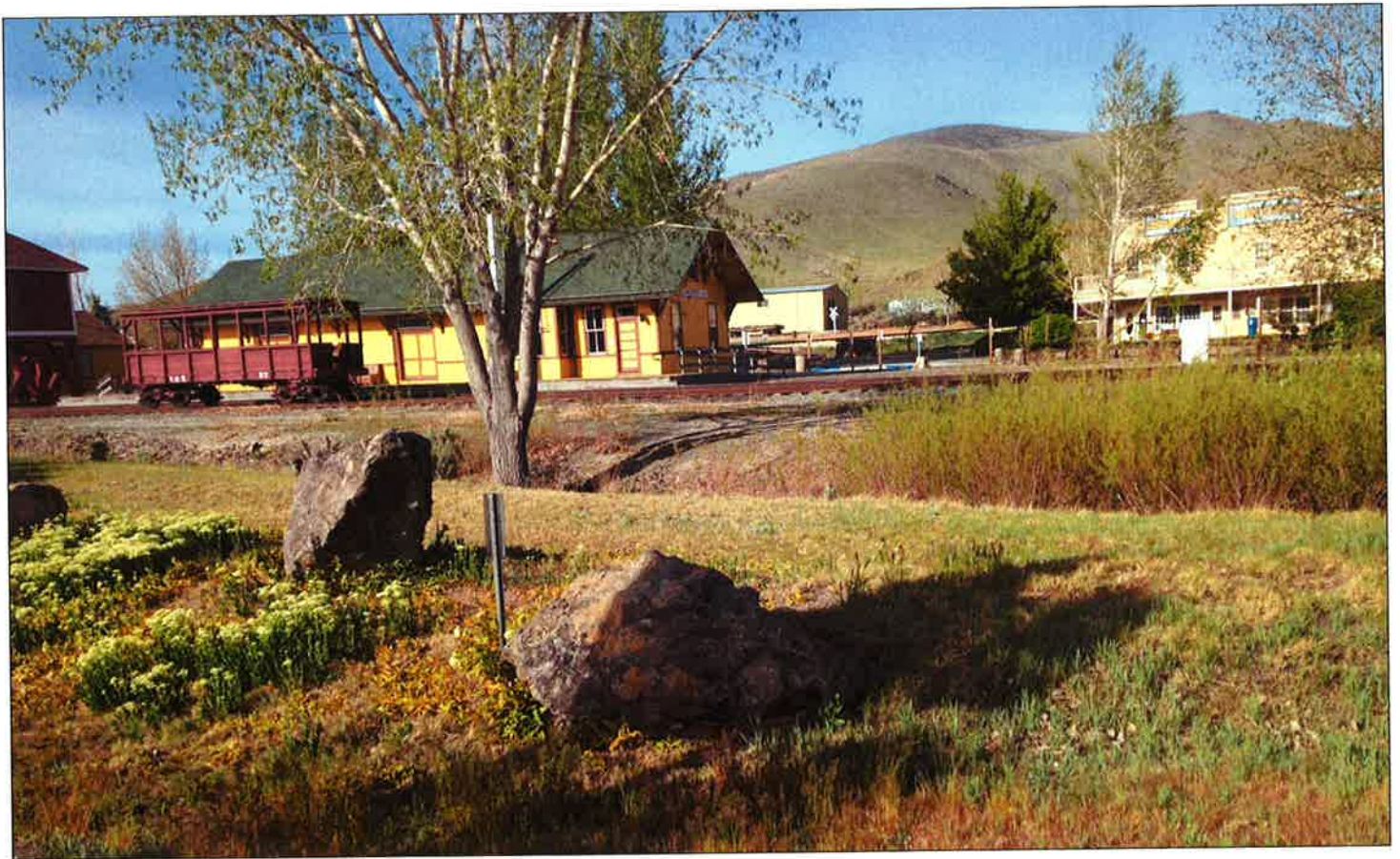
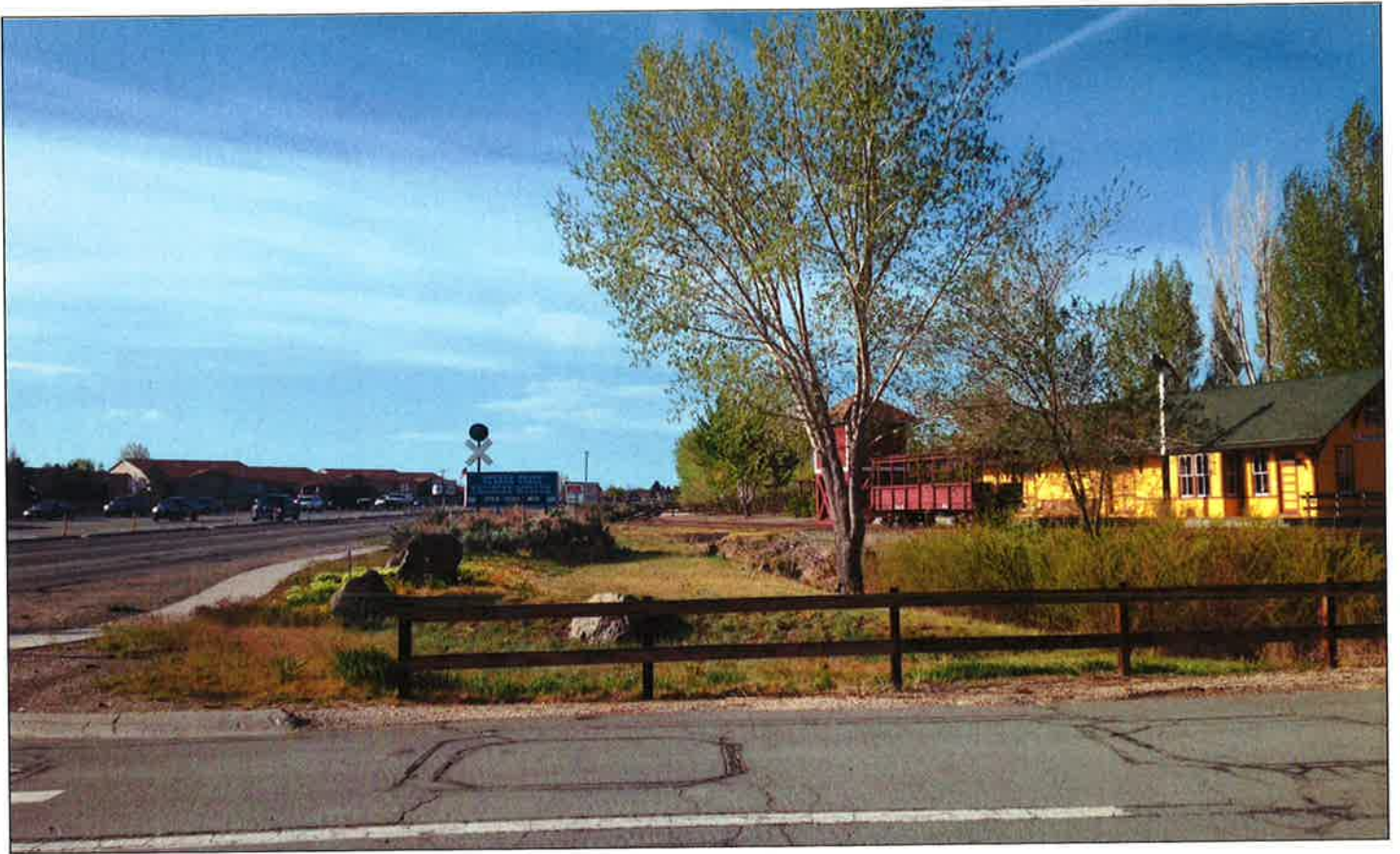
Based upon the project complying with the Carson City Historic District Guidelines, the Historic Resources Commission Policies, and that the plans as submitted are in general conformance, it is recommended that the Historic Resources Commission approve the application submitted for HRC-14-014 subject to the recommended conditions of approval outlined in this staff report.

Attachments:

Site Photos

National Register of Historic Places Listing – Wabuska Railroad Station
Application (HRC-14-014)





UNITED STATES DEPARTMENT OF THE INTERIOR
NATIONAL PARK SERVICENATIONAL REGISTER OF HISTORIC PLACES
INVENTORY -- NOMINATION FORM

FOR NPS USE ONLY

RECEIVED JUL 17 1984
DATE ENTERED AUG 30 1984SEE INSTRUCTIONS IN HOW TO COMPLETE NATIONAL REGISTER FORMS
TYPE ALL ENTRIES -- COMPLETE APPLICABLE SECTIONS**1 NAME**

HISTORIC

Wabuska Railroad Station

AND/OR COMMON

2 LOCATION

STREET & NUMBER

US Highway 395/S. Carson Street

n/a NOT FOR PUBLICATION

CITY, TOWN

Carson City

VICINITY OF

2

CONGRESSIONAL DISTRICT

STATE

Nevada

CODE
32

COUNTY

Carson City

CODE
025**3 CLASSIFICATION**

CATEGORY

☐ DISTRICT
☒ BUILDING(S)
☐ STRUCTURE
☐ SITE
☐ OBJECT

OWNERSHIP

☒ PUBLIC
☐ PRIVATE
☐ BOTH

PUBLIC ACQUISITION

☒ IN PROCESS
☐ BEING CONSIDERED

STATUS

☐ OCCUPIED
☐ UNOCCUPIED
☒ WORK IN PROGRESS
ACCESSIBLE
☒ YES: RESTRICTED
☐ YES: UNRESTRICTED
☐ NO

PRESENT USE

☐ AGRICULTURE ☒ MUSEUM when
☐ COMMERCIAL ☐ PARK restored
☐ EDUCATIONAL ☐ PRIVATE RESIDENCE
☐ ENTERTAINMENT ☐ RELIGIOUS
☐ GOVERNMENT ☐ SCIENTIFIC
☐ INDUSTRIAL ☐ TRANSPORTATION
☐ MILITARY ☐ OTHER:

4 OWNER OF PROPERTY

NAME

Nevada State Museum

STREET & NUMBER

600 N. Carson Street (Capitol Complex)

CITY, TOWN

Carson City

VICINITY OF

STATE
Nevada 89710**5 LOCATION OF LEGAL DESCRIPTION**COURTHOUSE,
REGISTRY OF DEEDS, ETC.

Carson City Courthouse

STREET & NUMBER

198 N. Carson Street

CITY, TOWN

Carson City

STATE
Nevada**6 REPRESENTATION IN EXISTING SURVEYS**

TITLE

n/a

DATE

☐ FEDERAL ☐ STATE ☐ COUNTY ☐ LOCALDEPOSITORY FOR
SURVEY RECORDS

CITY, TOWN

STATE

7 DESCRIPTION

CONDITION

—EXCELLENT

—GOOD

XFAIR

—DETERIORATED

—RUINS

—UNEXPOSED

CHECK ONE

XUNALTERED

—ALTERED

CHECK ONE

—ORIGINAL SITE

XMOVED DATE July 22, 1983

DESCRIBE THE PRESENT AND ORIGINAL (IF KNOWN) PHYSICAL APPEARANCE

Summary

The Wabuska Railroad Station was erected in 1906 by the Southern Pacific Railroad as a freight and passenger facility serving the Mason Valley region of Lyon County, Nevada. The single story, wood-frame depot was constructed by company carpenters and remained in active service until 1979. The intact station is representative of early twentieth century Nevada depot architecture, few examples of which survive in the state today.

In 1982 the Southern Pacific Transportation Company donated the vacant depot to the Nevada State Museum with the understanding that the station would become part of the Virginia & Truckee Railroad Museum in Carson City. The alternative to moving the depot to Carson City was the demolition of the building. On July 22, 1983, the station was moved to the museum site where it is undergoing restoration for use as an interpretative center which will house the museum's collection of period railroad furnishings.

Site

The Wabuska Railroad Station was originally located in the SE $\frac{1}{4}$ of the SE $\frac{1}{4}$ of Section 21, Township 15N, Range, 25E, M.D.B.&M., east of US 95 Alternate. The structure was sited south and parallel to the Southern Pacific branch line which ran between Hazen and Mina. The twenty-four by eighty foot building was oriented to the north amid a sagebrush covered plain in the northern portion of Mason Valley. The Wabuska Hotel and Bar (c. 1883) was located to the west of the station. A small, plastic manufacturing complex was situated across the tracks to the northeast. This industrial complex was composed of contemporary metal building constructed c. 1980. Structures in the vicinity of the station included several shacks, a mobile home and the remnants of buildings associated with the modest farming and transportation center of Wabuska.

The Wabuska Railroad Station was moved to the Virginia & Truckee Railroad Museum in Carson City, Nevada, in July of 1983. The museum site encompasses eighteen acres located at the southern end of Carson City and is bounded by South Carson Street/US 395 and South Curry Street. The Wabuska station, which is visible from the highway, is oriented to the north and adjoins the Museum's proposed track bed. The topography and vegetation of the museum site are similar to those of the depot's original site. Other facilities found at the museum are a railroad restoration shop/museum building, a storage yard and a turntable. These facilities are located southwest of the depot and do not affect the immediate depot site.

Exterior Description

The Station is a one story, wood frame building supported by a series of twenty-six inch wood piers. The twenty-four by eighty foot depot terminates in a gable roof sheathed in wood shingles. The roof design includes extended eaves which project approximately four feet from the structure's exterior wall planes. The resulting overhang is supported by brackets located at eight foot intervals. The depot's roof employs a common rafter system incorporating collar ties, purlins and vertical braces.

8 SIGNIFICANCE

PERIOD		AREAS OF SIGNIFICANCE -- CHECK AND JUSTIFY BELOW			
<input type="checkbox"/> PREHISTORIC	<input type="checkbox"/> ARCHEOLOGY-PREHISTORIC	<input type="checkbox"/> COMMUNITY PLANNING	<input type="checkbox"/> LANDSCAPE ARCHITECTURE	<input type="checkbox"/> RELIGION	
<input type="checkbox"/> 1400-1499	<input type="checkbox"/> ARCHEOLOGY-HISTORIC	<input type="checkbox"/> CONSERVATION	<input type="checkbox"/> LAW	<input type="checkbox"/> SCIENCE	
<input type="checkbox"/> 1500-1599	<input type="checkbox"/> AGRICULTURE	<input type="checkbox"/> ECONOMICS	<input type="checkbox"/> LITERATURE	<input type="checkbox"/> SCULPTURE	
<input type="checkbox"/> 1600-1699	<input checked="" type="checkbox"/> ARCHITECTURE	<input type="checkbox"/> EDUCATION	<input type="checkbox"/> MILITARY	<input type="checkbox"/> SOCIAL/HUMANITARIAN	
<input type="checkbox"/> 1700-1799	<input type="checkbox"/> ART	<input type="checkbox"/> ENGINEERING	<input type="checkbox"/> MUSIC	<input type="checkbox"/> THEATER	
<input type="checkbox"/> 1800-1899	<input checked="" type="checkbox"/> COMMERCE	<input type="checkbox"/> EXPLORATION/SETTLEMENT	<input type="checkbox"/> PHILOSOPHY	<input checked="" type="checkbox"/> TRANSPORTATION	
<input checked="" type="checkbox"/> 1900-	<input type="checkbox"/> COMMUNICATIONS	<input type="checkbox"/> INDUSTRY	<input type="checkbox"/> POLITICS/GOVERNMENT	<input type="checkbox"/> OTHER (SPECIFY)	
		<input type="checkbox"/> INVENTION			

SPECIFIC DATES 1906-1979

BUILDER/ARCHITECT Southern Pacific Company

STATEMENT OF SIGNIFICANCE

Summary

The Wabuska Railroad Station is significant as an intact example of early twentieth century Nevada railroad architecture. The depot is among the last surviving railroad stations associated with the Hazen to Mina branch of the Southern Pacific Railroad. Of the seven stations constructed to serve the line in the early twentieth century, only the Wabuska and Mina Freight stations survive.

The Wabuska Railroad Station is also historically significant as the major shipping point for Mason Valley. During the early 1900's, the Wabuska region served as the principal supplier of agricultural products for the mining camps of Tonopah and Goldfield. Wabuska also served as the transfer point for the Nevada Copper Belt Railroad, a major carrier of copper ore which operated between 1910 and 1947.

Historical Associations

The Southern Pacific Railroad Station was the second depot constructed in Wabuska. The first facility was built by the Carson and Colorado Railroad Company; a narrow gauge railroad which operated between Mound House, Nevada and Keeler, California. A modest board and batten Carson and Colorado Station was erected four months after service was established through Wabuska in February, 1881. Although settlement in the Wabuska area can be traced to 1874, development was sporadic until the Carson and Colorado Railroad provided easy access to the region.

Wabuska was an agricultural community until the turn of the century. Initial development was confined to the depot and a hotel complex incorporating a hotel, general store, post office, saloon and Wells, Fargo and Co express office.

In 1900 the Southern Pacific Railroad purchased the Carson and Colorado Railroad Company. Increased freight traffic between Wabuska and neighboring mining camps coupled with the discovery of copper ore in the Mason Valley prompted The Southern Pacific Railroad to improve their Wabuska facilities in early 1900. On August 2, 1906, a crew of eight Southern Pacific carpenters began work on a new Wabuska station. The "Lyon County Times" documented the progress of the depot, noting on August 18th that the structure was "of modern design" and nearly complete by September 8th. The new depot was in use by October 6th of 1906.

9 MAJOR BIBLIOGRAPHICAL REFERENCES

Myrick, David F., "Railroads of Nevada", 1962, Berkeley, CA., Howell-North Books.
"Lyon County Times", Yerington, NV 1904-1906 issues.
Virginia and Truckee RR. Collection of papers, Univ. of Nevada-Reno, Special Coll.
Nevada State Museum Library, Carson City, NV
Bancroft Library, Univ. of California, Berkeley, CA

10 GEOGRAPHICAL DATA

ACREAGE OF NOMINATED PROPERTY less than 1 acre

UTM REFERENCES Quandrangle name Carson City

Quandrangle scale 7.5

A 1, 1 2, 6, 0 9, 0, 0 4, 3, 3, 6 8, 8, 0
ZONE EASTING NORTHING

B
ZONE EASTING NORTHING

C

D

VERBAL BOUNDARY DESCRIPTION

Parcel bounded by Curry Street and Carson Street in Carson City, NV.

Section 20, Township 15 North, Range 10 East

LIST ALL STATES AND COUNTIES FOR PROPERTIES OVERLAPPING STATE OR COUNTY BOUNDARIES

STATE	CODE	COUNTY	CODE
N/A			

STATE	CODE	COUNTY	CODE
-------	------	--------	------

11 FORM PREPARED BY

NAME / TITLE

Richard C. Datin, Curator, Virginia & Truckee Railroad Museum/

with additions by
Pat Lawrence-Dietz

ORGANIZATION

DATE

Nevada State Museum/Office of Historic Preservation and Archeology 5/20/83 - January 84

STREET & NUMBER

TELEPHONE

Capitol Complex, 201 S. Fall Street, Room 106 (702) 885-4810 (702) 5138

CITY OR TOWN

STATE

Carson City

Nevada 89710

12 STATE HISTORIC PRESERVATION OFFICER CERTIFICATION

THE EVALUATED SIGNIFICANCE OF THIS PROPERTY WITHIN THE STATE IS:

NATIONAL

STATE X

LOCAL

As the designated State Historic Preservation Officer for the National Historic Preservation Act of 1966 (Public Law 89-665), I hereby nominate this property for inclusion in the National Register and certify that it has been evaluated according to the criteria and procedures set forth by the National Park Service.

STATE HISTORIC PRESERVATION OFFICER SIGNATURE

TITLE

Ronald M. Jones
Deputy State Historic Preservation Officer

DATE

11 July, 1984

FOR NPS USE ONLY

I HEREBY CERTIFY THAT THIS PROPERTY IS INCLUDED IN THE NATIONAL REGISTER

DATE

8/30/84

DIRECTOR, OFFICE OF ARCHEOLOGY AND HISTORIC PRESERVATION

ATTEST

DATE

KEEPER OF THE NATIONAL REGISTER

UNITED STATES DEPARTMENT OF THE INTERIOR
NATIONAL PARK SERVICE

**NATIONAL REGISTER OF HISTORIC PLACES
INVENTORY -- NOMINATION FORM**

FOR NPS USE ONLY

RECEIVED

DATE ENTERED

CONTINUATION SHEET Description ITEM NUMBER 7 PAGE 1

The structure's roof was originally punctuated by a brick stove chimney. This element was later replaced by a metal stove pipe vent.

Exterior walls combine four foot, vertical tongue and groove wainscotting with five inch, rustic channel siding. The east elevation includes a three by ten foot bay window which originally housed the station ticket office. The east elevation bay is defined by double hung windows similar to those found on the body of the building.

Wooden, four panel, sliding freight doors are located on the south, east and west building elevations and provide access to the depot's freight room. Above these doors are found eight-light transoms. Door units are enframed by simple board surrounds. East and west passenger and freight entrances originally included poured concrete platforms. These slabs were not moved to the Carson City site; however, similar platforms will be reproduced during the restoration.

Eight foot station signs, reading "Wabuska" were originally fixed to the building's gable ends. These signs included the distance to San Francisco and Keeler, California, stations located along the Southern Pacific line. These original station signs were later replaced by smaller fixtures, one of which survives in the museum's collection.

The interior of the depot is divided into three major service areas by twelve foot interior partitions. These areas were designed to accommodate freight, the depot ticket office, and passenger facilities. The southern half of the building is occupied by the freight section which adjoins the freight agent's office. The northern end of the structure includes the passenger waiting room, rest rooms and storage areas.

Surviving interior features include the station's original ticket counter found between the waiting room and ticket office. Interior partitions are also original and are characteristic of those used in Southern Pacific stations of the period. These partitions are constructed of five foot, vertical, tongue and groove wainscotting and horizontal, tongue and groove panelling. Lower walls are painted dark green while upper portions are painted a contrasting lighter green as is characteristic of Southern Pacific stations.

WABUSKA RAILROAD STATION
UNITED STATES DEPARTMENT OF THE INTERIOR
NATIONAL PARK SERVICE

**NATIONAL REGISTER OF HISTORIC PLACES
INVENTORY -- NOMINATION FORM**

FOR NPS USE ONLY

RECEIVED

DATE ENTERED

CONTINUATION SHEET SIGNIFICANCE ITEM NUMBER 8 PAGE 1

The Carson and Colorado station was dismantled shortly after the Southern Pacific depot was put in service. The C&C depot was subsequently moved to Churchill, Nevada, where it was reconstructed. The C&C station has not survived.

The Southern Pacific continued to ship agricultural products to the neighboring mining camps through Wabuska during the first years of depot operation. Shipments of agricultural products were supplemented by copper ore by 1910.

In that year the Nevada Copper Belt Railroad was operating between Wabuska and the mining camp of Ludwig. The Nevada Copper Belt linked the Nevada Douglas, Mason Valley and Blue-stone mines with the Southern Pacific Railroad until 1947. Declining freight and passenger traffic forced the Wabuska station to close as of September 1, 1979.

RECEIVED

MAR 24 2014

CARSON CITY
PLANNING DIVISION

Carson City Planning Division
108 E. Proctor Street • Carson City NV 89701
Phone: (775) 887-2180 • E-mail: planning@carson.org

FOR OFFICE USE ONLY:

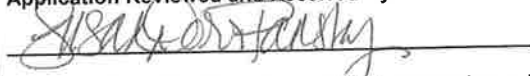
**HISTORIC RESOURCES
COMMISSION**

FEE: None

SUBMITTAL PACKET

- ☒ Application Form with signatures
- ☒ Written Project Description
- ☒ 16 Completed Application Packets-Application form, maps, supporting documentation (1 Original + 15 Copies)
- ☒ CD containing application data (pdf format)
- N/A -- ☐ Documentation of Taxes Paid-to-Date

Application Reviewed and Received By:



Submittal Deadline: See attached HRC application submittal schedule.

FILE # HRC - 14 - 014

Nevada State Railroad Museum 775-687-6953

APPLICANT

PHONE #

2180 S Carson Street, Carson City, NV 89701

MAILING ADDRESS, CITY, STATE, ZIPgfackerman@nevadaculture.org**EMAIL ADDRESS**

State of Nevada, Division of State Lands 775-684-2720

PROPERTY OWNER

PHONE #

901 S Stewart Street, Suite 5003, Carson City, NV 89701

MAILING ADDRESS, CITY, STATE, ZIPdmarlow@lands.nv.us**EMAIL ADDRESS**

Frank Ackerman

775-687-6953

APPLICANT AGENT/REPRESENTATIVE

PHONE #

2180 S Carson Street, Carson City, NV 89701

MAILING ADDRESS, CITY, STATE ZIPgfackerman@nevadaculture.org**EMAIL ADDRESS**

Project's Assessor Parcel Number(s):

003-302-04, 003-302-02

Street Address

2180 S Carson St, Carson City, NV

ZIP Code

89701

Project's Master Plan Designation

Public/Quasi-Public

Project's Current Zoning

Public

Nearest Major Cross Street(s)

Fairview Dr and S Carson St/US 395

Briefly describe the work to be performed requiring HRC review and approval. In addition to the brief description of your project and proposed use, provide additional page(s) to show a more detailed summary of your project and proposal. NOTE: The Historic District Ordinance and Historic District Design Guidelines, as well as Policy Statements, are available in the Planning Division to aid applicants in preparing their plans. If necessary, attach additional sheets.

The Nevada State Railroad Museum (NSRM) is proposing to install a new sign and landscaping at the NSRM.

The project is funded in part by the Nevada Department of Transportation and the Federal Highway Administration.

The project will remove the existing sign, and install a new sign with a dynamic LED display. The overall dimensions of the new sign will be 11' high, by 10' wide, by 2' deep.

Landscaping elements of the project include: removing the decorative boulders along US 395, prune one cottonwood tree and remove willows in a drainage ditch, install a border of fractured rock between the sidewalk and US 395, replenish the fractured rock along fence lines and around the train tracks, construct new fencing and gates, construct a portable platform to allow photographers to safely cross the rails to photograph the engine on special occasions.

Does the project require action by the Planning Commission or the Board of Supervisors? ☐ Yes ☐ No If Yes, please explain

Yes, the new sign will require a special use permit approved by the Carson City Planning Commission.

Will the project involve demolition or relocation of any structure within or into the Historic District? ☐ Yes ☐ No If Yes, please describe:

The Nevada State Railroad Museum is not within the West Side Historic District; however, the parcel does include the Wabuska Depot which is listed on the National Register of Historic Places.

Reason for project:

Improve visibility and appearance of the museum sign and grounds.

SUPPORTING DOCUMENTATION

Each application requires 16 copies, folded to 8 1/2 x 11 inches, of quality site plan and drawings showing work to be performed on the subject project which requires HRC approval. Basically, this is any work which will affect the exterior of any structure and any modifications to the site, i.e., fences, walls, or major landscaping. The name of the person responsible for preparation of the plans and drawings shall appear on each sheet.

Attached is a Plan Checklist to aid preparation of plans and architectural drawings. It is understood that all checklist items will not be included in all projects. The list is intended to give the applicant an idea of the breadth of review by the Commission on those items which are included in the subject project. Photographs can be used for illustration and discussion, but are not acceptable as substitutes.

By: Charles Donahue
Owner's Signature ACTING ADMINISTRATOR

STATE OF NEVADA
Owner's Printed Name

Frank Ackerman
Applicant's/Agent's Signature Museum Director

Nevada State Railroad Museum
Applicant's/Agent's Printed Name

Nevada State Railroad Museum Gateway Improvement Project
Carson City, NV



Additional Documentation for the Historic Resources Commission

April 10, 2014

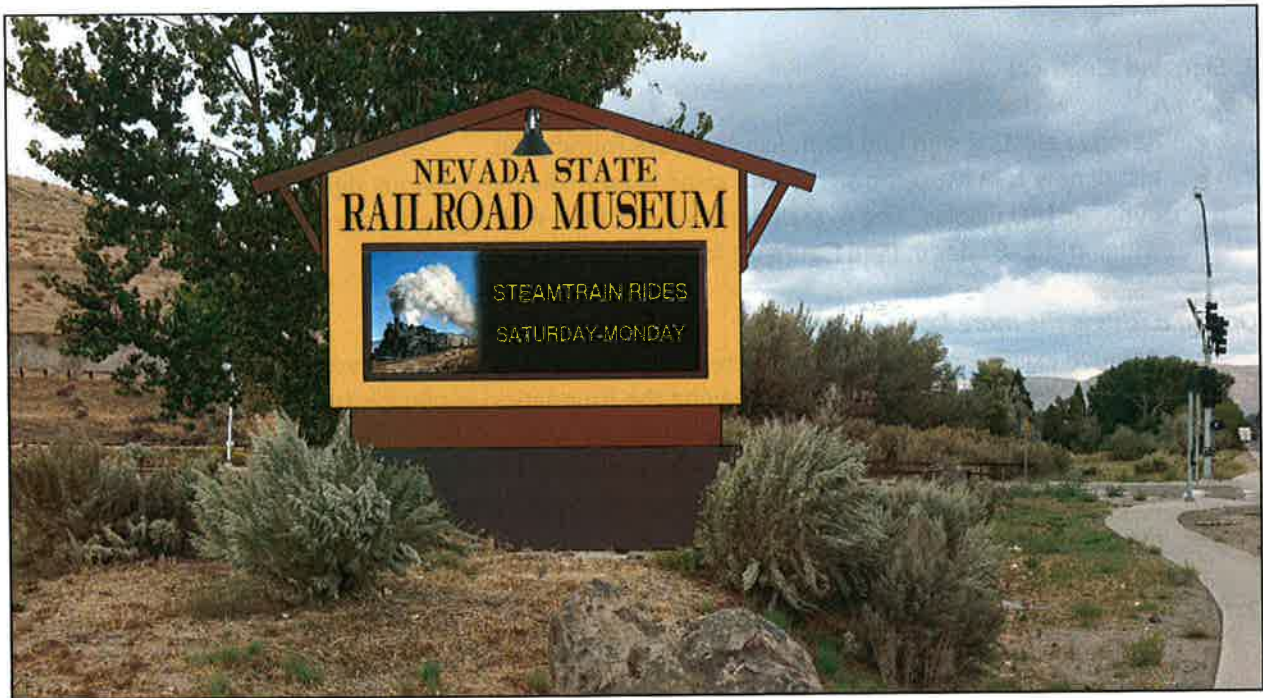


Photo simulation of the proposed Nevada State Railroad Museum sign. (Sandra Wendall & Associates)

Project Summary

The Nevada State Railroad Museum (NSRM) is proposing a project to install new landscaping and a sign with an LED display at the NSRM grounds at 2180 S Carson Street in Carson City. The NSRM grounds contain three historic structures: the Wabuska Railroad Station built in 1906, the Louis Nelson House built c 1920s, and the McKeen Motor Car #22 built in 1910. The Wabuska Depot, despite being moved to its present location in 1983, is listed on the National Register of Historic Places (NRHP). The McKeen Motor Car #22 is a National Landmark.

Because the Wabuska Depot is listed on the NRHP, projects that may affect it fall under the review of the Carson City Historic Resource Commission (HRC).

Because the project uses federal funds, it must also comply with Section 106 of the National Historic Preservation Act of 1966, including consultation with the State Historic Preservation Office (SHPO). SHPO has reviewed the project. At the SHPO's request, changes were made to the design of the new sign to ensure that it meets the Secretary of the Interior's Standards for the Treatment of Historic Properties. SHPO has concurred that the project will have no adverse effect to any historic property.

Written Project Description

The NSRM has received funding for the project from the Federal Highway Administration (FHWA). The funds for the project are limited and the complete scope of work may not be able to be completed for several years. However, the NSRM would like the HRC to review all of the project elements at this time, to give the HRC an understanding of the scope of the project in its entirety, and to be more efficient. The NSRM understands that if the scope of work changes, they will have to request another HRC review on the project.

The scope of the project, *in order of funding priority*, includes:

1. Sign and Electrical

Option A:

- Remove existing sign and demolish concrete foundation.
- Install new NSR Museum sign in the same location as the previous sign. The new sign will have a dynamic LED display. The overall dimensions of the new sign are approximately 11' high, by 10' wide, and 2'-6" deep. (See Figure 22 for sign details.)

Option B (this is the more likely option):

- Remove existing sign but leave concrete foundation in place and cover with an inch of soil so it will not be visible. The purpose of leaving the foundation in place is to save money and prevent the infiltration of noxious weeds in the disturbed soil.
- Install new NSR Museum sign a few feet to the south of the original sign location. The new sign will have a dynamic LED display. The overall dimensions of the new sign are approximately 11' high, by 10' wide, and 2'-6" deep.

2. Boulders

- Remove the decorative boulders presently in front of the NSR Museum and stockpile the boulders behind the NSR Museum.

3. Elm Tree, Willows, Cottonwood Tree

- Remove one elm tree at north end of drainage ditch
- Remove willows at north end of drainage ditch
- Have a certified arborist prune the existing cottonwood tree

4. Drainage Improvement, Crushed Rock

- Fill voids where water accumulates east of the railroad tracks
- Add 2" layer of 1.5" fractured brown/black river rock and thoroughly compact (see Figure 20)
- Install 4 foot border of 3x10" fractured river rock along edge of the level area adjacent to the west side of the drainage ditch (see Figure 21).
- Spread pre-emergent granules in all rock areas to prevent weed seed germination

5. Access Gate

- Construct new fencing and gates to limit access to museum staff only.

6. Entry Fence Addition

- Construct new fencing to improve site entry definition. Fencing will be made of wood, with two horizontal rails and match existing fencing.

7. Restricted Access Improvements

- Construct new fencing and gate to limit access to museum staff and RR engineers. Fencing will be made of wood, with two horizontal rails. It will match the existing fence.

8. Rock Mulch

- Add 1.5" layer of "Nevada Gold" fractured rock, or equivalent, to the landscape border on the south side of Access Rd between the two-rail fencing and Access Rd (see Figures 18 and 19).
- Remove soil along the edge of the roadway so that the top of the rock mulch is flush with the top of the roadway and thus contained. Currently the area between Access Rd and the fenceline is graveled.

9. NDOT Planter Improvement (between existing sidewalk and US 395)

- Remove and dispose of dirt and cinders to a depth of 3".
- Add 2.5" deep layer of 1.5" Nevada Gold fractured rock, keeping it .5" below the top of the adjacent sidewalk and curbs.

10. Portable Platform

- Construct a moveable platform to allow for tourists/photographers to safely cross the rails to photograph the engine on special occasions. To be designed at a future date.

NSRM expects to start construction on project elements 1 through 5 this summer, 2014.

Review of Historic Properties

Historic Architectural Resources

An architectural survey was not conducted for the undertaking. The Area of Potential Effect contains three architectural resources constructed during or prior to 1959:

- The Wabuska Railroad Station, built 1906
- Louis Nelson House, built c 1920s
- The McKeen Motor Car #22, built 1910

The Wabuska Railroad Station was listed on the National Register of Historic Places (NRHP) in 1984, a year after it was moved from its original location in Wasbuska, Lyon County, to its current site on the grounds of the NSR Museum. The building is listed at the state level of significance.

The Louis Nelson House was relocated to the NSR Museum grounds in 1985. Originally, it stood at 211 W 8th Street, Carson City. The building is being treated as eligible for Section 106 purposes.

The McKeen Motor Car #22, was listed as a National Historic Landmark in 2013. By its nature, the McKeen Motor Car is a movable object, and it regularly travels on tracks that run through the APE. It is permanently housed at the NSR Museum's engine house located southwest of the APE.

Please note that the Water Tank and the "A" Frame Gallows Turntable are modern replicas and are not historic. The Water Tank is a two-thirds scale replica of the Virginia & Truckee Railroad water tank once

located at Franktown. The Water Tank replica was constructed in 1987. The Standard "A" Frame Gallows Turntable is a modern structure based on a 1906 Southern Pacific design.

Archaeology

The project is in a developed urban area and the potential for archaeological resources is low. In the event of an inadvertent archaeological discovery, no further construction in the area of discovery will proceed until the requirements of 36 CFR 800.13 and Nevada Revised Statutes 383 have been satisfied.

Finding of Effect

36 CFR 800.5 says that, "An adverse effect is found when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling, or association." The two historic properties in the APE have been relocated and the properties' characteristics that qualify them for inclusion in the National Register do not include location or setting. However, through sensitive landscaping and arrangement of the buildings, the NSR Museum has recreated a historically appropriate setting as much as is possible in the current location along busy US 395.

Historically, the primary purpose for landscaping around railroads was safety. To prevent fires, vegetation around tracks and buildings was discouraged. Intentional plantings were usually limited to shade trees, often cottonwoods which could survive the arid Nevada climate with a minimum of care. Historic photographs of the Wabuska Depot shows cleared, hard packed ground surrounding the depot with rock ballast along the tracks. The Nevada State Railroad Museum Gateway Improvement Project will continue to enhance the historically appropriate setting created around the relocated Wabuska Depot. The preservation of the native saltgrass and use of native plants such as sagebrush will be in keeping with a historic railroad depot setting. Angular ballast (fractured river rock) will be replaced along the tracks and rock mulch will be replenished along the fencelines to keep invasive weeds from growing along the fences. Rock mulch will also be placed along the section between the existing concrete sidewalk and US 395.

Based on 36 CFR 800.5, the Nevada State Railroad Museum Gateway Improvement Project will have no adverse effect to the National Register listed Wabuska Depot, the National Register eligible Nelson House, or the National Landmark listed McKeen Motor Car for the following reasons:

- (i) The project will not destroy or damage all or part of the historic properties.
- (ii) The project will not alter the historic properties.
- (iii) The project will not remove of the historic properties from their historic location. The two buildings were previously removed from their historic locations as part of a different project and the McKeen Motor car is an object that was intended to move.
- (iv) The project will not change the character of the properties' use or of physical features within the property's setting that contribute to its historic significance. The new sign will follow the Secretary of the Interior's Standards of being compatible with the historic surroundings in scale, material, massing, and style while still being identifiable as a modern element. The removal of

the boulders and the refreshing of rock ballast around the railroad tracks and rock mulch along fence lines is consistent with the historic setting of the Wabuska Depot.

(v) The project will not introduce visual, atmospheric or audible elements that diminish the integrity of the property's significant historic features. With input from the SHPO, the minimal landscaping and new sign have been designed to meet the Secretary of the Interior's Standards and will not diminish the property's significant historic features.

(vi) The project will not result in the neglect of the historic properties. The project will maintain the setting through tree pruning, and removal of overgrown plants and will prevent neglect.

(vii) The project will not transfer, lease, or sell the property out of Federal ownership. Currently, the property is owned by the state and the project will not change the status of ownership.

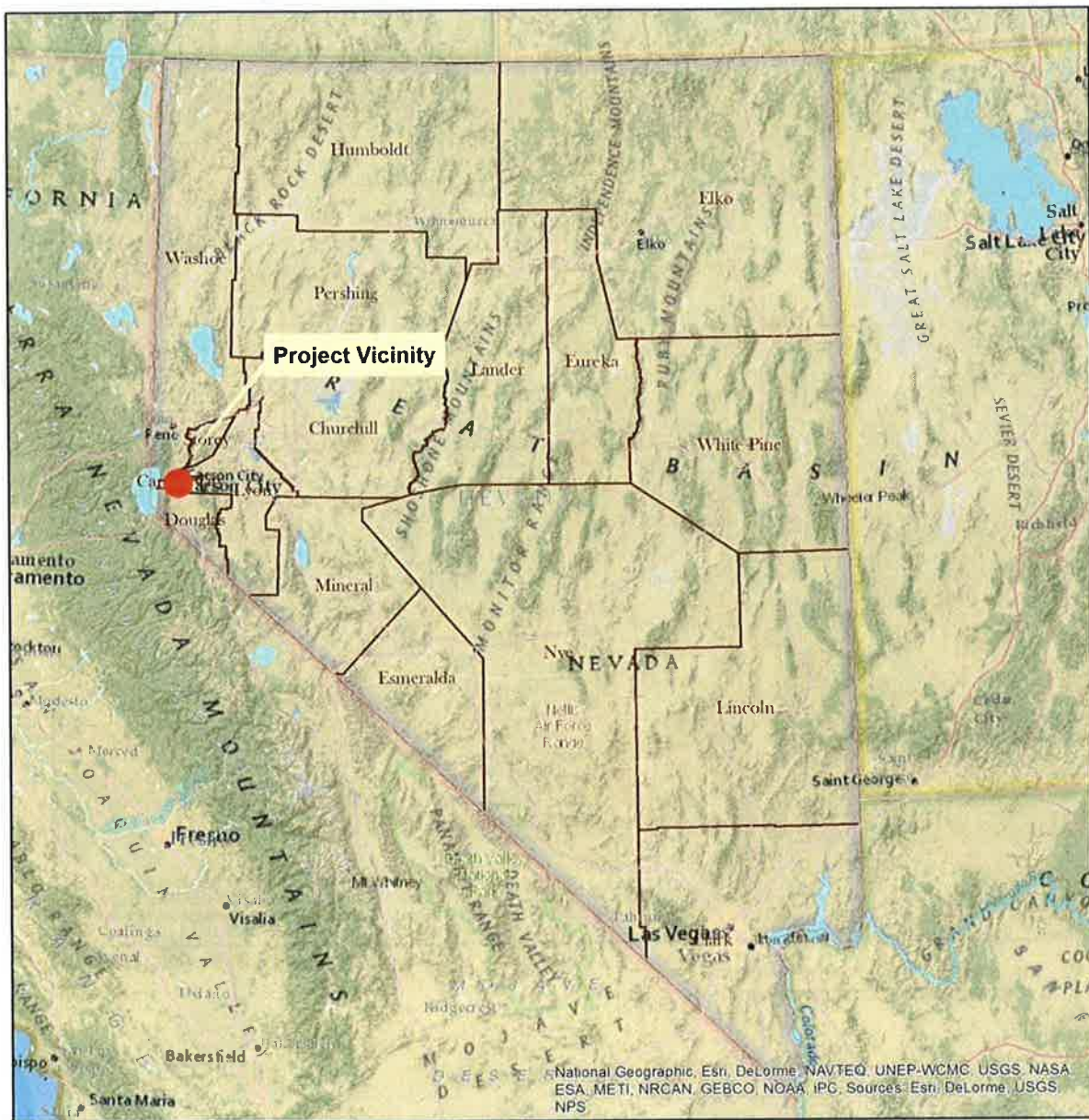
In a letter dated March 12, 2014, the SHPO concurred that the project would have no adverse effect.

Documentation of Taxes Paid-to-Date

Not applicable. The NSRM is a state property and does not pay taxes.

Maps

Nevada State Railroad Museum Gateway Project Carson City, Carson City County, NV SHPO Undertaking # 2013-2669



0 15 30 60 90 120
Miles

Map for informational purposes only.
Compiled from best available Data.
NDOT Cultural Resources Section, January 2014

Figure 1: General Vicinity Map

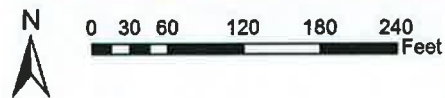
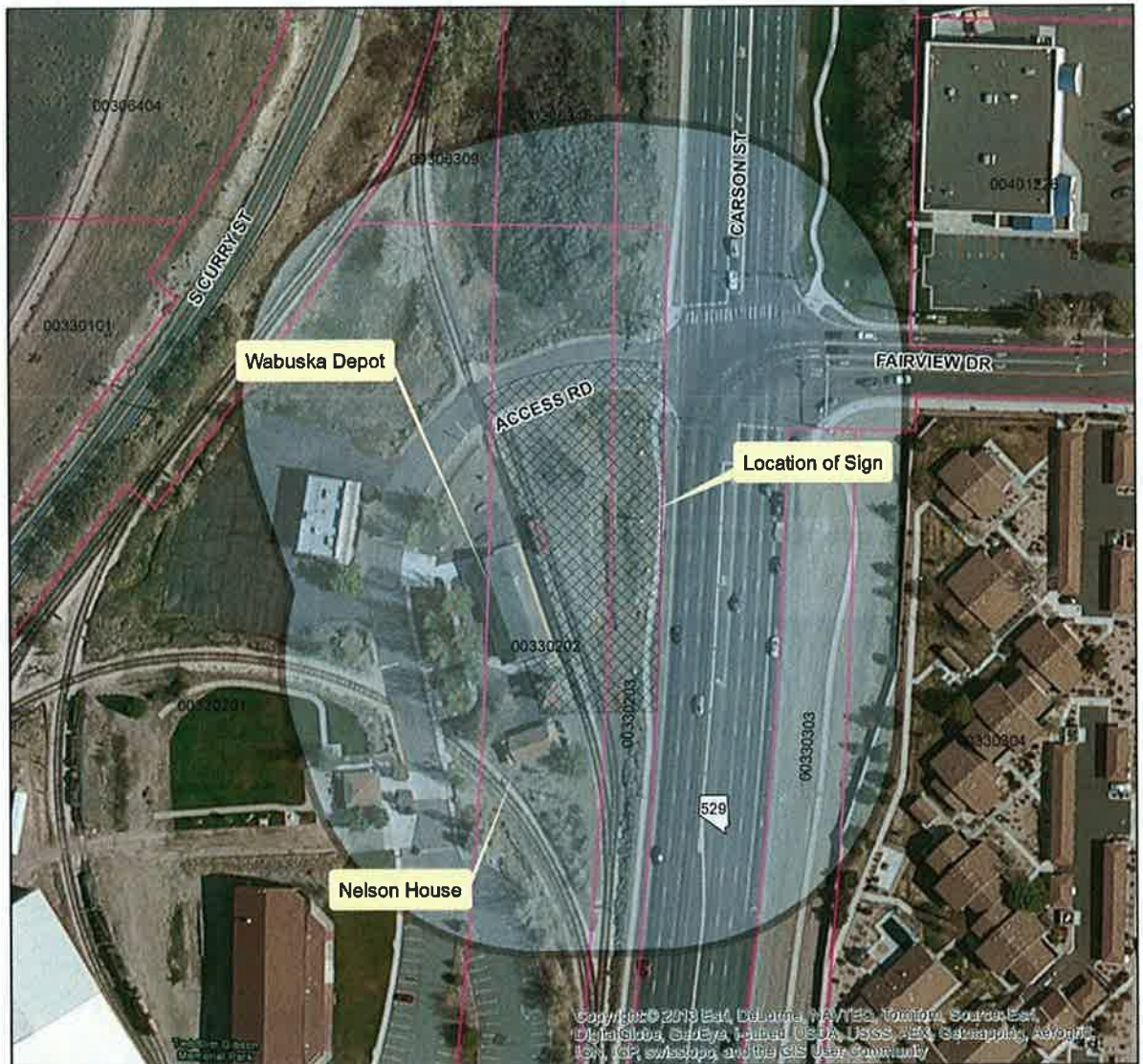
**Nevada State Railroad Museum Gateway Project
Carson City, Carson City County, NV
SHPO Undertaking # 2013-2669**



Map for informational purposes only.
Compiled from best available data.
NDOT Cultural Resources Section, January 2014

Figure 2: APE Map at 1:24,000 scale.

**Nevada State Railroad Museum Gateway Project
Carson City, Carson City County, NV
SHPO Undertaking # 2013-2669**



1:1,400

- Area of Direct Impacts
- Area of Potential Effects
- Parcels

Map for informational purposes only.
Compiled from best available data.
NDOT Cultural Resources Section, January 2014

Figure 3: APE Map at 1:1,400 scale.

Photographs of Project Area



Figure 4: View to the southwest from the intersection of Carson Street (US 395) and Fairview Dr (Google Streetview).



Figure 5: View to the west from Carson Street (US 395) (Google Streetview).



Figure 6: Existing conditions at the corner of US 395 and Access Rd. The area between the sidewalk and US 395 will be landscaped with “Nevada Gold” rock mulch. The existing sign will be replaced and the new sign installed in approximately the same location (Google Streetview).



Figure 7: Existing conditions: ballast and rock mulch between Access Rd and the fence line and between the railroad tracks and the drainage ditch. If funding is available, a gate and additional fence sections that match the existing fence sections will close off this area from the public. (Google Streetview).



Figure 8: Nelson House, built the 1920s and moved to its current location on the NSR Museum grounds in 1985. (Friends of the Nevada State Railroad Museum).



Figure 9: Wabuska Depot, with replica water tank in background. Photo by Richard Jacobs, November 25 2013. (www.akronrrclub.wordpress.com/tag/nevada-state-railroad-museum)



Figure 10: The McKean Motor Car #22 pulls up to the Wabuska Depot (in Carson City), May 11, 2010. (Scott Schrantz)

Historic Photographs



Figure 11: Wabuska Depot in 1925 (David Myrick. *Railroads of Nevada and Western California Volume III*, 2007, page 82).

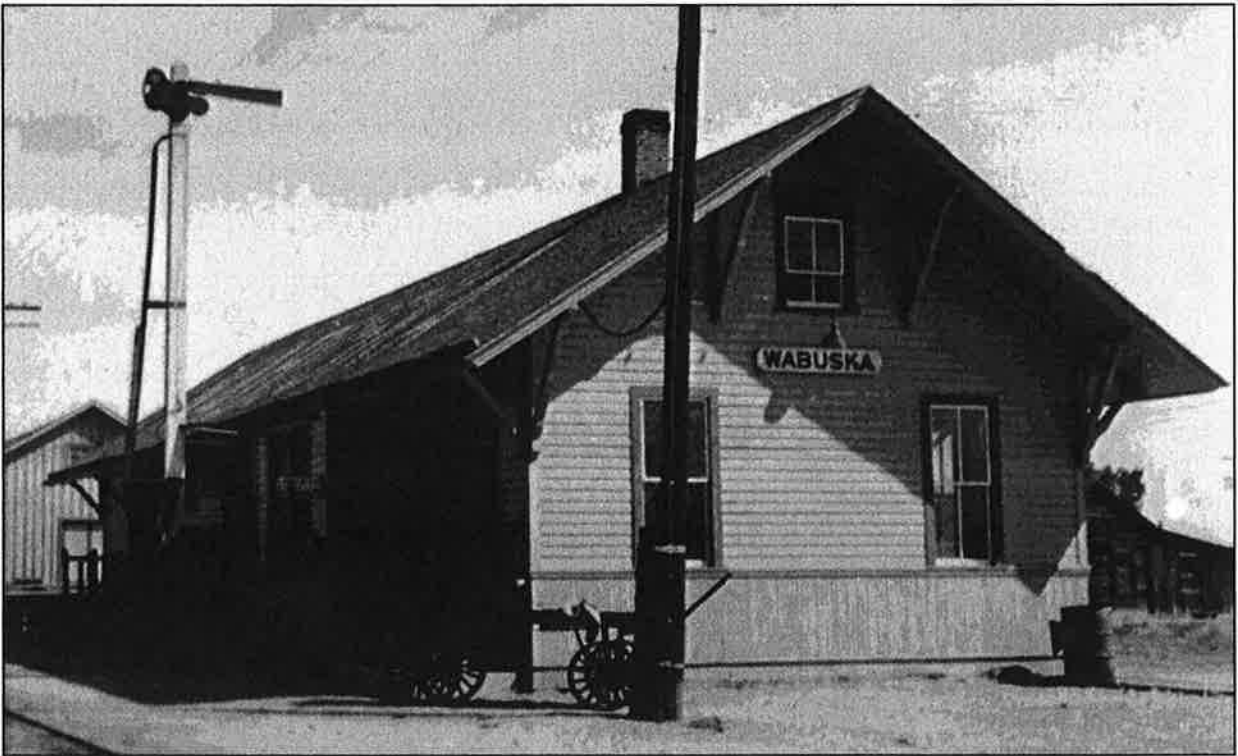


Figure 12: Wabuska Depot, September 1946. Al Phelps photo, NSRM



Figure 13: Wabuska Depot, 1969. (Hancock, Nevada State Railroad Museum Collection)



Figure 14: Wabuska Depot, 1969. (Hancock, Nevada State Railroad Museum Collection)



Figure 15: Wabuska Depot, 1969. (Hancock, Nevada State Railroad Museum Collection)



Figure 16: Wabuska Depot shortly before being relocated to the Nevada State Railroad Museum in Carson City. Photo by Rob Sarberenyi, July 1980. www.pbase.com

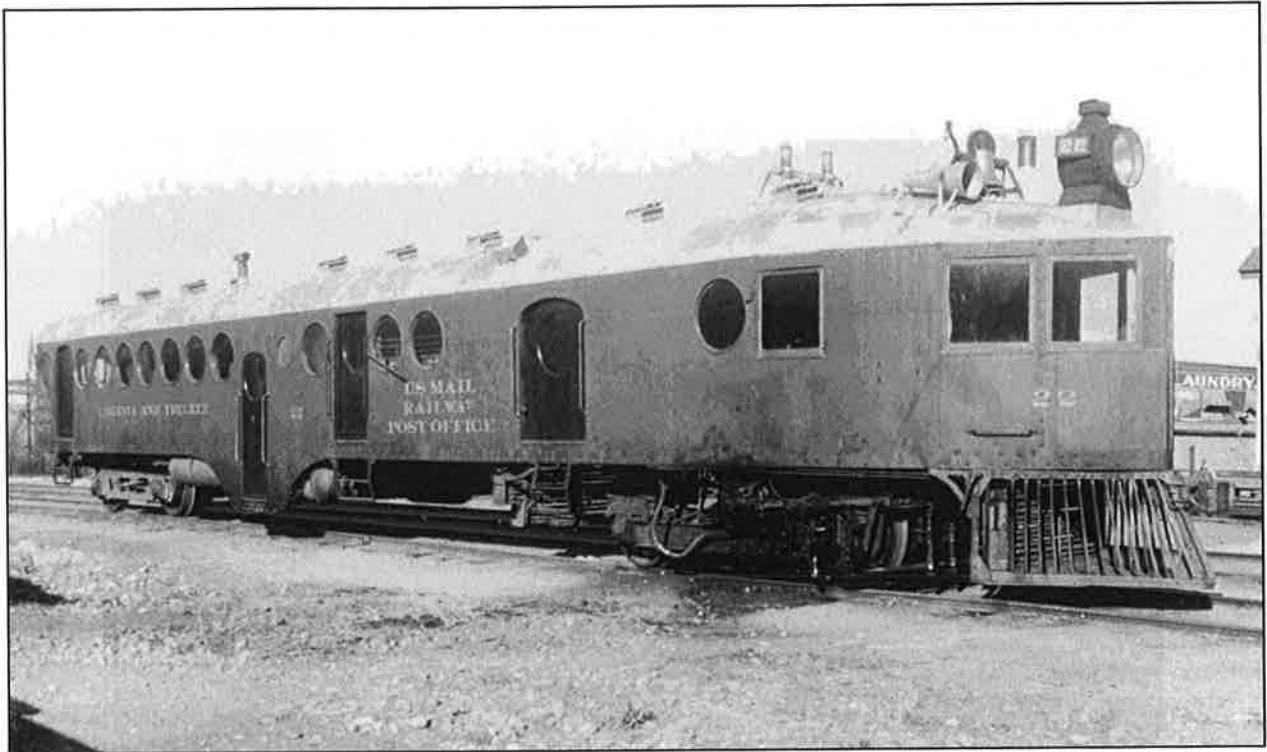


Figure 17: McKean Motor Car #22 in Reno, c 1938. (Nevada State Railroad Museum Collection).

Rock Mulches for the Nevada State Railroad Museum Gateway Improvement Project



Figure 18: “Nevada Gold” rock mulch.



Figure 19: “Nevada Gold” rock mulch.



Figure 20: 1.5" Fractured river rock to be used as ballast and ground cover along railroad tracks.



Figure 21: 3x10" Fractured river rock.

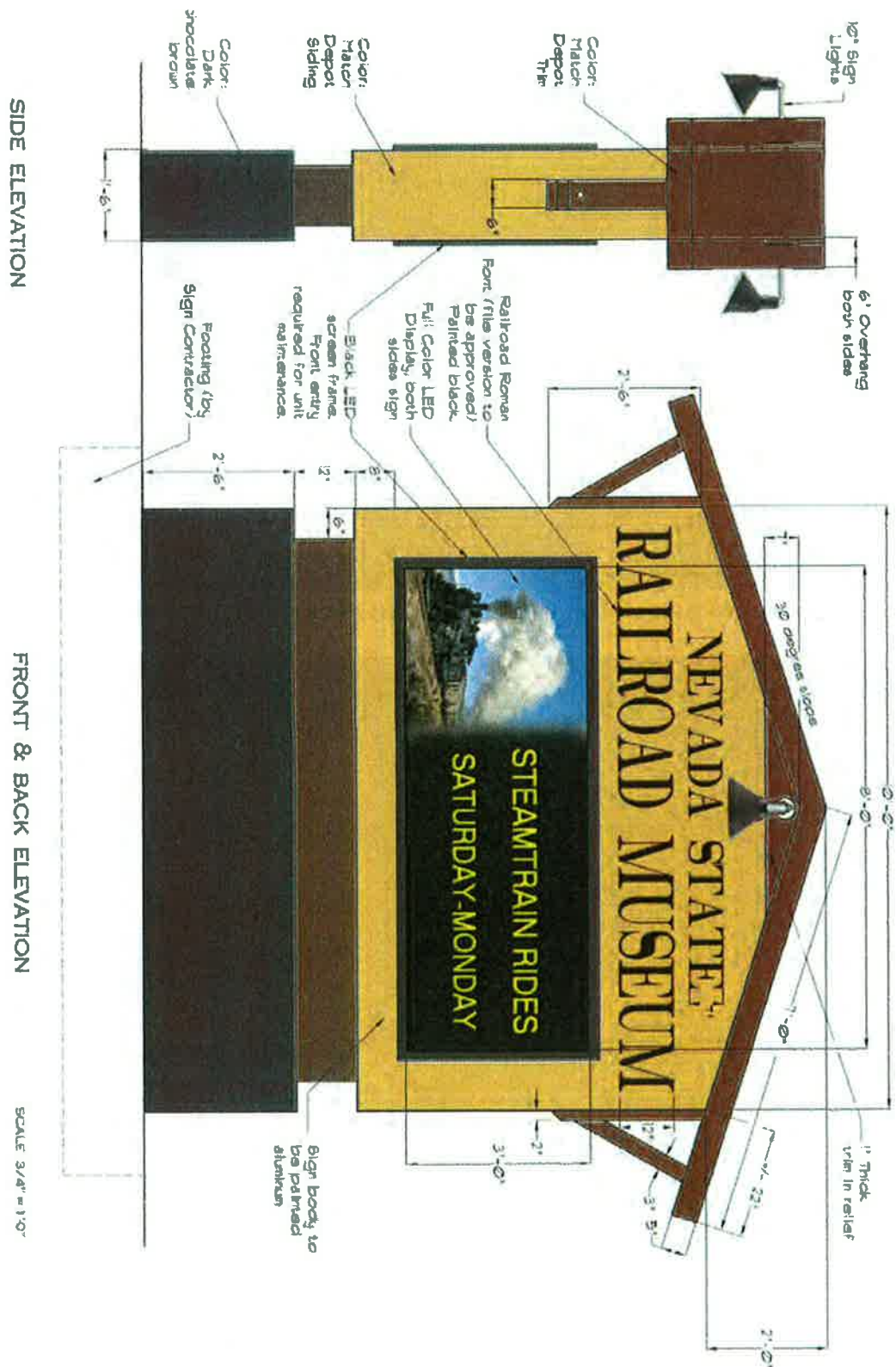


Figure 22: Based on comments from the SHPO, the sign depicted in the 60% plan set was revised, as shown in this figure.

LEO M. DROZDOFF, P.E.
Director
Department of Conservation and
Natural Resources

REBECCA L PALMER
State Historic Preservation Officer

BRIAN SANDOVAL
Governor

STATE OF NEVADA



Address Reply to:
901 S. Stewart St, Suite 5004
Carson City, NV 89701-5248
Phone: (775) 684-3448
Fax: (775) 684-3442

www.nvshpo.org

DEPARTMENT OF CONSERVATION AND NATURAL RESOURCES
STATE HISTORIC PRESERVATION OFFICE

March 12, 2014

Abdelmoez A. Abdalla
Environmental Program Manager
US Department of Transportation
Federal Highway Administration
705 North Plaza Street, Suite 220
Carson City, Nevada 89701

**RE: Nevada State Railroad Museum Gateway Improvement Project
Intersection of Fairview Drive and South Carson Street
Carson City and County, Nevada
EA # 73802, Federal #TAP-0529(002) NDOT Report #CC13-007
SHPO Undertaking 2013-2669**

Dear Mr. Abdalla:

The State Historic Preservation Office (SHPO) is in receipt of your request for consultation dated February 18, 2014 (received February 20, 2014) for the above referenced undertaking.

This undertaking scope of work includes ten items. The SHPO offers the following comments:

Item 1.

Sign and electrical, either Option A, demolish the concrete base and install sign at same place or Option B, leave present concrete foundation and install sign adjacent to the foundation. In Option B, the old concrete would be covered with soil to obscure the concrete.

The Railroad Museum and the SHPO both prefer Option A. The SHPO recommends more than 1" of soil over the existing foundation as suggested in the submission.

The SHPO has reviewed and commented about the design of the proposed sign and sent comments to NDOT for Railroad Museum review. SHPO requests were incorporated into the present design presented within the most recent submission. The massing and size of the new sign is significantly larger than the present sign, however, arguments were made by the Railroad Museum and its constituents that if there was going to be a new sign it needed to be of an adequate size and scale to be readable from a passing car.

Abdelmoez A. Abdalla

March 12, 2014

Page 2 of 2

Items 2 through 9.

There are significant historic properties at this location, including the National Register of Historic Places (NRHP) listed *Wabaska Railroad Station* (1906) and the *McKeen Motor Car #22*, a National Historic Landmark (NHL). Items 2 through 9 have previously been submitted to, and reviewed by, the SHPO for adherence to the *Secretary of the Interior's Standards for the Treatment of Historic Properties*.

Item 10.

Portable Platform, This moveable platform for photographers has not yet been designed and will require SHPO review prior to construction.

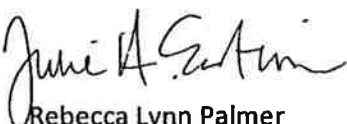
In accordance with Section 106 of the National Historic Preservation Act (NHPA), as amended, the SHPO concurs with the Federal Highway Administration (FHWA) Area of Potential Effect (APE) for architectural resources and the determination that this undertaking will have no adverse effect to historic properties.

The SHPO notes that a correction to Page 4 of the submission letter has been received via e-mail from NDOT, on behalf of the Federal Highway Administration (FHWA). The sentence should read "In consultation with the SHPO, FHWA determined that an archaeological survey was not necessary for this undertaking."

If any buried and previously unidentified resources are located during the project activities, the SHPO recommends that all work in the vicinity of the find cease and this office be contacted for additional consultation per 36 CFR 800.13.b.3.

Should you have any questions concerning this correspondence, please contact SHPO staff architectural historian Mara Thiessen Jones at (775) 684-3439 or by e-mail at mara.jones@shpo.nv.gov.

Sincerely,

for 
Rebecca Lynn Palmer
Acting State Historic Preservation Officer

19020



LOCATION MAP

SHEET INDEX:

- LANDSCAPING
- L1 TITLE PAGE
 - L2 EXISTING CONDITIONS PLAN
 - L3 PROPOSED IMPROVEMENTS PLAN
 - L4 SIGN DESIGN-BUILD SPECIFICATIONS
 - L5 IMPROVEMENT DETAILS
 - L6 IMPROVEMENT DETAILS

- ELECTRICAL ENGINEERING
- E1 SPECS AND SCHEDULES
 - E2 SITE PLAN

OWNER:

STATE OF NEVADA
NEVADA DEPT. OF TOURISM AND CULTURAL AFFAIRS
DIVISION OF MUSEUMS & HISTORY
CONTACT: FRANK ACKERMAN, MUSEUM DIRECTOR III
2180 SOUTH CARSON STREET
CARSON CITY, NEVADA 89101
(775) 681-6953, EXT. 223 FAX (775) 681-8294
gfackerman@nevadaculture.org

LANDSCAPE ARCHITECT:

SANDRA WENDEL & ASSOCIATES
CONTACT: SANDRA WENDEL, PRINCIPAL
1624 10TH STREET, SUITE 3
MINDEN, NEVADA 89423
(775) 782-8942
sandra@swlsa.com

ELECTRICAL ENGINEER:

JENSEN ENGINEERING, INC.
CONTACT: GERALD G. JENSEN, P.E.
9655 GATEWAY DRIVE, SUITE A
RENO, NEVADA 89521
(775) 852-2288 FAX (775) 852-3388
jeneng@nvcbell.net

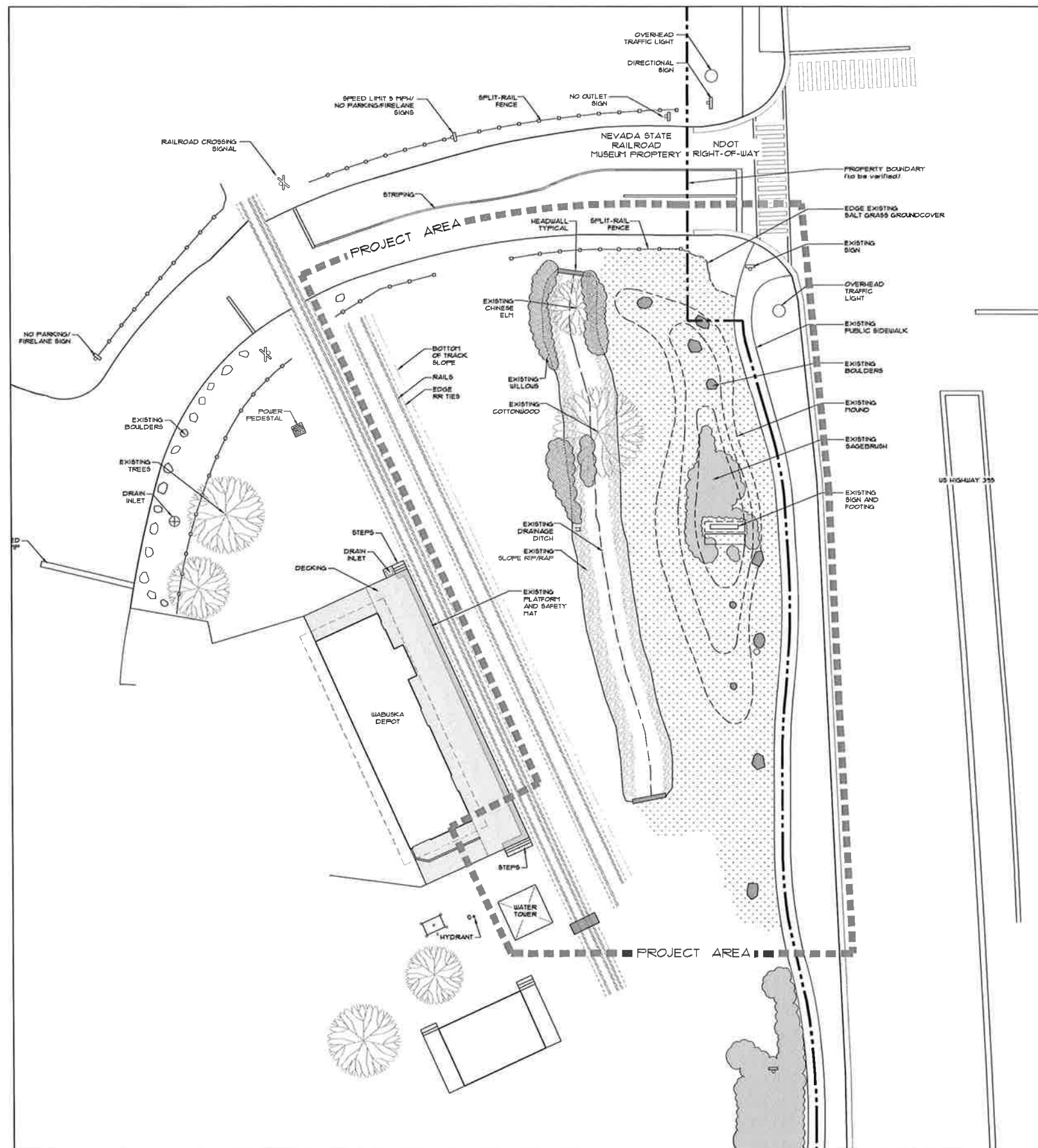
NEVADA DEPT. OF TRANSPORTATION:

FEDERAL PROJECT NO.: TAP-0529(002)
LOCAL PUBLIC AGENCY PROJECT NO.: 73802
CONTACT: KIRSTEN E. KEHE, P.E.
1263 STEWART STREET
CARSON CITY, NEVADA 89102
(775) 888-7669

REVISION	
Δ	
Δ	
Δ	
Δ	
Δ	

DATE	FEB 3, 2014
SCALE	NO SCALE
DRAWN	SW
CHECKED	SW

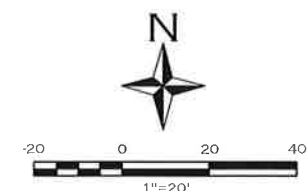
DRAWING TITLE	
TITLE SHEET	
SHEET:	L1
OF 6 SHEETS	



NOTES & CONSTRUCTION REQUIREMENTS

1. An engineered survey was not required or prepared as part of this project. Maps have been prepared by the Landscape Architect based on Google map projections and field verification. The Landscape Architect cannot be held responsible for measurements and field locations that are not 100% accurate. The Contractor shall bring any discrepancies to the attention of the Owner and Landscape Architect prior to beginning work.
2. All construction shall be done in accordance with these plans, the General Conditions and project specifications, the Standard Specifications for Public Works construction, Uniform Building Code or other local or state regulations that may apply - except as modified by these plans or Special Provisions.
3. Call Underground Service Alert '811' prior to initiating work. The existing lines shall be marked prior to digging or trenching. The Contractor shall protect all utilities and above grade features to remain from damage during all phases of construction. The Landscape Architect assumes no responsibility for the utilities not shown or not in the location shown on these plans.
4. The acquisition of all necessary permits associated with construction shall be the Contractor's responsibility.
5. The Contractor shall maintain all existing drainage facilities and grade elevations related to drainage and shall not erect any obstruction which shall interfere with drainage or storm drains.
6. It is understood that minor deviation may be necessary during the course of the project. These deviations may be done if approved by the Landscape Architect or Owner and if consistent with the original approved design.
7. No materials of any kind shall be stockpiled or construction equipment parked on concrete or asphalt surfaces. Stockpiling of materials and parking of vehicles or equipment should only occur in non-planting areas.
8. Excess or unsuitable material which cannot be used onsite shall be disposed of offsite at the sole expense of the Contractor.
9. The Contractor shall assume sole and complete responsibility of job site conditions during the course of construction for this project, including safety of all persons and property. This requirement shall apply continuously, and not be limited to normal working hours.
10. The Contractor shall protect from damage existing improvements on and around the site including, but not limited to, pavement, curb and gutter, sidewalk, landscaping, signage, storm and sanitary sewers, utilities, telephone and cable television. The Contractor shall assume sole responsibility for the repair and/or replacement of any improvements (new or existing) damaged throughout the course of construction, either as a direct result of his activities or his failure to adequately protect the improvements, at no cost to the Owner.
11. It is the Contractor's responsibility to verify locations and/or elevations and areas of possible conflict prior to beginning construction. Should the Contractor find any discrepancies between the conditions existing in the field and the information shown on these drawings, he shall notify the Owner's representative before proceeding with construction.
12. 48 hours minimum notice shall be given to any parties involved in inspections.
13. Sign Warranty to be per the terms of the Owner-Purchaser Contract.
14. The Contractor shall provide copies of all Sign and Electrical Plans, Warranties, Operations and Maintenance Manuals used on the project and instruct the Owner in the proper usage of the Sign.
15. All submittals shall meet the approval of the Landscape Architect and Owner prior to the start of production or installation. Shop drawings are required per notes Sheet L4 that shall meet or exceed the stated requirements.

EXISTING CONDITIONS



SANDRA WENDEL
& ASSOCIATES
1924 10TH STREET, SUITE 3
MIDCEN, NV 89423
775-782-8042 FAX 775-782-8043
NV #201 AND CA #22008
LANDSCAPE ARCHITECT



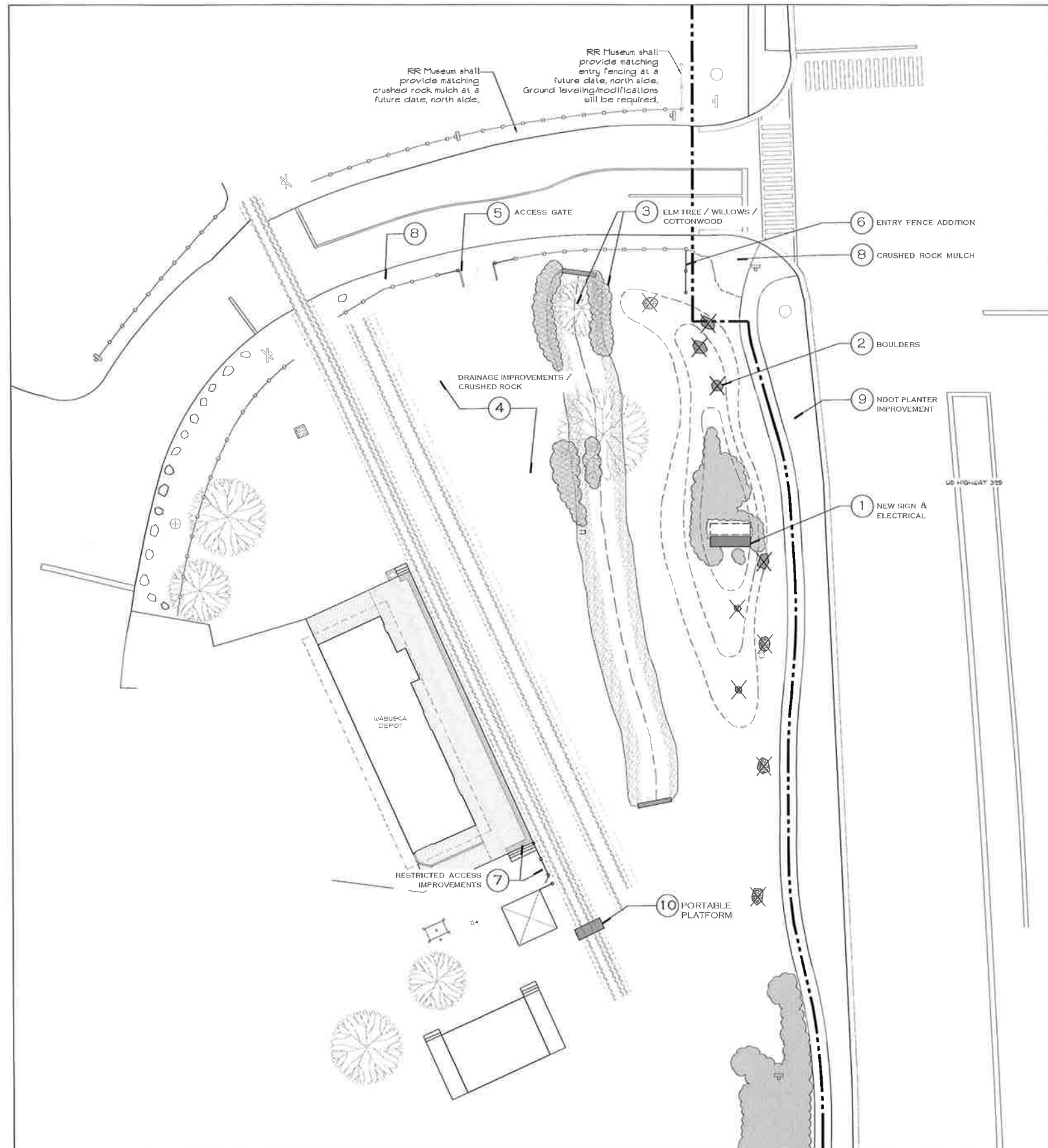
NEVADA STATE RAILROAD MUSEUM
GATEWAY IMPROVEMENT PROJECT
2180 SOUTH CARSON STREET
FEDERAL PROJECT # TAP-0529(002) - PROJECT # 73802

REVISION	
1	
2	
3	
4	
5	

DATE
FEB. 3, 2014
SCALE
1" = 20'
DRAWN
SW
CHECKED
SW

DRAWING TITLE
OVERVIEW
SHEET:
L2
OF 6 SHEETS

90% SUBMITTAL

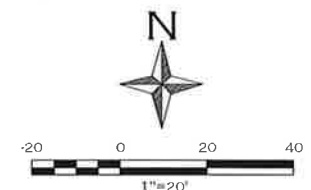


The following items have been determined by the Owner and site staff to be the most desirable aesthetic, safety and maintenance improvements. This plan has shown them not only for the purpose of the current budgeted improvement project, but to provide a future roadmap for work internally as additional funds become available.

DESCRIPTION OF IMPROVEMENTS IN ORDER OF BID PRIORITY

- 1 **SIGN & ELECTRICAL**
 - a. Demolish existing sign, posts, everything but the concrete footing, and remove from site. Leave footing in place and cover with thin layer of soil to conceal.
 - b. Install new approved sign per design/build specifications Sheet L4. Install adjacent to and south of existing footing.
 - c. Install all digital and electronic features, including burial of all conduits as specified on the Electric Drawings Sheets E1 and E2.
 - d. Repair all decking disturbed during the underground installation of power and digital lines connecting the building to the grounds on the south side of the depot.
- 2 **BOULDERS**
 - a. Remove the 10 existing boulders and stockpile in a location behind the railroad museum as directed by staff.
 - b. Do not disturb existing salgrass or drive on vegetated areas during the removal of boulders.
 - c. Fill all voids or depressions with clean topsoil free of debris, rocks, chemicals or other contaminants and water to settle in.
- 3 **ELM TREE / WILLOWS / COTTONWOOD**
 - a. Remove all above grade willows and below ground plant parts by mechanical means. Access shall be limited to the west side of the ditch to prevent disturbance of vegetation along the front of the site. Dispose of all material offsite.
 - b. Remove Elm tree completely.
 - c. Recompact disturbed slopes and mechanically stabilize. Install new or recycled fractured rock rip-rap on slopes to match existing. Spread approved pre-emergent granules in all disturbed areas to prevent weed seed germination.
 - d. Contract with certified Arborist/tree worker to prune existing Cottonwood for the purposes of tree health and improving visibility into the site.
- 4 **DRAINAGE IMPROVEMENT / CRUSHED ROCK**
 - a. Fill voids where water accumulates east of the railroad tracks as shown on Sheet L5 with 3/4 inch minus crushed roadbase.
 - b. Add 2 inches of 1 1/2 inch fractured brown/black River Rock and thoroughly compact in areas shown.
 - c. Install 4 foot border of 3x10 fractured River Rock (color to match) along the edge of the level area adjacent to the west side of the drainage ditch.
 - d. Spread pre-emergent granules in all rocky areas to prevent weed seed germination and water in per manufacturer's specifications.
- 5 **ACCESS GATE**
 - a. Construct new fencing and gates per details Sheets L5 to limited access to Museum staff. Fencing to match existing style and materials.
- 6 **ENTRY FENCE ADDITION**
 - a. Construct new fencing per details Sheet L6 to improve site entry definition. Fencing to match existing style and materials.
- 7 **RESTRICTED ACCESS IMPROVEMENTS**
 - a. Construct new fencing and gate per details Sheet L6 to limited access to Museum staff and RR Engineers.
- 8 **CRUSHED ROCK MULCH**
 - a. Add additional (one layer) 1 1/2 inch Nevada Gold fractured rock to the landscape border between the fencing and the roadway. Remove soil along the edge of the roadway so that the top of the rock mulch is flush with the top of the roadway and thus contained.
- 9 **NDOT PLANTER IMPROVEMENT**
 - a. Remove and dispose of dirt and cinders to a depth of 3 1/2 inches in the area shown.
 - b. Add 3 inches of 1 1/2 inch Nevada Gold fractured rock keeping it 1/2 inch below the top of adjacent sidewalk and curbs.
 - c. Install approved heavy duty strength weed cloth prior to laying rock.
 - d. Rock shall be thoroughly washed prior to installation and again after placement.
 - e. Install approved pre-emergent granules and water in per Manufacturer's specifications over the improved area.
- 10 **PORTABLE PLATFORM**
 - a. Construct moveable platform to allow for tourists/photographers to safely cross the rails to photograph the engine on special occasions. See Sheet L6.

IMPROVEMENT PLANS



SANDRA WENDEL
& ASSOCIATES
1624 100 STREET SUITE 3
MIDCOT, NV 89425
PH: 775-792-6643
FAX: 775-792-6645
WWW.SANDRAWENDEL.COM
NV #001 AND CA #2506



NEVADA STATE RAILROAD MUSEUM
GATEWAY IMPROVEMENT PROJECT
2180 SOUTH CARSON STREET
FEDERAL PROJECT # TAP-0529(002) - PROJECT # 73802

REVISION

1	
2	
3	
4	
5	

DATE FEB. 3, 2014

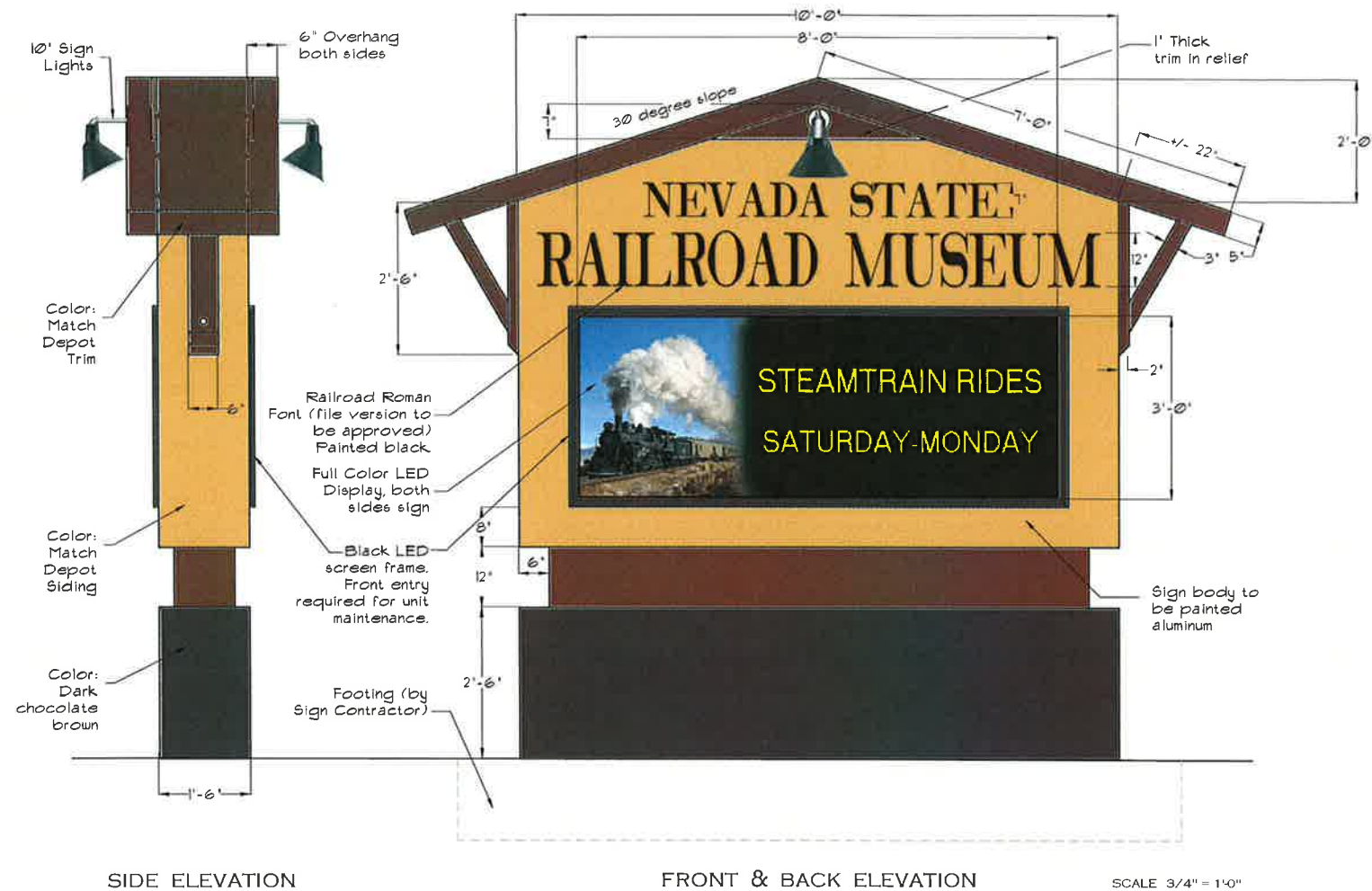
SCALE 1" = 20'

DRAWN SW

CHECKED SW

DRAWING TITLE
IMPROVEMENTS
SHEET:
L3
OF 6 SHEETS

90% SUBMITTAL

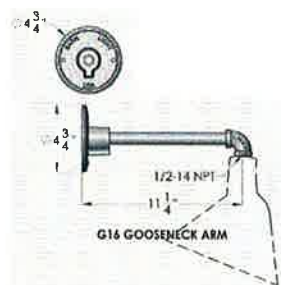


PROPOSED SIGN STREET VIEW

1 DESIGN / BUILD SIGN SPECIFICATIONS

SHOP DRAWING & CONSTRUCTION REQUIREMENTS:

- Conceptual approval from the Nevada State Railroad Museum for the sign shown in these drawings has been secured.
- Remove existing sign and footing completely. Care shall be taken to prevent damage to the surrounding landscape.
- Final design approval shall be obtained, based on detailed Shop Drawings submitted by the Sign Contractor.
- Physical submittals shall be supplied that clearly show key visual elements such as construction material, colors, and finishes, etc. The Contractor shall supply additional visual submittal if requested during the design/build process.
- Construction criteria include longevity in terms of materials ability to withstand climate, deterioration, vandalism, limited annual maintenance. All materials, paints, glues, finishes, lettering, fasteners and assembly shall be of the highest quality and conform to the best practices available.
- All notes and recommendations in the adjacent drawings are provided solely as design 'suggestions' based on the Landscape Architect's discussions with sign purveyors and construction professionals.
- The Contractor assumes full responsibility for final design/ build features.
- Conformance to the visual, scaled representation is required.
- Colors to match existing Depot.
- Provide the Railroad Museum with unopened, one gallon cans of each type of stain, paint or finish used in sign construction.
- See Civil Drawings, Sheets E1 and E2 for electrical and cable requirements. The Sign Contractor shall verify that proposed electrical design will suffice for this project.
- The LED performance specifications shall be determined by the Sign Contractor and approved for use by the Owner.
- Sign Design and Construction shall meet all applicable Carson City Building Codes, including the following: 2012 Northern Nevada Amendments, 2012 International Building Code and the 2011 National Electrical Code.



LIGHT SHADE SPECIFICATIONS:

- Fixture: Frontier Angle Shade
- Supplier: Barnlight Electric (or approved equal)
- Color: Black
- G16 Gooseneck Arm



EXISTING SIGN TO BE REMOVED

SIGN DESIGN-BUILD SPECIFICATIONS

SANDRA WENDEL
& ASSOCIATES
1624 10TH STREET, SUITE 3
CARSON CITY, NV 89801
PH: 775-233-1775
FAX: 775-233-1776
WWW.SANDRAWENDEL.COM
LANDSCAPE ARCHITECT



NEVADA STATE RAILROAD MUSEUM
GATEWAY IMPROVEMENT PROJECT
2180 SOUTH CARSON STREET
FEDERAL PROJECT # TAP-0529(002) - PROJECT # 73802

REVISION

1	
2	
3	
4	
5	

DATE

FEB. 3, 2014

SCALE

VARIES

DRAWN

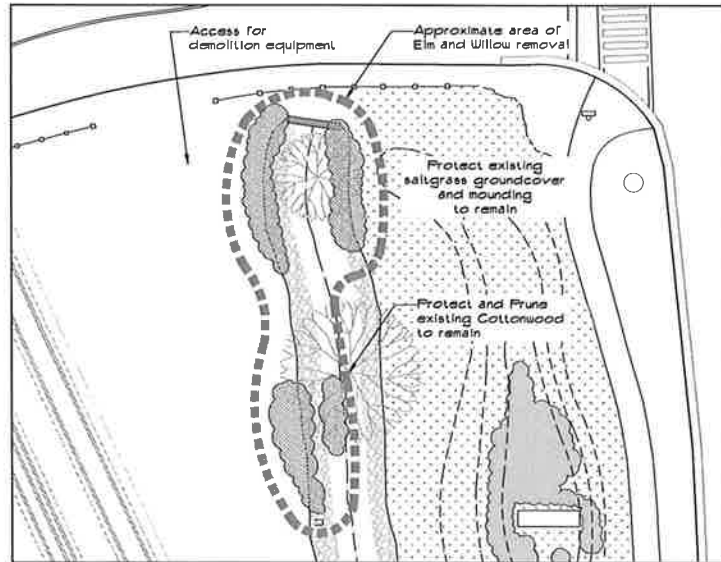
SW

CHECKED

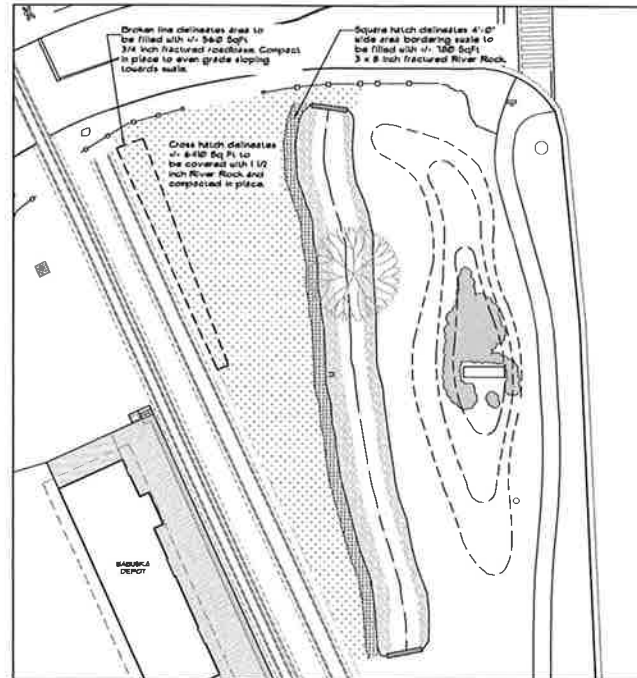
SW

DRAWING TITLE
SIGN
SHEET:
L4
OF 6 SHEETS

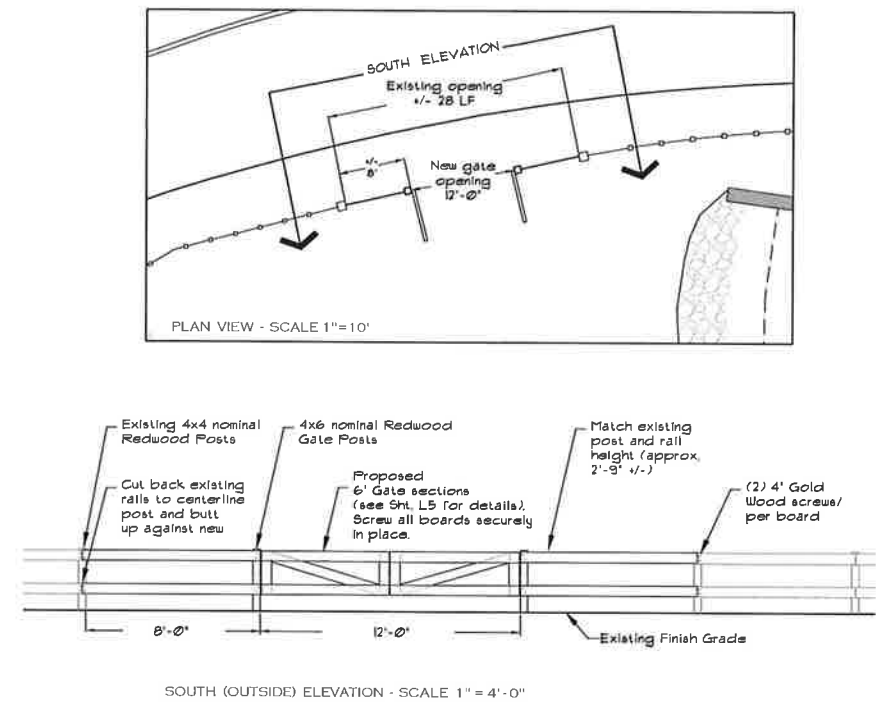
90% SUBMITTAL



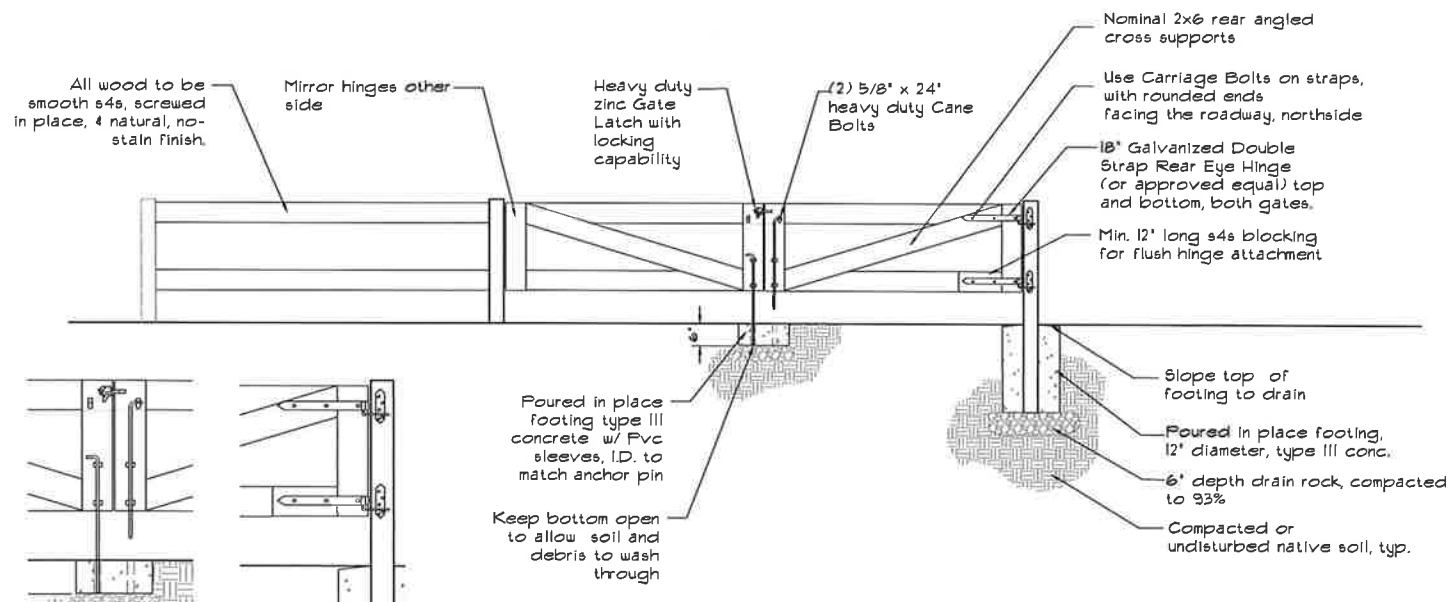
3 ELM TREE / WILLOW REMOVAL
SCALE 1"=20'



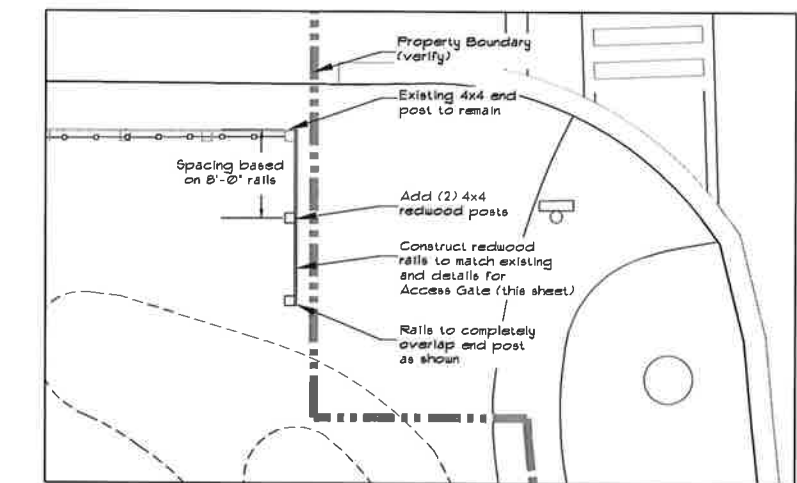
4 DRAINAGE IMPROVEMENT / CRUSHED ROCK
SCALE 1"=30'



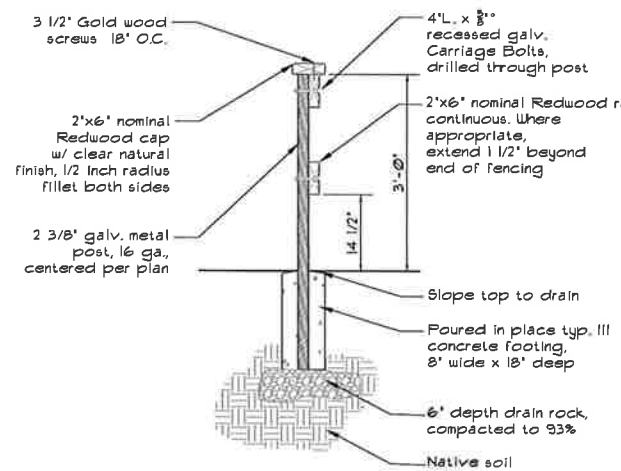
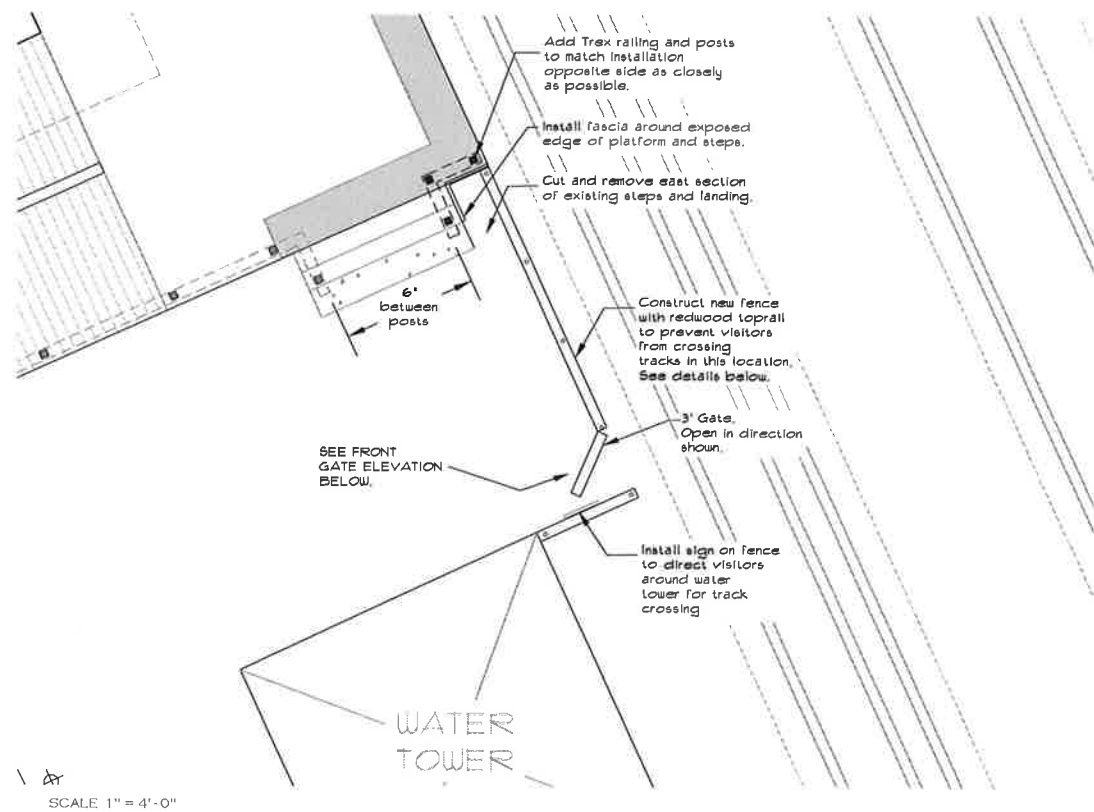
5 ACCESS GATE - PLAN VIEW & ELEVATION
SOUTH (OUTSIDE) ELEVATION - SCALE 1" = 4'-0"



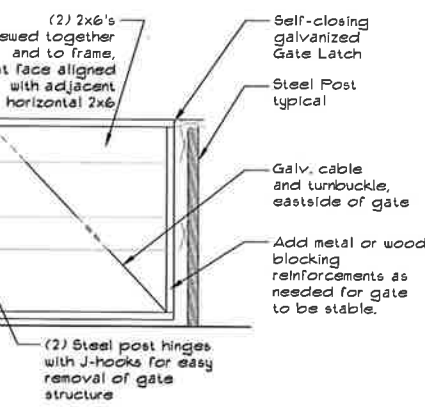
5 ACCESS GATE - CONSTRUCTION DETAILS
NORTH (INSIDE) ELEVATION - SCALE 1/2" = 1'-0"



6 ENTRY FENCE ADDITION
SCALE 1" = 8'-0"

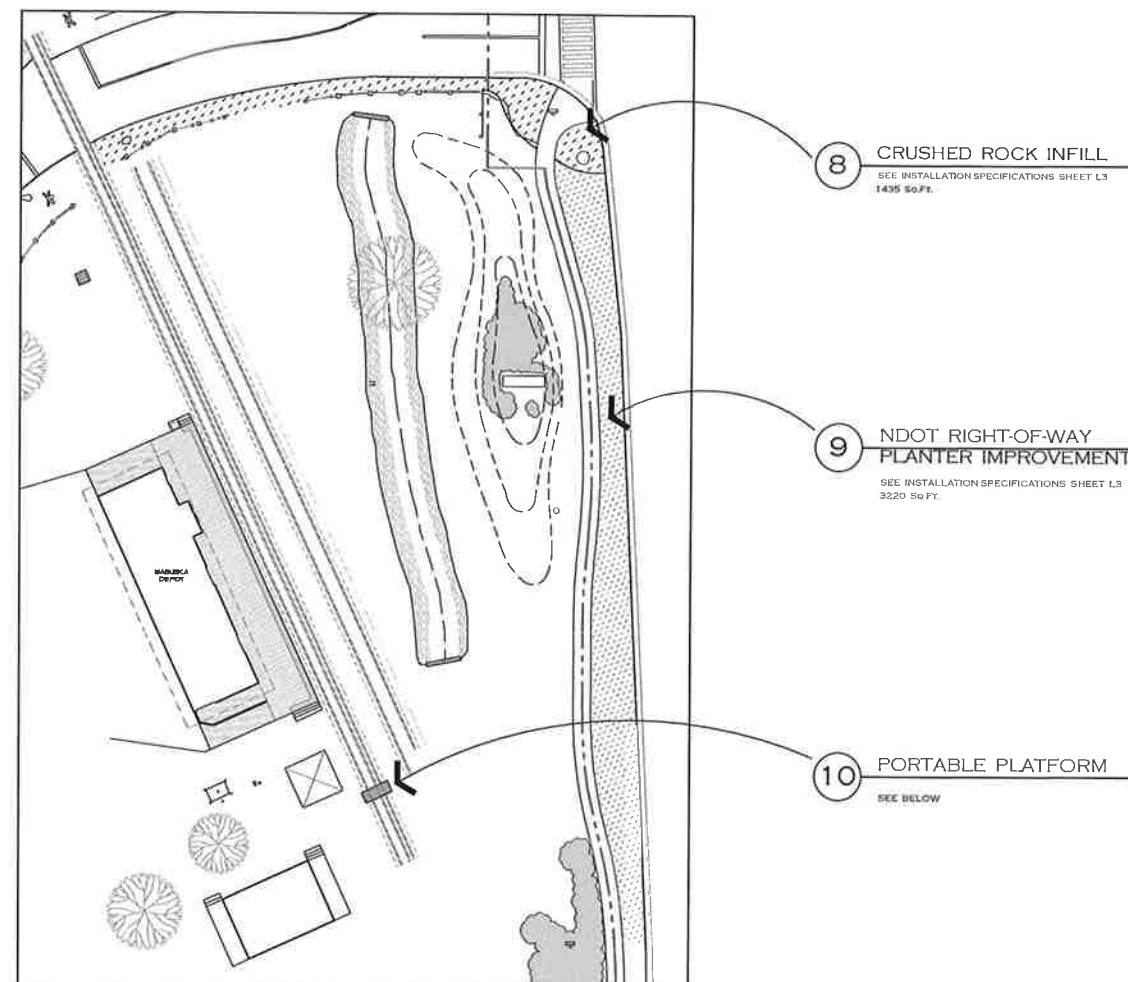


7 RESTRICTED ACCESS IMPROVEMENTS

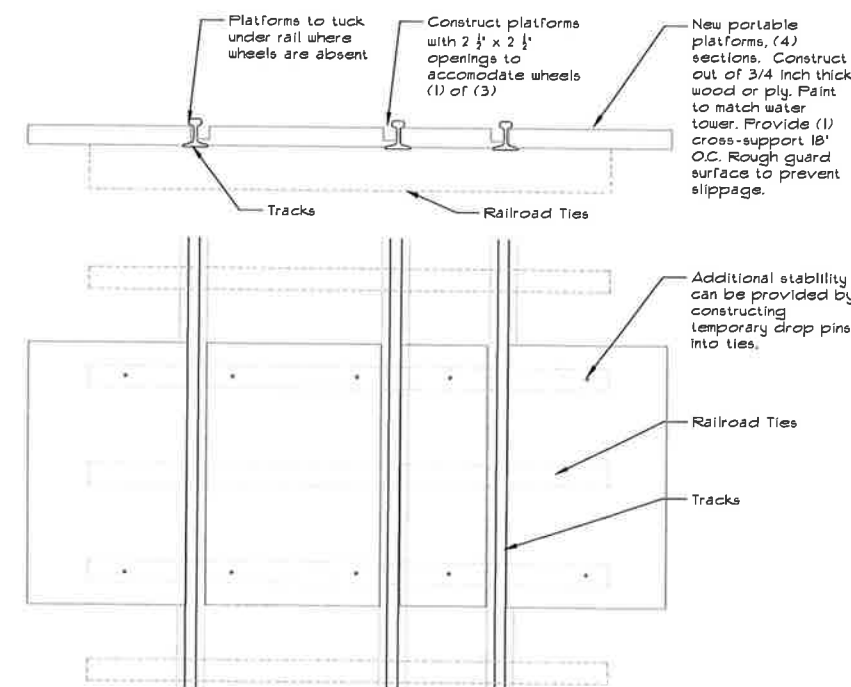


FRONT GATE ELEVATION

SCALE 3/4" = 1'-0"



SCALE 1" = 30'-0"



10 PORTABLE PLATFORM

SCALE 3/4" = 1'-0"

IMPROVEMENT DETAILS

REVISION	
1	
2	
3	
4	
5	

DATE	FEB. 3, 2014
SCALE	VARIES
DRAWN	SW
CHECKED	SW

SECTION 16000 – ELECTRICAL SPECIFICATIONS

- 1.0 GENERAL PROVISIONS
CONSIDER THE GENERAL CONTRACT REQUIREMENTS AS PART OF THE SPECIFICATIONS. WHERE REQUIREMENTS OF THESE SPECIFICATIONS DO NOT CONCOIDE WITH GENERAL CONTRACT REQUIREMENTS, THE STRICTER REQUIREMENTS WILL GOVERN.
- 2.0 WORK INCLUDED
PROVIDE ALL LABOR, MATERIALS, TOOLS, EQUIPMENT, APPARATUS, ETC. INCLUDING THOSE REASONABLY INFERRED, AS REQUIRED FOR A COMPLETE INSTALLATION OF ALL ELECTRICAL WORK SHOWN WITHIN THESE ELECTRICAL CONSTRUCTION DOCUMENTS, OR DETAILED IN THE ELECTRICAL SPECIFICATIONS. ALL ELECTRICAL WORK SHALL BE PERFORMED BY LICENSED ELECTRICIANS AND TECHNICIANS. REPAIRS AND REPLACEMENTS FOR DEFECTIVE EQUIPMENT OR MATERIALS AS COVERED BY WARRANTY/GUARANTEE SHALL BE CONSIDERED WITHIN THE SCOPE OF WORK AND SHALL BE PERFORMED AT NO ADDITIONAL COST (EXCEPTION: LIGHTING LAMPS).
- 3.0 RELATED WORK SPECIFIED ELSEWHERE
ELECTRICAL CONSTRUCTION DOCUMENTS AND GENERAL CONTRACT REQUIREMENTS SHALL BE CONSIDERED AS A WHOLE. ALL ELECTRICAL WORK SHALL ACCOMMODATE THE REQUIREMENTS OF THIS DIVISION TO ENSURE PROPER PERFORMANCE OF THE ELECTRICAL WORK REQUIRED.
- 4.0 SITE INVESTIGATION
ELECTRICAL CONTRACTOR SHALL THOROUGHLY INVESTIGATE ALL EXISTING CONDITIONS AT THE PROJECT SITE PRIOR TO SUBMITTING A BID. NO EXTRA PAYMENT WILL BE MADE FOR FAILURE TO OBSERVE EXISTING CONDITIONS EMDENT DURING THE SITE VISIT.
- 5.0 REGULATIONS AND STANDARDS
ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE LATEST ADOPTED EDITIONS OF: THE NATIONAL ELECTRICAL CODE (NEC); THE NATIONAL LIFE SAFETY CODE; THE INTERNATIONAL BUILDING CODE (IBC); OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION REGULATIONS (OSHA); AND THE WASHOE-RTC STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION (ORANGE BOOK). ADDITIONALLY, ALL STATE AND LOCAL CODES AND ORDINANCES SHALL BE UPHELD. CONTRACTOR SHALL NOT OMIT ANY COMPONENT OR PORTION OF THESE ELECTRICAL CONSTRUCTION DOCUMENTS WHICH MAY EXCEED THE LISTED STANDARDS. NO ELECTRICAL WORK DETAILED IN THE ELECTRICAL CONSTRUCTION DOCUMENTS OR ELECTRICAL SPECIFICATION SECTION SHALL BE INTERPRETED TO PERMIT WORK WHICH FAILS TO CONFORM TO THESE REQUIREMENTS. WHERE CONFLICTS OCCUR, HIGHER STANDARDS SHALL GOVERN. RULINGS AND INTERPRETATIONS BY GOVERNING AGENCIES SHALL BE CONSIDERED A PART OF THIS DIVISION IF COMMONLY IMPOSED UPON THE TRADE PRIOR TO SUBMISSION OF BID.
- 6.0 FEES
ELECTRICAL CONTRACTOR SHALL INCLUDE ALL FEES AND ROYALTIES FOR TESTING IN THE BID.
- 7.0 DRAWINGS AND SPECIFICATIONS
7.1 ELECTRICAL CONTRACTOR SHALL VERIFY THE EXACT LOCATIONS, ELEVATIONS, AND INSTALLATION REQUIREMENTS OF ALL ELECTRICAL EQUIPMENT PRIOR TO "ROUGH-IN". LOCATIONS SHALL BE VERIFIED FOR APPROPRIATE SPACE (LENGTH, WIDTH, AND HEIGHT) AND CLEARANCE PRIOR TO FABRICATING OR ORDERING ANY EQUIPMENT OR MATERIALS. ANY DISCREPANCIES SHALL BE REPORTED TO THIS ENGINEER IMMEDIATELY. FINAL CONNECTIONS TO EQUIPMENT SHALL BE MADE IN ACCORDANCE WITH MANUFACTURER'S APPROVED WIRING DIAGRAMS, DETAILS AND INSTRUCTIONS.
- 7.2 ELECTRICAL CONTRACTOR SHALL SUBMIT WRITTEN REQUEST-FOR-INFOEMATION (RFI) DOCUMENTS TO THIS ENGINEER PRIOR TO PERFORMING ANY WORK WHERE AMBIGUITY ARISES. ELECTRICAL CONTRACTOR SHALL NOT PERFORM ANY FIELD MODIFICATIONS OR DEVIATIONS FROM THE ELECTRICAL CONSTRUCTION DOCUMENTS AND/OR SPECIFICATIONS WITHOUT APPROVED WRITTEN RESPONSE FROM THIS ENGINEER THROUGH AN APPROPRIATELY SUBMITTED RFI.
- 7.3 ELECTRICAL CONTRACTOR SHALL PERFORM ELECTRICAL WORK PER MANUFACTURER'S DIRECTIONS WHERE SAID DETAILS ARE NOT SPECIFICALLY COVERED OR INDICATED BY THESE ELECTRICAL CONSTRUCTION DOCUMENTS AND ELECTRICAL SPECIFICATIONS. WHEN CONFLICT ARISES, SECTION 7.2 OF THIS SPECIFICATION SHALL DICTATE PERFORMANCE.
- 7.4 ALL SERVICES RENDERED BY THIS ENGINEER ARE PROFESSIONAL OPINIONS AND RECOMMENDATIONS, ONLY. UNDER NO CIRCUMSTANCE IS IT THE INTENT OF THIS ENGINEER TO DIRECTLY CONTROL THE PHYSICAL ACTIVITIES OF THE CONTRACTOR OR HIS EMPLOYEES.
- 8.0 MATERIALS
8.1 APPEARANCE AND FINISH OF MATERIALS SHALL BE OF THE HIGHEST STANDARDS. ALL MATERIALS AND EQUIPMENT SHALL BE NEW, OPERATING NORMALLY, AND SHALL BE OF HIGHEST AVAILABLE QUALITY. ANY DEFECTIVE MATERIALS SHALL BE IDENTIFIED AND REMOVED FROM SITE, IMMEDIATELY.
- 8.2 PROVIDE UNDERWRITERS LABORATORY (UL) LISTED MATERIALS AND EQUIPMENT WHEREVER STANDARDS FOR SAID ITEMS HAVE BEEN ESTABLISHED. UNLISTED DEVICES SHALL BE INSTALLED IN EQUIPMENT NORMALLY LISTED OR LABELED, AND SHALL CONFORM TO 'LISTED' STANDARDS AS FAR AS POSSIBLE.
- 9.0 CLEAN WORK
REGULARLY REMOVE DEBRIS FROM THE PROJECT SITE, AS REQUIRED, DURING CONSTRUCTION. CLEAN ALL RACEWAYS, FIXTURES, AND OTHER EQUIPMENT PRIOR TO FINAL ACCEPTANCE BY OWNER.
- 10.0 CLOSING IN OF UNINSPECTED WORK
ELECTRICAL CONTRACTOR SHALL NOT CLOSE IN OR CONCEAL ANY ELECTRICAL WORK PERFORMED PRIOR TO INSPECTION AND APPROVAL.
- 11.0 STANDARD FIELD TESTS
11.1 ELECTRICAL CONTRACTOR SHALL PERFORM POWER CONDUCTOR INSULATION TESTING.
- 11.2 CONDUCTOR INSULATION SHALL BE TESTED USING A 1000 VOLT DC INSULATION TESTER. TEST POWER CONDUCTORS ACCORDING TO TEST EQUIPMENT MANUFACTURES RECOMMENDED TESTING PROCEDURES. TEST POWER CONDUCTORS DISCONNECTED IN PLACE. MINIMUM RESISTANCE VALUES FOR BRANCH CIRCUIT CONDUCTORS SHALL BE 100 MEGOHMS. PROMPTLY REPORT ANY LOWER VALUE TO THE ENGINEER.
- 11.3 PERFORM POLARITY TESTS FOR ALL UTILIZATION EQUIPMENT.
- 11.4 PROVIDE WRITTEN TABULATED RESULTS OF CONDUCTOR INSULATION TESTING, TO THE ENGINEER, PROMPTLY AFTER TESTING AND PRIOR TO ENERGIZING OF CONDUCTORS.
- 11.5 ANY WORK WHICH FAILS TO MEET THE REQUIREMENTS OF THESE ELECTRICAL CONSTRUCTION DOCUMENTS, OR ANY OF THE ABOVE-LISTED TESTING SHALL BE CONSIDERED DEFECTIVE AND SHALL BE PROMPTLY CORRECTED OR REMOVED FROM SITE.
- 12.0 IDENTIFICATION
ELECTRICAL CONTRACTOR SHALL PROVIDE AND INSTALL IDENTIFICATION TAGS ON CONDUCTORS AT ALL PULLBOXES AND AT EQUIPMENT SERVED. IDENTIFICATION TAGS SHALL INDICATE DEVICE DESIGNATION, EQUIPMENT SERVED, VOLTS/PHASE/WIRE, AND PANEL / CIRCUIT NUMBER.
- 13.0 PENETRATIONS
PROVIDE AND INSTALL COMMERCIALY ACCEPTED METHODS FOR WATER-TIGHT PENETRATION OF EXTERIOR WALL. CONSULT OWNER REPRESENTATIVE FOR DETERMINATION OF EXACT PENETRATION LOCATIONS FOR EXTERIOR WALL.
- 14.0 EXCAVATION AND BACKFILL
14.1 PROVIDE ALL EXCAVATION, BACKFILL, TRENCHING, HORIZONTAL DIRECTIONAL BORING, COMPACTION, PATCHING ETC. REQUIRED FOR THE ELECTRICAL WORK DETAILED IN THE ELECTRICAL CONSTRUCTION DOCUMENTS AND THIS ELECTRICAL SPECIFICATION. ELECTRICAL WORK SHALL CONFORM TO REQUIREMENTS OF THE WASHOE-RTC STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION (ORANGE BOOK).
- 14.2 INSTALL RACEWAYS AND CONDUIT PROMPTLY. PERFORM BACKFILL PROMPTLY UPON INSPECTION AND APPROVAL.
- 14.3 UNDERGROUND ELECTRICAL WORK SHALL BE PERFORMED WITH CONSIDERATION FOR EXISTING SERVICES. ELECTRICAL CONTRACTOR SHALL PROMPTLY AND COMPLETELY REPAIR OR REPLACE ANY SYSTEMS DISRUPTED DURING ELECTRICAL INSTALLATION AND RETURN TO WORKING ORDER.
- 14.4 BACKFILL SHALL CONFORM TO REQUIREMENTS OF WASHOE-RTC STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION (ORANGE BOOK).

SECTION 16000 – ELECTRICAL SPECIFICATIONS (CONTINUED)

- 15.0 SUPPORTS AND FASTENINGS
ELECTRICAL CONTRACTOR SHALL PROVIDE AND INSTALL SUPPORTS AND HANGARS FOR ALL RACEWAYS, CABINETS, BOXES, BACK-BOXES, AND EQUIPMENT. SUPPORTS AND HANGARS SHALL BE SECURELY ATTACHED TO STRUCTURE USING: (a) STRUCTURAL CHANNEL, BEAM-CLAMPS, THREADED HANGAR RODS FOR STEEL STRUCTURE (b) EXPANSION BOLTS FOR SOLID MASONRY, (c) CONCRETE PRESET INSERTS FOR EXPANSION BOLTS FOR CONCRETE, (d) MACHINE SCREWS, OR BOLTS AND BEAM CLAMPS FOR METAL.
- 16.0 RECORD DRAWINGS
ELECTRICAL CONTRACTOR SHALL PROVIDE A NEAT AND COMPLETE SET OF "AS-BUILT" RECORD DRAWINGS TO THE OWNER WITHIN TEN (10) DAYS OF FINAL ACCEPTANCE OF WORK. RECORD DRAWINGS SHALL INDICATE ALL DEVIATIONS FROM THE ELECTRICAL CONSTRUCTION DOCUMENTS, LOCATIONS OF STUB-OUTS AND CONCEALED ITEMS (PER FIELD DIMENSIONS). RECORD DRAWINGS SHALL BE OF EQUAL TO OR HIGHER QUALITY THAN THESE ELECTRICAL CONSTRUCTION DOCUMENTS (INCLUDING SIZE, CLARITY AND MEDIUM). ALL MAINTENANCE AND OPERATING MANUALS AS WELL AS ANY ADDITIONAL INSTRUCTIONS, SHALL BE CONSIDERED A PART OF THE RECORD DRAWINGS.
- 17.0 ELECTRICAL SUBMITTALS
ELECTRICAL CONTRACTOR SHALL SUBMIT FOUR (4) COPIES OF FACTORY DATA SHEETS FOR ELECTRICAL MATERIALS PROPOSED FOR THIS PROJECT TO THE ENGINEER. ELECTRICAL CONTRACTOR SHALL OBTAIN WRITTEN APPROVAL FROM THE ENGINEER, FOR ALL PROPOSED DATA SHEETS. ELECTRICAL CONTRACTOR SHALL ORDER AND/OR RELEASE ANY MATERIALS PROMPTLY AFTER SUBMITTAL APPROVAL. NO SUBSTITUTIONS OR ALTERNATE METHODS SHALL BE ACCEPTED AS A RESULT OF UNTIMELY WORK.
- 18.0 OWNERSHIP OF DOCUMENTS
THESE ELECTRICAL CONSTRUCTION DOCUMENTS AND SPECIFICATIONS, AND ALL RELATED DOCUMENTS CONSTITUTE COPYWRITTEN MATERIALS OF JENSEN ENGINEERING, INC. ALL RIGHTS RESERVED. THESE MATERIALS SHALL REMAIN THE SOLE PROPERTY OF JENSEN ENGINEERING, INC. AND MAY NOT BE REPRODUCED, DISTRIBUTED, OR USED FOR ANY PURPOSE WITHOUT THE EXPLICIT WRITTEN CONSENT OF JENSEN ENGINEERING, INC.
- 19.0 ONGOING OPERATIONS
ELECTRICAL CONSTRUCTION WORK SHALL BE PERFORMED WITH DUE CONSIDERATION FOR ONGOING OPERATIONS AT THE PROJECT SITE AND SHALL MINIMIZE DISRUPTIONS IN EVERY RESPECT. PROVIDE AND INSTALL BARRICADES, DUST CONTAINMENT, NOISE ABATEMENT, ETC. TO ENSURE SAFETY OF NON-CONSTRUCTION PERSONNEL ON THE PROJECT SITE. ANY INTERRUPTIONS TO EXISTING SERVICES SHALL BE COORDINATED WITH THE OWNER PRIOR TO ENGAGEMENT. DISRUPTIONS SHALL BE TEMPORARY AND MUST BE SCHEDULED 2 WEEKS IN ADVANCE.
- 20.0 TEMPORARY POWER
ANY TEMPORARY CONSTRUCTION POWER SYSTEM SHALL COMPLY WITH THE REQUIREMENTS OF THE NEC, AS WELL AS OSHA. COSTS FOR TEMPORARY POWER EQUIPMENT ARE THE RESPONSIBILITY OF THE ELECTRICAL CONTRACTOR.
- 21.0 MATCH EXISTING
IDENTIFY ANY COMPONENTS WHERE DIRECTED TO MATCH EXISTING; PROVIDE AND INSTALL THE ITEM(S) EXACTLY. WHERE EXACT REPLACEMENT IS NOT AVAILABLE, NEW EQUIPMENT SHALL MATCH EXISTING EQUIPMENT IN FUNCTION AND APPEARANCE, AND MUST BE OF EQUAL OR GREATER QUALITY THAN EXISTING EQUIPMENT. WHERE SCOPE OF WORK OCCURS AT AN EXISTING FACILITY, ALL MATERIALS AND WORKMANSHIP SHALL BE OF GREATER QUALITY THAN EXISTING. TRANSITIONS FROM NEW WORK AREAS TO EXISTING AREAS SHALL BE SEAMLESS IN APPEARANCE AND FUNCTION.
- 22.0 CONDUCTORS
ALL CONDUCTORS SHALL CONFORM TO THE REQUIREMENTS STATED UNDER SECTION 5.0 OF THIS ELECTRICAL SPECIFICATION, AND UL LISTING STANDARDS. ALL CONDUCTORS SHALL BE COPPER, STANDARD AMERICAN WIRE GAUGE, SOFT-DRAWN, CONCEALED IN CONDUIT. CONDUCTOR INSULATION SHALL BE 600 VOLT, 90°C RATED TYPE THHN/THWN. CONDUCTORS SHALL BE CONTINUOUS FROM ORIGIN TO EQUIPMENT WITHOUT SPLICES, PER NEC. WHERE REQUIRED, SPLICES SHALL OCCUR IN SPLICE BOXES OR WIREWAYS PER PLAN. WIRE/CABLE SHALL BE HANDLED TO AVOID DAMAGE TO BOTH CONDUCTOR AND INSULATION. WIRE/CABLE SHALL BE DELIVERED IN STANDARD COILS OR REELS WITH SUITABLE PROTECTION FROM WEATHER AND DAMAGE DURING STORAGE, HANDLING, AND INSTALLATION. ELECTRICAL CONTRACTOR SHALL COLOR CODE CONDUCTORS CONSISTENTLY THROUGHOUT THE PROJECT ACCORDING TO THE FOLLOWING OUTLINE:
- | VOLTAGE | PHASE A | PHASE B | PHASE C | NEUTRAL | GROUND |
|----------|---------|---------|---------|---------|--------|
| 120/208V | BLACK | RED | BLUE | WHITE | GREEN |
- 23.0 CONDUIT SYSTEMS
23.1 CONDUIT ROUTES SHOWN ON THESE DOCUMENTS SHALL BE FOLLOWED A CLOSELY AS POSSIBLE.
- 23.2 TYPE RMC (GRS) CONDUIT SHALL BE USED FOR ALL EXPOSED LOCATIONS.
- 23.3 TYPE RNC (PVC SCHEDULE 40) CONDUIT SHALL BE USED FOR ALL UNDERGROUND AND UNDER CONCRETE SLAB APPLICATIONS. ABOVE GRADE AND EXPOSED RISERS SHALL BE TYPE RMC. UNDERGROUND CONDUITS SHALL BE BURIED IN SAND ENVELOPE, MINIMUM OF 24" (TWO FEET) BELOW FINISHED GRADE. PROVIDE AND INSTALL GREEN INSULATED GROUNDING CONDUCTORS IN ALL NON-METALLIC CONDUITS.
- 23.4 TYPE FMC CONDUIT (INCLUDING TYPE 'MC' CABLE) IS NOT PERMITTED.
- 23.5 TYPE LFMC CONDUIT IS NOT PERMITTED.
- 23.6 TYPE LFNC CONDUIT IS NOT PERMITTED.
- 23.7 TYPE ENT CONDUIT IS NOT PERMITTED.
- 23.8 TYPE NM & NM-C CABLE IS NOT PERMITTED.
- 23.9 PROVIDE AND INSTALL 200# PULL STRING IN ALL EMPTY OR SPARE CONDUITS.
- 24.0 TERMINATIONS AND SPLICES
CONNECTORS, SPLICES, AND LUGS SHALL BE LISTED FOR THE APPLICATION. CHECK ALL BUS CONNECTIONS FOR PROPER CONTACT PRESSURE. COMPLETE MECHANICAL LUG CONNECTIONS USING A CALIBRATED TORQUE WRENCH (OR SCREWDRIVER) ACCORDING TO LUG MANUFACTURES TIGHTENING RECOMMENDATIONS.
- 25.0 VANDAL PROTECTION
ALL ELECTRICAL WORK THAT IS ACCESSIBLE TO THE PUBLIC SHALL BE TAMPER-PROOF AND VANDAL RESISTANT. ALL EQUIPMENT WITH OPERABLE-DOOR ENCLOSURES SHALL BE LOCKING TYPE, OR PAD-LOCKING.
- 26.0 GROUNDING AND BONDING
PROVIDE AND INSTALL GROUNDING FOR ALL EQUIPMENT IN ACCORDANCE WITH NEC ARTICLE 250.
- 27.0 FINAL OBSERVATION
UPON ACCEPTANCE OF OPERATION AND MAINTENANCE INSTRUCTIONS AND RECORD DRAWINGS, THIS ENGINEER SHALL MAKE A FINAL OBSERVATION OF THE PROJECT AND GENERATE A PUNCH-LIST OF ANY UNACCEPTABLE ITEMS. FINAL PROJECT ACCEPTANCE SHALL NOT BE CONSIDERED UNTIL ALL ITEMS ON THE PUNCH-LIST HAVE BEEN CORRECTED.
- 28.0 PRELIMINARY OPERATION
IF THE OWNER DEMANDS THAT ANY PORTION OF THE ELECTRICAL SYSTEM BE OPERATED PRIOR TO FINAL COMPLETION AND ACCEPTANCE OF THE WORK, THE CONTRACTOR SHALL CONSENT, AND SUCH OPERATION SHALL BE PERFORMED UNDER SUPERVISION AND DIRECTION OF THE CONTRACTOR. ALL OPERATING EXPENSE THEREOF SHALL BE PAID BY THE OWNER. PRELIMINARY OPERATION AND PAYMENT THEREOF SHALL NOT BE CONSTRUED AS ACCEPTANCE OF WORK.
- 29.0 WARRANTY/GUARANTEE
ELECTRICAL CONTRACTOR SHALL PROVIDE A GUARANTEE OF THE ELECTRICAL WORK TO BE FREE FROM DEFECTS IN MATERIALS AND WORKMANSHIP FOR A PERIOD OF ONE YEAR FROM DATE OF FINAL ACCEPTANCE. LIGHTING LAMPS ARE EXEMPT FROM THIS REQUIREMENT, HOWEVER, LAMPS SHALL BE NEW AND IN PERFECT OPERATING CONDITION AT THE TIME OF FINAL ACCEPTANCE.

ELECTRICAL SYMBOLS AND NOMENCLATURE

SYMBOL	DESCRIPTION
	DOUBLE-OUTLET 'DUPLEX' RECEPTACLE
	THREE-PHASE (3Ø) RECEPTACLE (CONFIGURATION AS NOTED)
	SPECIAL RECEPTACLE (VOLTAGE/CONFIGURATION AS NOTED)
	JUNCTION BOX
	SAFETY DISCONNECT SWITCH (SIZE/VOLTAGE/CONFIGURATION AS NOTED)
	ABOVE GRADE CONDUITS AND WIRING AS NOTED
	UNDERGROUND CONDUITS AND WIRING AS NOTED
	SHEET NOTE INDICATOR
	REVISION 'DELTA' NOTE INDICATOR
48'	48' MOUNTING HEIGHT AFF. (ACTUAL HEIGHT AS NOTED)
AFF.	ABOVE FINISHED FLOOR
AFG.	ABOVE FINISHED GRADE
A.T.S.	AUTOMATIC TRANSFER SWITCH
C	CONDUIT
CB	CIRCUIT BREAKER
(E)	EXISTING
F.B.O.	FURNISHED BY OTHERS
GFI	GROUND FAULT INTERRUPTING TYPE
LTG	LIGHTING
LV	LOW VOLTAGE
MCB	MAIN CIRCUIT BREAKER
MSB'	MAIN SWITCH BOARD
(N)	NEW
TYP	TYPICAL
UG	UNDER GROUND
WP	WEATHER PROOF
TRANS	TRANSFORMER

Panel Schedule by Jensen Engineering Inc.

PANEL 1 (E) 100 / 240 VOLT 3 PHASE 4 WIRE

100 AMPERE MAIN L.O. 200 AMPERE BUS

LOCATION (E) AT DEPO WEST WALL MOUNTING SURFACE INTERRUPTING (E) A.I.C.

BUS	LOAD			LOAD DESCRIPTION	CIRCUIT BREAKER	BUS	CIRCUIT BREAKER	LOAD DESCRIPTION	LOAD		
	LINE A	LINE B	LINE C						LINE A	LINE B	LINE C
1											
2											
3					1	20	5	1			2
4					1	20	5	1			4
5											
6											
7				BLANK	1	15	5	1			6
8											
9					1	20	20	1			8
10											
11				BLANK	1	20	20	1			10
12											
13											
14				(N) SIGN LIGHTING (1)	1	20	50	7			12
15											
16				BLANK	1	20	20	1			14
17											
18					1	20	20	1			16
19											
20					1	20	20	1			18
21											
22				BLANK	1	20	20	1			20
23											
24					1	100	20	1			22
25											
26					1	100	20	1			24
27											
28				BLANK	1	100	20	1			26
29											
30					2	100	15	1			28
31											
32											
33											
34											
35				BLANK	1	20	20	1			30
36											
37											
38					1	20	20	1			32
39											
40											
41											
42											

SUB TOTALS

BUS MATERIAL: NEW: N EXISTING: Y

LINE TOTALS

LINE AMPS

TOTAL KVA LOAD

KEYED NOTES: (THIS SHEET ONLY)

- 1 PROVIDE AND INSTALL NEW 20A / 1-POLE BREAKER IN EXISTING PANEL AT POSITION 15, AND CONNECT TO POWER NEW SIGN.



90% REVIEW SET



1/21/14

NEVADA STATE RAILROAD MUSEUM
GATEWAY IMPROVEMENT PROJECT
2180 SOUTH CARSON STREET
FEDERAL PROJECT # TAP-0529(002) - PROJECT # 73802

REVISION

- 1
2
3
4
5

DATE

1-27-14

SCALE

AS SHOWN

DRAWN

SCB

CHECKED

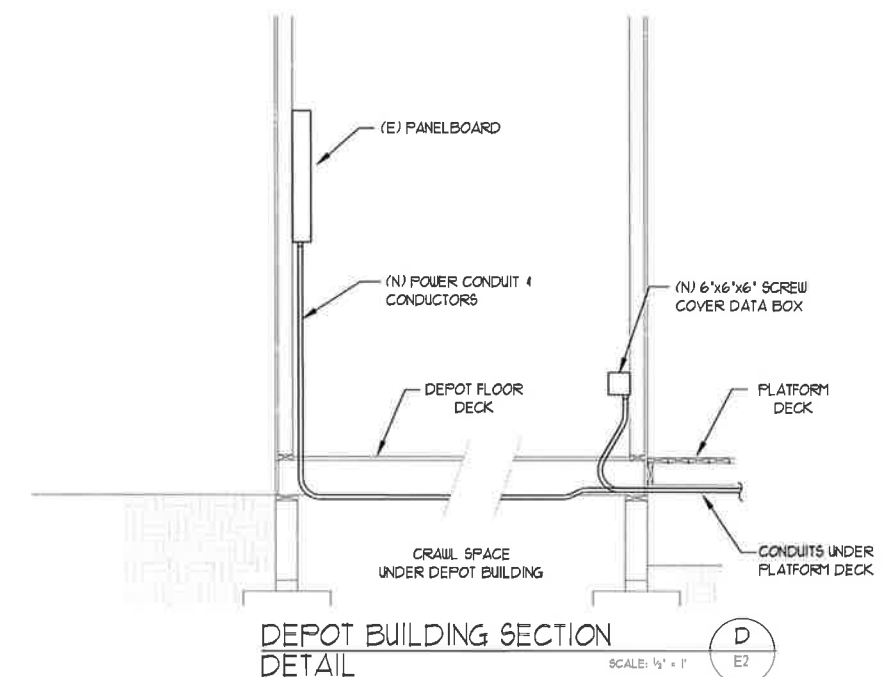
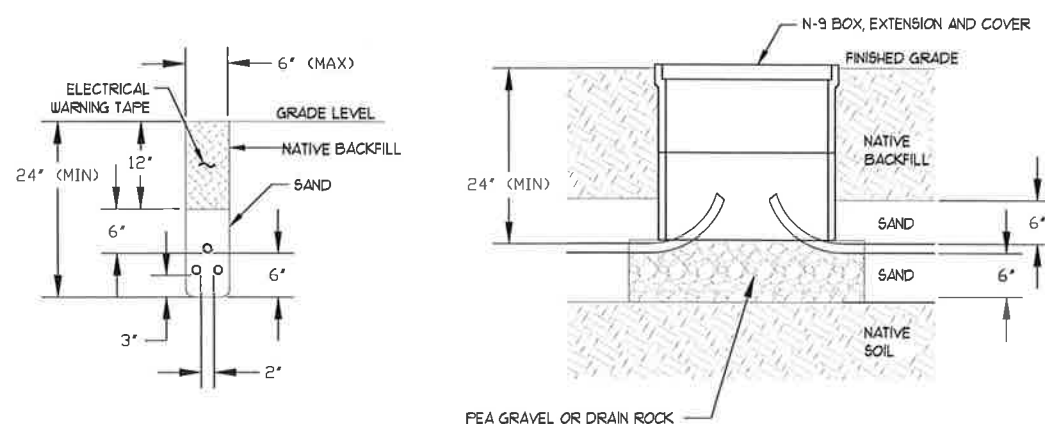
GGJ

DRAWING TITLE
SPECS & SCHEDULES

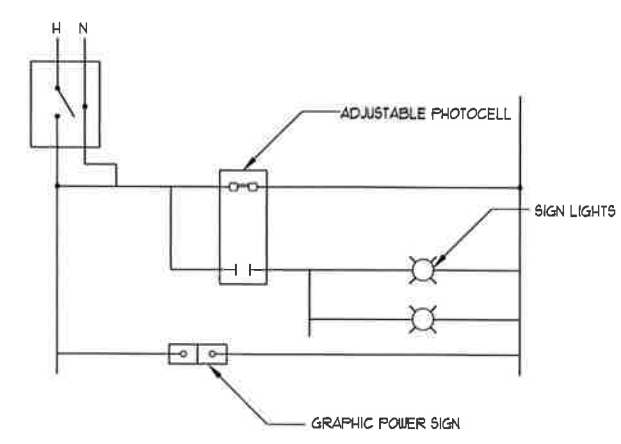
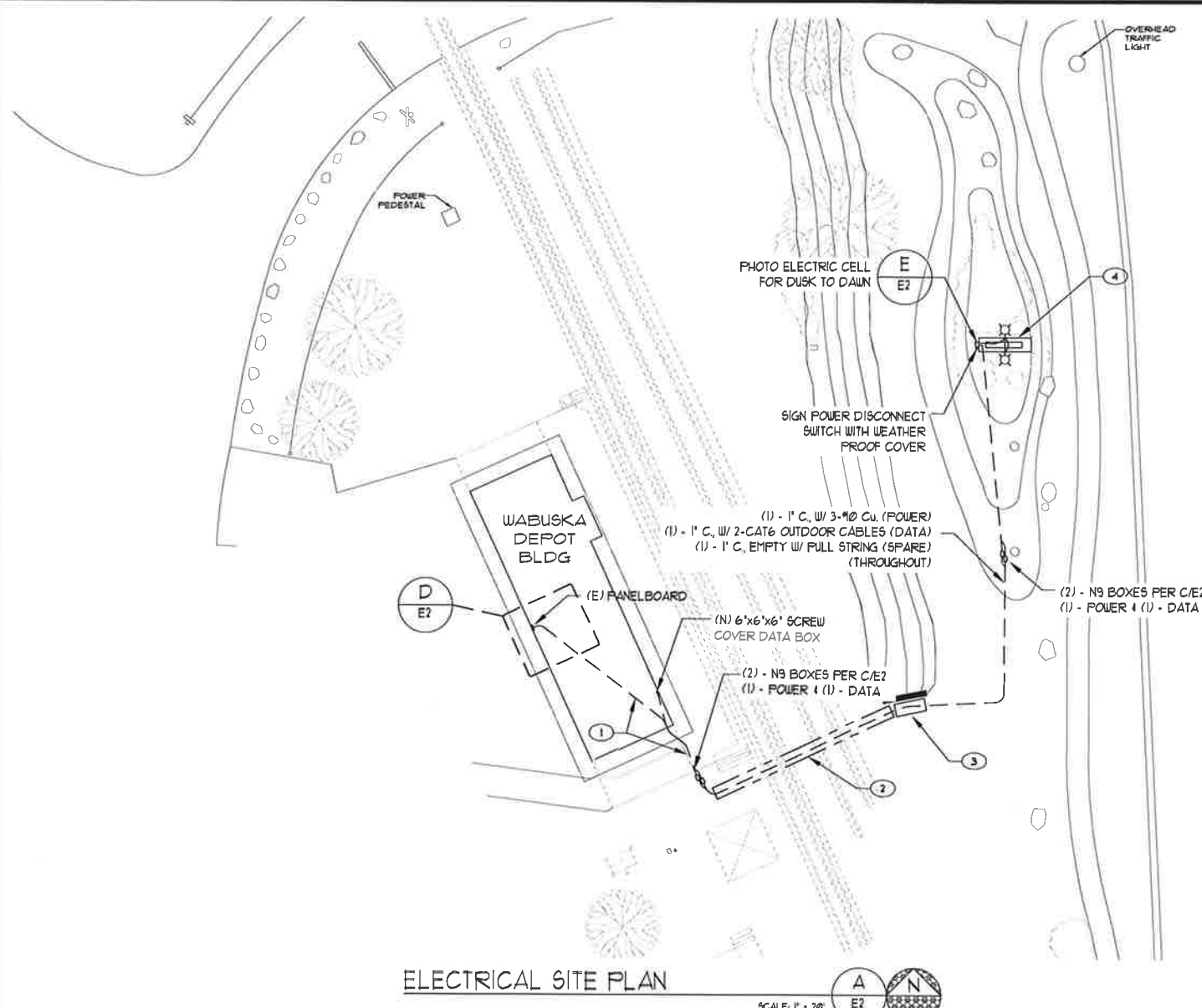
SHEET:

E1

OF 1 SHEETS 2



- KEYED NOTES: (THIS SHEET ONLY)**
1. RUN CONDUITS THROUGH CRAWL SPACE UNDER DEPOT BUILDING AND OUT UNDER THE PLATFORM DECK. TRANSITION CONDUITS TO UNDERGROUND UNDER THE PLATFORM DECK WITH OUT EXPOSING TO PUBLIC VIEW. REMOVE AND REINSTALL TREX DECK PLANKING AS NECESSARY TO INSTALL CONDUITS. SEE C/E2.
 2. INSTALL CONDUITS UNDER RAILROAD TRACKS USING HORIZONTAL DIRECTIONAL DRILLING. BORING UNDER TRACKS SHALL BE A MINIMUM OF 5' BELOW FINISHED GRADE AND SHALL EXTEND A MINIMUM OF 15' ON EACH SIDE OF NEAREST TRACK RAIL.
 3. PROVIDE CONCRETE ENCASUREMENT OF CONDUITS WHERE THE CONDUITS CROSS THE EXISTING CULVERT AND RISE TO LESS THAN 24" BELOW FINISHED GRADE.
 4. EXISTING MUSEUM SIGN TO BE REMOVED AND REPLACED WITH NEW PROGRAMMABLE ELECTRIC SIGN. CONNECT NEW POWER AND DATA SYSTEMS TO NEW SIGN AND CONNECT COMPLETE PER SIGN MANUFACTURERS SHOP DRAWINGS. PROVIDE AND INSTALL 20A RATED SWITCH FOR POWER DISCONNECTING MEANS.
 5. TRENCH FORM RAIL ROAD TRACKS TO SIGN SHALL BE ACCOMPLISHED USING A WALK-BEHIND SLOT TYPE TRENCHER TO MINIMIZE DAMAGE TO SURROUNDING LANDSCAPE.



JENSEN ENGINEERING INC.
 Electrical Engineers
 3830 S. 1200 E. SUITE 200
 RENO, NEVADA 89502-3348
 PH: (775) 822-2218
 EMAIL: jensen@jenseng.net
 WEB: www.jenseng.com

90% REVIEW SET

NEVADA STATE RAILROAD MUSEUM GATEWAY IMPROVEMENT PROJECT
2180 SOUTH CARSON STREET
 FEDERAL PROJECT # TAP-0529(002) - PROJECT # 73802

REVISION

1	
2	
3	
4	
5	

DATE 1-27-14
SCALE AS SHOWN
DRAWN SCB
CHECKED GGJ

DRAWING TITLE SITE PLAN & DETAILS
SHEET: **E2**
 OF 2 SHEETS 2