

## STAFF REPORT FOR PLANNING COMMISSION MEETING OF JUNE 26, 2019

FILE NO: SUP-19-083

AGENDA ITEM: G-3

STAFF CONTACT: Hope Sullivan, Planning Manager

**AGENDA TITLE:** For Possible Action: Discussion and possible action regarding a Special Use Permit for Outdoor Storage on property zoned General Commercial and Public Regional, located on the west side of Airport Road, south of Butti Way, APN 010-041-76. (Hope Sullivan, hsullivan@carson.org).

**STAFF SUMMARY:** The applicant is seeking to construct a boat and RV storage facility on a 7.28 acre parcel in the Lompa Ranch North Specific Plan Area. The property has two zoning districts. Given that the use is a conditional use in one of the two zoning districts, a Special Use Permit is required. The Planning Commission is authorized to approve a Special Use Permit.

**RECOMMENDED MOTION:** "I move to approve SUP-19-083 based on findings and subject to conditions of approval contained in the staff report."

### VICINITY MAP:



### RECOMMENDED CONDITIONS OF APPROVAL:

1. The applicant must sign and return the Notice of Decision for conditions for approval within 10 days of receipt of notification. If the Notice of Decision is not signed and returned within 10 days, then the item may be rescheduled for the next Planning Commission meeting for

further consideration.

2. All development shall be substantially in accordance with the development plans approved with this application, except as otherwise modified by these conditions of approval.
3. All on- and off-site improvements shall conform to City standards and requirements.
4. The use for which this permit is approved shall commence within 12 months of the date of final approval. A single, one year extension of time may be requested in writing to the Planning Division thirty days prior to the one year expiration date. Should this permit not be initiated (obtain a Building Permit) within one year and no extension granted, the permit shall become null and void.
5. All requirements of the Lompa Ranch North Specific Plan development agreement must be met.
6. The LP dispenser should be shown on the building permit and tank size must be indicated. Installation must be approved by the NV LPG Board.
7. A site specific geotechnical investigation must be performed as recommended by the feasibility geotechnical investigation, and construction recommendations be incorporated into the design.
8. The multi-use path must be extended along the frontage as shown.
9. The project must meet the Carson City Municipal Code, Development Standards, and Standard Detail requirements including:
  - The project must balance flood zone cut and fill quantities.
  - The project must provide funds to the City to process a CLOMR.
  - The water main must be extended along the frontage as shown.
10. The project site is adjacent to a future three acre dog park associated with the proposed Lompa Ranch East development. The applicant will be required to install the screen wall upslope from the park's south property line and adjacent to the storage facility. The landscape design for the screening buffer between the proposed park and project site will be reviewed and approved by the Parks, Recreation & Open Space Department.
11. The Unified Pathway Master Plan identifies Airport Road as an on-street bicycle facility. The applicant will be required to protect the existing bike lanes during the project's construction and repair lanes/restripe the pavement markings at their expense, if damage occurs. Any repairs and restriping to the bike lanes has to be approved by Development Engineering.
12. Design all project driveway approaches that cross the 10' pedestrian multi-use path along Airport Road to meet ADA standards.
13. The applicant will be required to maintain all common landscape and open space areas on the project site in perpetuity, including any landscaping in the road right-of-way.
14. Incorporate "best management practices" into the applicant's construction documents and specifications to reduce the spread of noxious weeds. The Parks, Recreation & Open

Space Department is willing to assist the applicant with this aspect of their project.

15. The applicant will be required to remove any graffiti on the perimeter screening wall, per Carson City Municipal Code.
16. A detailed landscaping plan is required at the time of site improvement permit. This plan shall demonstrate that disturbed areas are either landscaped or otherwise improved, shall utilize drought tolerant plants, shall incorporate a buried irrigation system, and shall incorporate a minimum 15 foot wide landscape strip along Airport Road planted with 1 tree per 25 lineal feet of street frontage, and six shrubs per tree.
17. A detailed lighting plan is required at the time of site improvement permit and shall include a photometric survey demonstrating even lighting throughout the site, with no spillover lighting.
18. Proposed signage requires a sign permit and shall meet the standards of Section 2.1.6 of the Lompa Ranch North Specific Plan.
19. The site must be fully enclosed with six foot solid fencing consistent with fencing proposed.
20. Site improvement plans shall show the location of the wash area and dumpster locations. These areas must be inside the six foot fence.
21. Site improvement plans must include a detail of the dumpster enclosures and placement consistent with Section 2.1.8 of the Lompa Ranch North Specific Plan.

**LEGAL REQUIREMENTS:** CCMC 18.02.080 (Special Use Permits), 18.04.135 General Commercial (GC); 18.04.185 Public Regional (PR); 18.04.020 Determination of Districts

**MASTER PLAN DESIGNATION:** Mixed Use Residential and Mixed Use Employment, Lompa Ranch North Specific Plan

**PRESENT ZONING:** General Commercial and Public Regional

**KEY ISSUES:** Does the application meet the required findings for approval of a Special Use Permit? Is the proposed use compatible with surrounding properties?

**SURROUNDING ZONING AND LAND USE INFORMATION:**

NORTH: Single Family 6,000 / vacant (tentative map approved)

EAST: Public Regional / Animal Facility

SOUTH: Agriculture / Drainage Facility

WEST: Agriculture / Drainage Facility

**ENVIRONMENTAL INFORMATION:**

1. FLOOD ZONE: Zone AE (100 year flood plain), XS (Between 100 year and 500 year flood plain), and X (areas of minimal flooding)

2. EARTHQUAKE FAULT: Severe, Zone I

### 3. SLOPE/DRAINAGE: Flat

#### **SITE DEVELOPMENT INFORMATION:**

LOT SIZE: 7.28 acres

EXISTING LAND USE: Vacant

VARIANCES REQUESTED: None

#### **DISCUSSION:**

A Special Use Permit is required for the following reason:

- According to CCMC Title 18.04.020.6.d where a parcel of land is divided by a zoning district boundary, the following shall apply: "To utilize the entire parcel with a use that is only allowed in 1 of the 2 zoning districts, a zoning map amendment or special use permit is required for that portion of the property not zoned for the use."

The vacant property is split zoned General Commercial and Public Regional. Outside storage is allowed in Public Regional, but not allowed in General Commercial.

The applicant is proposing a covered RV and boat storage facility for approximately 275 vehicles. The facility will include a 1000 square foot sales office on site that will include a small retail component with items related to RVs and boats. The site will also have a dumping station, a RV / boat wash station, and a propane refill area. No mechanic services will be on site, and customers will be prohibited from repairing or maintaining their vehicles on site.

Hours of operation are proposed to be 6:00 AM to 10:00 PM, with the office open from 8:00 AM to 6:00 PM daily. The site will entirely fenced with solid fencing and secured with a gate. Customers will have a gate code.

Per the provisions of 18.02.080, the Planning Commission has the authority to approve a Special Use Permit upon making each of the seven required findings in the affirmative.

**PUBLIC COMMENTS:** Public notices were mailed to 32 property owners within 1000 feet of the subject property on June 7, 2019. As of the writing of this report, no comments have been received. Any comments that are received after this report is completed will be submitted to the Planning Commission prior to or at the meeting on June 26, 2019, depending on the date of submittal of the comments to the Planning Division.

**OTHER CITY DEPARTMENTS OR OUTSIDE AGENCY COMMENTS:** The following comments were received by various city departments. Recommendations that are not code requirements have been incorporated into the recommended conditions of approval, where applicable.

#### **Fire Department:**

1. Project must comply with the currently adopted International Fire Code and northern Nevada fire code amendments
2. An additional fire hydrant(s) is required on the interior of the project to meet hydrant spacing per 2018 IFC Appendix C. The additional hydrant added on Airport Rd at the south east corner of the parcel does not comply.

#### Notes:

- Depending on final design, the storage structures may require fire sprinklers.

- The LP dispenser should be shown on the building permit and tank size must be indicated. Installation must be approved by the NV LPG Board.
- Electric gate requires a Knox key switch on the street side pedestal and a pavement loop or Knox key switch on the egress side pedestal if present.
- Project is subject to the fire protection fee outlined in the Lompa Ranch North Specific Plan, section 3.8

## **Engineering Department**

The Engineering Division has no preference or objection to the special use request provided that the following conditions are met:

- A site specific geotechnical investigation must be performed as recommended by the feasibility geotechnical investigation, and construction recommendations be incorporated into the design.
- The multi-use path must be extended along the frontage as shown.
- The project must meet the Carson City Municipal Code, Development Standards, and Standard Detail requirements including:
  - The project must balance flood zone cut and fill quantities.
  - The project must provide funds to the City to process a CLOMR.
  - The water main must be extended along the frontage as shown.

## **ENGINEERING DISCUSSION:**

The Engineering Division has reviewed the application within our areas of purview relative to adopted standards and practices and to the provisions of CCMC 18.02.080, Conditional Uses. The Engineering Division offers the following discussion:

### **CCMC 18.02.080 (5a) - Master Plan**

The request is not in conflict with any Engineering Master Plans.   

### **CCMC 18.02.080 (5b) – Use, Peaceful Enjoyment, Economic Value, Compatibility**

Development Engineering has no comment on this finding.

### **CCMC 18.02.080 (5c) - Traffic/Pedestrians**

The existing infrastructure and proposed drive isles are sufficient to provide safe access and circulation if conditions are met.

### **CCMC 18.02.080 (5d) - Public Services**

The existing sewer, water, and storm drain infrastructure are sufficient to provide service to the project.

### **CCMC 18.02.080 (5e) – Title 18 Standards**

Development Engineering has no comment on this finding.

### **CCMC 18.02.080 (5f) – Public health, Safety, Convenience, and Welfare**

The project meets engineering standards for health and safety.

### **CCMC 18.02.080 (5g) – Material Damage or Prejudice to Other Property**

Development Engineering has no comment on this finding.

### **CCMC 18.02.080 (5h) – Adequate Information**

The plans and reports provided were adequate for this analysis.

**Parks and Recreation:**

The Parks, Recreation & Open Space Department has the following comments on the above referenced Special Use Permit application.

1. The project site is adjacent to a future three acre dog park associated with the proposed Lompa Ranch East development. The applicant will be required to install the screen wall upslope from the park's south property line and adjacent to the storage facility. The landscape design for the screening buffer between the proposed park and project site will be reviewed and approved by the Parks, Recreation & Open Space Department.
2. The Unified Pathway Master Plan identifies Airport Road as an on-street bicycle facility. The applicant will be required to protect the existing bike lanes during the project's construction and repair lanes/restripe the pavement markings at their expense, if damage occurs. Any repairs and restriping to the bike lanes has to be approved by Development Engineering.
3. Design all project driveway approaches that cross the 10' pedestrian multi-use path along Airport Road to meet ADA standards.
4. The applicant will be required to maintain all common landscape and open space areas on the project site in perpetuity, including any landscaping in the road right-of-way.
5. Incorporate "best management practices" into the applicant's construction documents and specifications to reduce the spread of noxious weeds. The Parks, Recreation & Open Space Department is willing to assist the applicant with this aspect of their project.
6. The applicant will be required to remove any graffiti on the perimeter screening wall, per Carson City Municipal Code.

**FINDINGS:** Staff's recommendation is based upon the findings as required by CCMC Section 18.02.080 (Special Use Permits) enumerated below and substantiated in the public record for the project.

***1. Will be consistent with the objectives of the Master Plan elements.***

The subject property is part of the Lompa Ranch North Specific Plan, and is designated as Mixed Use Residential. This is the only non-residential component in the portion of the Lompa Ranch North Specific Plan east of the freeway.

Section 2.1 of the Specific Plan provides design standards and guidelines for the commercial uses, as follows.

**2.1 Commercial Uses**

**2.1.1 Commercial Site Planning Standards**

- a) *Building placement and orientation shall be designed to create visual interest along public*

streets. *Multiple buildings in a single project shall demonstrate a positive functional relationship to one another.*

The one building proposed will be along Airport Road. In addition, a six foot solid fence utilizing vertical board and dense landscaping will be located along Airport Road. The parking to serve the building is to the side of the building as opposed to between the street and the building. Staff finds this design to be consistent with the Master Plan.

*b) To the extent possible, buildings located within a single project shall be clustered. Plazas and pedestrian areas shall also be an important element in the design of clustered buildings. When clustering is impractical, a visual link should be established between buildings through the use of architectural features, landscaping, etc.*

The site only has one building.

*c) For general commercial uses, a minimum of 15 percent of the building area should be located at or near the front setback line. This minimizes large, continuous areas of parking and encourages active streetscapes.*

The only building is located at the front setback line.

*d) Buildings shall be oriented so that public access or windows face adjoining streets.*

The building is oriented so that windows and a stone façade face the street.

*e) Plazas or common areas within a project shall be located near building entrances or areas of high pedestrian traffic to ensure their use.*

Given the proposed use, a plaza is not recommended as it will not be a use where people congregate or linger.

*f) To the extent possible, areas between buildings shall be utilized for plazas, outdoor seating, or landscape features in order to eliminate “dead zones” of underutilized space.*

There is only one building.

*g) Bicycle racks shall be provided within all commercial centers.*

The proposed use is not a commercial center.

### **2.1.2 Commercial Grading and Drainage**

a) *Design of commercial uses shall be sensitive to the natural terrain, and structures should be located to minimize necessary grading and preserve natural site features such as drainageways, wetlands, etc. Grading of commercial sites should blend with the natural topography of the site.*

The subject property has drainage facilities on two sides. The site will maintain existing drainage patterns sloping from north to south where flow will be collected and treated in a detention pond before being discharged to the detention facilities to the south.

b) *Grading within commercial areas shall be designed to complement the architectural and landscape design character of the center and surrounding area. Grading techniques can be used to screen parking and service areas, reduce the perception of height and mass on larger buildings, and provide reasonable transitions between uses.*

The site is being graded in a manner to allow for additional stormwater detention to the south.

c) *Graded slopes should properly transition to existing natural terrain at project borders.*

The proposed grading is intended to allow for additional stormwater detention.

d) *Man-made slopes shall not exceed an average of 3:1 slope and turf areas shall not exceed an average 4:1 slope.*

Slopes will be 6:1 adjacent to the drainage basins in order to provide for additional stormwater detention.

e) *Areas disturbed by grading activities shall be revegetated prior to the issuance of a certificate of occupancy. If climatic conditions or other circumstances prevent planting at the time of occupancy, a bond shall be provided for landscaping during the subsequent growing season. Drought tolerant plant species shall be utilized to help minimize erosion.*

A detailed landscaping plan will be obtained at the time of site improvement permit to ensure disturbed areas are either landscaped or otherwise improved.

f) *New commercial developments must include a final hydrology report to be reviewed and approved by the Carson City Engineering Department prior to the issuance of a building permit.*

The applicant has submitted a drainage letter prepared by a licensed engineer. A detailed drainage report will be required at the time of site improvement permit review.

g) *An erosion control plan shall be included with each grading permit.*

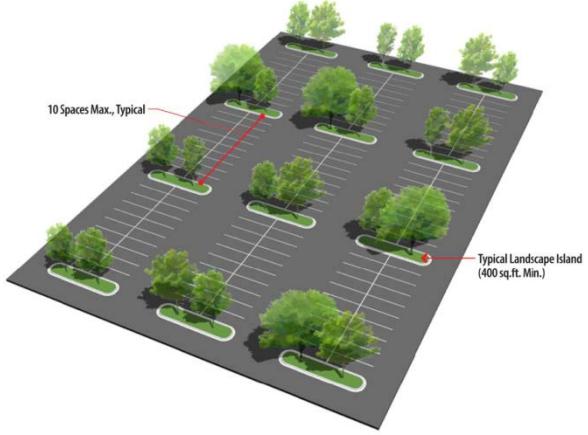
A detailed erosion control plan will be required at the time of site improvement permit review.

### 2.1.3 Commercial Parking Lots

a) A minimum of 10 feet of landscaping shall be provided between parking lots and the public streets.

A ten foot landscape area is provided between the parking lot and the road.

b) A minimum 400 square foot interior planter shall be provided at the end of parking aisles (refer to example to the right). Planters shall include a minimum of one deciduous tree (min. two inch caliper) – see example to right.

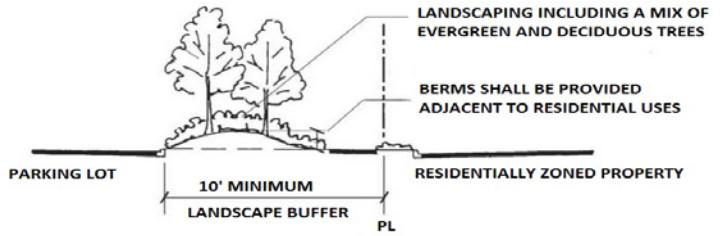


The parking area only has three spaces. Landscaping is located at each end of the parking lot.

c) Landscape islands (minimum of 400 square feet) shall be provided for every 10 spaces in large parking fields and shall include a minimum of one deciduous tree (two inch caliper minimum). See example to right.

The parking lot only has three spaces.

d) Pedestrian connections between parking lots and buildings shall be provided along with connections to sidewalks along adjoining public streets.



The project will include the extension of the multi-use path along the frontage of the property.

e) Parking should be located to the side and rear of a project site where feasible. However, no more than 10 percent of the required parking shall be in the rear service area (typically not used for general public access) of a project site.

Parking is located to the side of the building.

f) Parking areas shall be screened from adjoining residential areas through the use of landscaping and berthing. This buffer shall be a minimum of 10 feet in width (see example to right).

The subject property has drainage facilities on two sides, a proposed dog park to the north, and

the road to the east.

*g) Commercial centers that include tenants that utilize shopping carts shall provide a “cart corral” within 150 feet of 85 percent of their parking stalls.*

The proposed use will not utilize shopping carts.

*h) For commercial centers exceeding 5 acres, a maintenance plan shall be required for parking lots that includes regular sweeping and a snow removal/storage plan for winter weather events.*

The proposed use is not a commercial center.

*i) For commercial centers adjoining residential areas, parking lot sweeping shall be limited to the hours between 8:00 am and 9:00 pm.*

The proposed use is not commercial center.

*j) Parking lot design, including space dimensions, aisle widths, etc. shall comply with the provisions of the Carson City Municipal Code.*

The proposed parking lot design complies with the Municipal Code.

*k) Outdoor sales or special events may not reduce parking past minimum requirements mandated in the Carson City Municipal Code.*

Outdoor sales and special events are not proposed on the subject property.

#### *2.1.4 Commercial Landscaping*

*a) Landscaping, including plant materials and themes shall be consistent throughout the Lompa Ranch North SPA.*

It is anticipated that this will be the first project build on the east side of Lompa Ranch North Specific Plan Area. The plants will establish the theme for the balance of the east side.

*b) Landscaping standards contained in the Carson City Development Standards shall apply within Lompa Ranch North.*

Proposed landscaping will be consistent with the City's landscaping standards.

*c) Within commercial centers, areas not utilized for parking, buildings, plazas, or access/circulation shall be landscaped to the back of curb. Unbuilt pad areas shall be excluded*

from this standard.

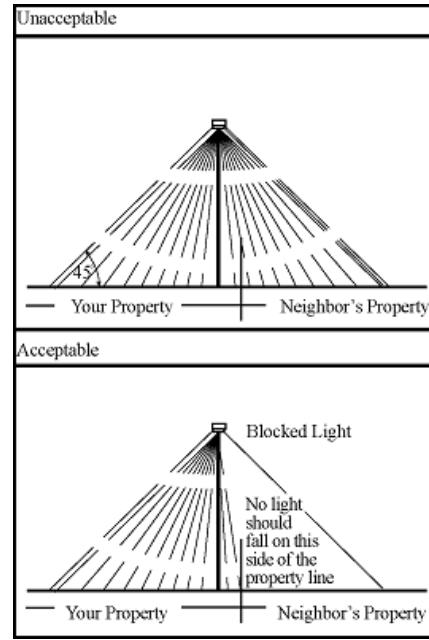
The proposed use is not a commercial center.

*d) Drought tolerant plantings shall be used in conjunction with low water demand principles and techniques.*

A detailed landscape plan will be submitted with the site improvement permit. Staff will verify landscaping is comprised of drought tolerant plants.

*e) All landscaped areas shall be irrigated with permanent automatic irrigation systems. All irrigation systems shall be placed underground.*

A detailed landscape plan will be submitted with the site improvement permit. Staff will verify the plans call for a buried irrigation system.



*f) Landscape maintenance within commercial areas shall be the responsibility of individual property owners or completed through a private maintenance association.*

All landscaping shall be the responsibility of the property owner.

*g) Landscaping along adjoining rights-of-way shall be a minimum width of 15 feet and provide a mix of trees, shrubs, and living groundcover. Trees shall be provided at a rate of 1 tree per 25 lineal feet of street frontage with a minimum of six shrubs per tree.*

The plans call for a 30 foot landscaped area along the right-of-way. A detailed landscape plan will be submitted with the site improvement permit. Staff will verify that the tree and shrubs meet the required numbers.

#### *2.1.5 Commercial Lighting*

*a) Adequate lighting shall be provided to ensure a safe pedestrian environment.*

A detailed lighting plan will be submitted at the time of building permit. Staff will ensure the adequacy of the plan at that time.

*b) Parking lot lighting within 75 feet of residential areas shall be limited to 12 feet in height and shall incorporate shielded fixtures. Additional height limitations for parking lot lighting within certain distances of residential areas are identified in the Carson City Development Standards.*

The parking lot is not within 75 feet of residential areas.

*c) Parking lot lighting shall use shielded/directed fixtures to ensure that spill-over and glare do not occur on adjoining properties. See example to right.*

A detailed lighting plan, including a photometric survey, will be submitted at the time of building permit. Staff will ensure the adequacy of the plan at that time.

*d) The use of bollard lighting is encouraged in pedestrian areas.*

Given the use, there are not identified pedestrian areas.

*e) Exterior lighting shall be used for purposes of illumination and safety only, and shall not be designed for, or used as, an advertising display.*

A detailed lighting plan will be submitted at the time of building permit. Staff will ensure the adequacy of the plan at that time.

#### 2.1.6 Commercial Signs

*Signs and their integration into the project is a critical element in the design of Lompa Ranch North. Careful use of forms, styles, materials, and colors will establish continuity throughout the community. Signs are intended to be utilized only where necessary, and in an understated manner, emphasizing an image of permanence and quality.*

*a) Signs shall be included on facades or entry canopies of buildings and illuminated or backlit with indirect lighting. All tenant identification signs shall be consistently located and integrated into the architectural design of the building entry. Storefront signs shall be proportional with the building architecture.*



The applicant has identified the location of a sign along Airport Road, but a specific sign has not been proposed. A sign permit will be required prior to erecting any signs. Compliance will be determined at that time.

*b) Flashing or animated signs are prohibited.*

A sign permit will be required prior to erecting any signs. Compliance will be determined at that time.

*c) Building signs that project more than 4 inches beyond the wall façade are prohibited, unless incorporated as an architectural element.*



A sign permit will be required prior to erecting any signs. Compliance will be determined at that time.

*d) Hanging signs may be included under eaves above walkways and shall maintain a minimum of 8 feet of clearance. These signs shall be architecturally compatible with the building they serve.*

A sign permit will be required prior to erecting any signs. Compliance will be determined at that time.

#### 2.1.7 Commercial Fencing

*a) Walls and fences shall be utilized to provide a buffer between incompatible uses. It is important, however, that walls are appropriately integrated into each project.*

The applicant has provided a wall detail for a six foot tall fence. The use is required to utilize a six foot solid wall of fence to screen the use from adjacent properties.

*b) Solid fencing (6 foot minimum) shall be installed between commercial uses within Lompa Ranch North and adjoining residential uses. This can include wood or vinyl fencing, concrete block walls, pre-cast wall systems, or similar.*

The applicant has provided a wall detail for a six foot tall fence. The use is required to utilize a six foot solid wall of fence to screen the use from adjacent properties.

*c) Chain link fencing shall be prohibited within commercial centers.*

The applicant's wall detail is not chain link fencing.

#### 2.1.8 Commercial Trash and Utility Areas

*a) Service and maintenance areas shall be screened from adjacent public right-of-ways,*

pedestrian plazas or adjacent residential uses with landscaped berms, walls or plantings. Storage areas shall be enclosed by a 100% site obscuring fence or wall, permanently installed and maintained at a minimum height of six feet.

At the time of building permit, wash area and dumpster locations will be identified. These areas will be screened by the six foot tall fence.

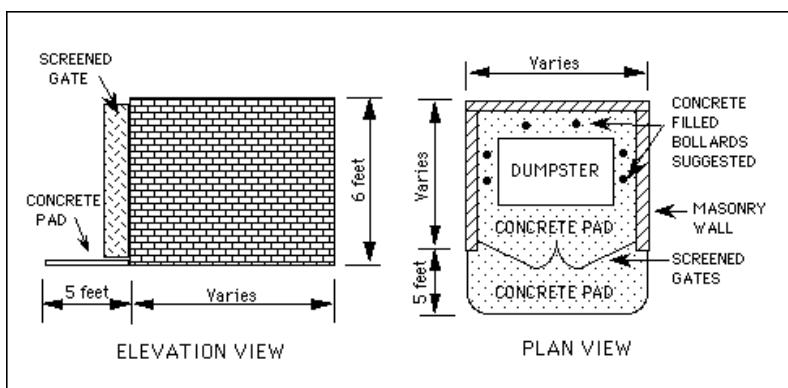
b) All trash and garbage bins shall be stored in an enclosure that includes solid screening, to the approval of the Carson City Community Development Department.

At the time of building permit, staff will verify dumpster enclosures.

c) Trash enclosures shall incorporate building materials, colors, etc. that are complementary to the overall project architecture. Gates shall be constructed of durable building materials that screens at a minimum 80% of the view into the trash enclosure. Wood or chain link gates are not allowed (see example to left).

At the time of building permit, staff will verify dumpster enclosures.

d) Trash enclosures must include provisions for concrete pads or appropriately designed asphalt sections in front of the enclosure. The area in front of the trash enclosure shall be a minimum of six (6) feet to reduce pavement damage from disposal trucks.



At the time of building permit, staff will verify the provision of a concrete pad or appropriately designed asphalt section in front of the trash enclosure, as well as a minimum of six feet to reduce pavement damage.

**2. Will not be detrimental to the use, peaceful enjoyment, economic value, or development of surrounding properties or the**

**general neighborhood; and is compatible with the integrity of adjacent development and neighborhoods or includes improvements or modifications either on-site or within the public right-of-way to mitigate development related to adverse impacts such as noise, vibrations, fumes, odors, dust, glare or physical activity.**

The subject property is across the street from the animal shelter, to the south of the proposed

dog park, and to the east and north of drainage facilities. The proposed storage use is a quiet use, and will likely establish before any of the houses in Lompa East. Staff does not find that the proposed use will be detrimental to the use, peaceful enjoyment, economic value or development of surrounding properties or the general neighborhood.

***3. Will have little or no detrimental effect on vehicular or pedestrian traffic.***

Based on the Institute of Traffic Engineer's Trip Generation Manual, the project will generate 63 daily trips. The existing roadway network can accommodate these trips. The use is not anticipated to have a detrimental effect on pedestrian traffic. In fact, due to the extension of the multi-use trail, the use will have a positive effect on pedestrian traffic.

***4. Will not overburden existing public services and facilities, including schools, police and fire protection, water, sanitary sewer, public roads, storm drainage, and other public improvements.***

The proposed use will not overburden existing public services. It is not residential, so there will be no students. The existing sewer, water and storm drain infrastructure are sufficient to provide service to the project.

***5. Meets the definition and specific standards set forth elsewhere in this Title for such particular use and meets the purpose statement of that district.***

Although the use is not allowed in the General Commercial zoning district, as the site is split zoned, the use may establish subject to a Special Use Permit.

***6. The use will not be detrimental to the public health, safety, convenience and welfare.***

The proposed boat and RV storage use will not be detrimental of public health, safety, convenience and welfare. It is a fairly passive use.

***7. Will not result in material damage or prejudice to other property in the vicinity, as a result of proposed mitigation measures.***

The proposed boat and RV storage use will not result in material damage or prejudice to other property in the vicinity.

Attachments:

Engineering Comments

Fire Department Comments

Parks Recreation Open Space Department Comments

Application: SUP-19-083

**Engineering Division  
Planning Commission Report  
File Number SUP-19-083**

**TO:** Hope Sullivan - Planning Department  
**FROM:** Stephen Pottéy – Development Engineering Department  
**DATE:** June 18, 2019

**SUBJECT TITLE:**

Engineering Comments for SUP-19-083 RV and Boat Storage at Airport Rd, apn 010-041-76

**RECOMMENDATION:**

The Engineering Division has no preference or objection to the special use request provided that the following conditions are met:

- A site specific geotechnical investigation must be performed as recommended by the feasibility geotechnical investigation, and construction recommendations be incorporated into the design.
- The multi-use path must be extended along the frontage as shown.
- The project must meet the Carson City Municipal Code, Development Standards, and Standard Detail requirements including:
  - The project must balance flood zone cut and fill quantities.
  - The project must provide funds to the City to process a CLOMR.
  - The water main must be extended along the frontage as shown.

**ENGINEERING DISCUSSION:**

The Engineering Division has reviewed the application within our areas of purview relative to adopted standards and practices and to the provisions of CCMC 18.02.080, Conditional Uses. The Engineering Division offers the following discussion:

**C.C.M.C. 18.02.080 (5a) - Master Plan**

The request is not in conflict with any Engineering Master Plans.

**C.C.M.C. 18.02.080 (5b) – Use, Peaceful Enjoyment, Economic Value, Compatibility**

Development Engineering has no comment on this finding.

**C.C.M.C. 18.02.080 (5c) - Traffic/Pedestrians**

The existing infrastructure and proposed drive isles are sufficient to provide safe access and circulation if conditions are met.

Engineering Division  
Planning Commission Report

**C.C.M.C. 18.02.080 (5d) - Public Services**

The existing sewer, water, and storm drain infrastructure are sufficient to provide service to the project.

**C.C.M.C. 18.02.080 (5e) – Title 18 Standards**

Development Engineering has no comment on this finding.

**C.C.M.C. 18.02.080 (5f) – Public health, Safety, Convenience, and Welfare**

The project meets engineering standards for health and safety.

**C.C.M.C. 18.02.080 (5g) – Material Damage or Prejudice to Other Property**

Development Engineering has no comment on this finding.

**C.C.M.C. 18.02.080 (5h) – Adequate Information**

The plans and reports provided were adequate for this analysis.

## **Fire Department Comments**

05/20/2019

Comments for SUP 19-083:

1. Project must comply with the currently adopted International Fire Code and northern Nevada fire code amendments
2. An additional fire hydrant(s) is required on the interior of the project to meet hydrant spacing per 2018 IFC Appendix C. The additional hydrant added on Airport Rd at the south east corner of the parcel does not comply.

Notes:

- Depending on final design, the storage structures may require fire sprinklers.
- The LP dispenser should be shown on the building permit and tank size must be indicated. Installation must be approved by the NV LPG Board.
- Electric gate requires a Knox key switch on the street side pedestal and a pavement loop or Knox key switch on the egress side pedestal if present.
- Project is subject to the fire protection fee outlined in the Lompa Ranch North Specific Plan, section 3.8

***Dave Ruben***  
Fire Marshal  
Carson City Fire Department  
777 S. Stewart Street  
Carson City, NV 89701

Direct 775-283-7153  
Main 775-887-2210  
FAX 775-887-2209

## **Parks & Rec Comments**

06/10/2019

SUP-19-083

Hope.....

The Parks, Recreation & Open Space Department has the following comments on the above referenced Special Use Permit application.

- 1) The project site is adjacent to a future three acre dog park associated with the proposed Lompa Ranch East development. The applicant will be required to install the screen wall upslope from the park's south property line and adjacent to the storage facility. The landscape design for the screening buffer between the proposed park and project site will be reviewed and approved by the Parks, Recreation & Open Space Department.
- 2) The Unified Pathway Master Plan identifies Airport Road as an on-street bicycle facility. The applicant will be required to protect the existing bike lanes during the project's construction and repair lanes/restripe the pavement markings at their expense, if damage occurs. Any repairs and restriping to the bike lanes has to be approved by Development Engineering.
- 3) Design all project driveway approaches that cross the 10' pedestrian multi-use path along Airport Road to meet ADA standards.
- 4) The applicant will be required to maintain all common landscape and open space areas on the project site in perpetuity, including any landscaping in the road right-of-way.
- 5) Incorporate "best management practices" into the applicant's construction documents and specifications to reduce the spread of noxious weeds. The Parks, Recreation & Open Space Department is willing to assist the applicant with this aspect of their project.
- 6) The applicant will be required to remove any graffiti on the perimeter screening wall, per Carson City Municipal Code.

Thank you for the opportunity to review the proposed project and provide comments.

If you have any questions about our department's comments, please feel free to contact me.

*Vern L. Krahn  
Senior Park Planner*

Carson City Parks, Recreation & Open Space Department  
3303 Butti Way, Bldg #9  
Carson City, NV 89701  
Office Phone: (775) 887-2262 x 7343  
Fax: (775) 887-2145  
[vkrahn@carson.org](mailto:vkrahn@carson.org)

# Special Use Permit

FOR

Sierra Tahoe

RV & Boat Storage

**Prepared For:**

Sierra Tahoe RV & Boat Storage  
Attn: Rick Ardizzone  
892 Southwood Blvd.  
Incline Village, NV 89451  
408.891.8149

**Prepared By:**



720 Tahoe Street, Suite 1  
Reno, NV 89509  
775.636.7905

**May 2019**

18.041

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**Carson City Planning Division**  
108 E. Proctor Street • Carson City NV 89701  
Phone: (775) 887-2180 • E-mail: planning@carson.org

## FILE # SUP - 18 -

|  |              |
|--|--------------|
| APPLICANT                                      | PHONE #      |
| Sierra Tahoe RV & Boat Storage LLC             | 408.891.8149 |
| MAILING ADDRESS, CITY, STATE, ZIP              |              |
| 892 Southwood Blvd., Incline Village, NV 89451 |              |
| EMAIL ADDRESS                                  |              |
| rardizzo@apr.com                               |              |
| PROPERTY OWNER                                 | PHONE #      |
| Sierra Tahoe RV & Boat Storage LLC             | 408.891.8149 |
| MAILING ADDRESS, CITY, STATE, ZIP              |              |
| 892 Southwood Blvd., Incline Village, NV 89451 |              |
| EMAIL ADDRESS                                  |              |
| rardizzo@apr.com                               |              |
| APPLICANT AGENT/REPRESENTATIVE                 | PHONE #      |
| Monte Vista Consulting, Ltd.                   | 775.636.7905 |
| MAILING ADDRESS, CITY STATE, ZIP               |              |
| 720 Tahoe Street, Suite 1, Reno NV 89509       |              |
| EMAIL ADDRESS                                  |              |
| mike@montevistaconsulting.com                  |              |

FOR OFFICE USE ONLY:

CCMC 18.02.080

## SPECIAL USE PERMIT

**FEE\*:** \$2,450.00 MAJOR  
\$2,200.00 MINOR (Residential  
zoning districts)  
+ noticing fee

\*Due after application is deemed complete by  
staff

**SUBMITTAL PACKET – 4 Complete Packets (1 Unbound  
Original and 3 Copies) including:**

Application Form  
Detailed Written Project Description  
Site Plan  
Building Elevation Drawings and Floor Plans  
Special Use Permit Findings  
Master Plan Policy Checklist  
Applicant's Acknowledgment Statement  
Documentation of Taxes Paid-to-Date  
Project Impact Reports (Engineering)

**CD or USB DRIVE with complete application in PDF**

**Application Received and Reviewed By:**

**Submittal Deadline:** See attached Planning Commission  
application submittal schedule.

**Note:** Submittals must be of sufficient clarity and detail for  
all departments to adequately review the request. Additional  
information may be required.

Project's Assessor Parcel Number(s):  
**010-041-76** Street Address  
**Airport Road**

Project's Master Plan Designation  
**Mixed-Use Employment/Open Space** Project's Current Zoning  
**PR/GC** Nearest Major Cross Street(s)  
**Airport Road/Butti Way**

Please provide a brief description of your proposed project and/or proposed use below. Provide additional pages to describe your request in more detail.

**The proposed project will be a private RV & Boat Storage Facility. Please see narrative for detailed description.**

### PROPERTY OWNER'S AFFIDAVIT

I, Rick Ardizzone, being duly deposed, do hereby affirm that I am the record owner of the subject property, and that I have knowledge of, and I agree to, the filing of this application.

Signature

Address

*561 San Benito Ave  
Los Gatos, Ca 95030*

*5/13/19*

Use additional page(s) if necessary for additional owners.

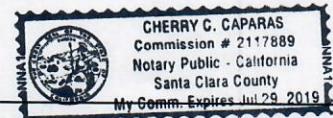
STATE OF NEVADA *California*  
COUNTY *Santa Clara* )

On May 13, 2019, Rick Ardizzone, personally appeared before me, a notary public, personally known (or proved) to me to be the person whose name is subscribed to the foregoing document and who acknowledged to me that he/she executed the foregoing document.

Notary Public



**NOTE:** If your project is located within the Historic District or airport area, it may need to be scheduled before the Historic Resources Commission or the Airport Authority in addition to being scheduled for review by the Planning Commission. Planning staff can help you make this determination.

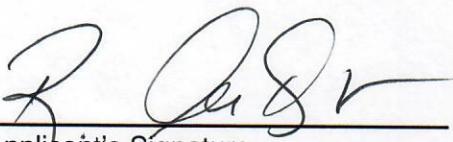
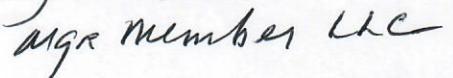


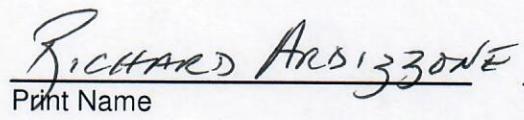
If there is any additional information that would provide a clearer picture of your proposal that you would like to add for presentation to the Planning Commission, please be sure to include it in your detailed description.

Please type and sign the statement on the following page at the end of your findings response.

**ACKNOWLEDGMENT OF APPLICANT**

I certify that the forgoing statements are true and correct to the best of my knowledge and belief. I agree to fully comply with all conditions as established by the Planning Commission. I am aware that this permit becomes null and void if the use is not initiated within one-year of the date of the Planning Commission's approval; and I understand that this permit may be revoked for violation of any of the conditions of approval. I further understand that approval of this application does not exempt me from all City code requirements.

  
\_\_\_\_\_  
Applicant's Signature  


  
\_\_\_\_\_  
Print Name

\_\_\_\_\_  
Date

# Master Plan Policy Checklist

## Special Use Permits & Major Project Reviews & Administrative Permits

### PURPOSE

The purpose of a development checklist is to provide a list of questions that address whether a development proposal is in conformance with the goals and objectives of the 2006 Carson City Master Plan that are related to non-residential and multi-family residential development. This checklist is designed for developers, staff, and decision-makers and is intended to be used as a guide only.

Development Name: Sierra Tahoe RV & Boat Storage

Reviewed By: \_\_\_\_\_

Date of Review: \_\_\_\_\_

### DEVELOPMENT CHECKLIST

The following five themes are those themes that appear in the Carson City Master Plan and which reflect the community's vision at a broad policy level. Each theme looks at how a proposed development can help achieve the goals of the Carson City Master Plan. A check mark indicates that the proposed development meets the applicable Master Plan policy. The Policy Number is indicated at the end of each policy statement summary. Refer to the Comprehensive Master Plan for complete policy language.

#### CHAPTER 3: A BALANCED LAND USE PATTERN



The Carson City Master Plan seeks to establish a balance of land uses within the community by providing employment opportunities, a diverse choice of housing, recreational opportunities, and retail services.

##### Is or does the proposed development:

- Meet the provisions of the Growth Management Ordinance (1.1d, Municipal Code 18.12)?
- Use sustainable building materials and construction techniques to promote water and energy conservation (1.1e, f)?
- Located in a priority infill development area (1.2a)?
- Provide pathway connections and easements consistent with the adopted Unified Pathways Master Plan and maintain access to adjacent public lands (1.4a)?

- Protect existing site features, as appropriate, including mature trees or other character-defining features (1.4c)?
- At adjacent county boundaries or adjacent to public lands, coordinated with the applicable agency with regards to compatibility, access and amenities (1.5a, b)?
- In identified Mixed-Use areas, promote mixed-use development patterns as appropriate for the surrounding context consistent with the land use descriptions of the applicable Mixed-Use designation, and meet the intent of the Mixed-Use Evaluation Criteria (2.1b, 2.2b, 2.3b, Land Use Districts, Appendix C)?
- Meet adopted standards (e.g. setbacks) for transitions between non-residential and residential zoning districts (2.1d)?
- Protect environmentally sensitive areas through proper setbacks, dedication, or other mechanisms (3.1b)?
- Sited outside the primary floodplain and away from geologic hazard areas or follows the required setbacks or other mitigation measures (3.3d, e)?
- Provide for levels of services (i.e. water, sewer, road improvements, sidewalks, etc.) consistent with the Land Use designation and adequate for the proposed development (Land Use table descriptions)?
- If located within an identified Specific Plan Area (SPA), meet the applicable policies of that SPA (Land Use Map, Chapter 8)?

## CHAPTER 4: EQUITABLE DISTRIBUTION OF RECREATIONAL OPPORTUNITIES



The Carson City Master Plan seeks to continue providing a diverse range of park and recreational opportunities to include facilities and programming for all ages and varying interests to serve both existing and future neighborhoods.

**Is or does the proposed development:**

- Provide park facilities commensurate with the demand created and consistent with the City's adopted standards (4.1b)?
- Consistent with the Open Space Master Plan and Carson River Master Plan (4.3a)?

## CHAPTER 5: ECONOMIC VITALITY



The Carson City Master Plan seeks to maintain its strong diversified economic base by promoting principles which focus on retaining and enhancing the strong employment base, include a broader range of retail services in targeted areas, and include the roles of technology, tourism, recreational amenities, and other economic strengths vital to a successful community.

**Is or does the proposed development:**

- Encourage a citywide housing mix consistent with the labor force and non-labor force populations (5.1j)
- Encourage the development of regional retail centers (5.2a)
- Encourage reuse or redevelopment of underused retail spaces (5.2b)?
- Support heritage tourism activities, particularly those associated with historic resources, cultural institutions and the State Capitol (5.4a)?
- Promote revitalization of the Downtown core (5.6a)?
- Incorporate additional housing in and around Downtown, including lofts, condominiums, duplexes, live-work units (5.6c)?

## CHAPTER 6: LIVABLE NEIGHBORHOODS AND ACTIVITY CENTERS



The Carson City Master Plan seeks to promote safe, attractive and diverse neighborhoods, compact mixed-use activity centers, and a vibrant, pedestrian-friendly Downtown.

### Is or does the proposed development:

- Use durable, long-lasting building materials (6.1a)?
- Promote variety and visual interest through the incorporation of varied building styles and colors, garage orientation and other features (6.1b)?
- Provide variety and visual interest through the incorporation of well-articulated building facades, clearly identified entrances and pedestrian connections, landscaping and other features consistent with the Development Standards (6.1c)?
- Provide appropriate height, density and setback transitions and connectivity to surrounding development to ensure compatibility with surrounding development for infill projects or adjacent to existing rural neighborhoods (6.2a, 9.3b 9.4a)?
- If located in an identified Mixed-Use Activity Center area, contain the appropriate mix, size and density of land uses consistent with the Mixed-Use district policies (7.1a, b)?
- If located Downtown:
  - Integrate an appropriate mix and density of uses (8.1a, e)?
  - Include buildings at the appropriate scale for the applicable Downtown Character Area (8.1b)?
  - Incorporate appropriate public spaces, plazas and other amenities (8.1d)?
- Incorporate a mix of housing models and densities appropriate for the project location and size (9.1a)?

## CHAPTER 7: A CONNECTED CITY



The Carson City Master Plan seeks to promote a sense of community by linking its many neighborhoods, employment areas, activity centers, parks, recreational

amenities and schools with an extensive system of interconnected roadways, multi-use pathways, bicycle facilities, and sidewalks.

**Is or does the proposed development:**

- Promote transit-supportive development patterns (e.g. mixed-use, pedestrian-oriented, higher density) along major travel corridors to facilitate future transit (11.2b)?
- Maintain and enhance roadway connections and networks consistent with the Transportation Master Plan (11.2c)?
- Provide appropriate pathways through the development and to surrounding lands, including parks and public lands, consistent with the Unified Pathways Master Plan (12.1a, c)?

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## Project Information

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Location: Airport Road

APN: 010-041-76

Site Area: 7.28 ac

Developed Area: ±7.0 ac

Disturbance: ±7.0 ac

Zoning: PR (±1.43 Ac)/GC (±5.85Ac)

Master Plan Designation: Mixed-Use Employment/Open Space

Proposed Use: Covered RV & Boat Storage Facility

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## Pre-Development Discussion

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### Existing Conditions & Development:

The subject property is part of the larger Lompa Ranch Development. It is currently undeveloped consisting of moderate vegetation consisting of native grasses and brush. It generally slopes from north to south with the western and southern boundaries of the site lower than the remainder of the property (Reference the attached site plan for location of the onsite flood zones). There is no existing infrastructure extended to the site with the exception of access from Airport Road. Water facilities exist to the north of the property and sanitary sewer services exist to the west. The site is bounded on two sides by large stormwater detention facilities which is where all existing storm flows are discharged.

### Surrounding Properties:

- North: Undeveloped (Lompa Ranch Residential Site) Zoning: MFA
- South: Open NDOT Detention Pond Zoning: A/P
- East: Municipal Buildings Zoning: PR
- West: Open NDOT Detention Pond Zoning: A

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## Proposed Development Discussion

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### Proposed Development:

Sierra Tahoe RV and Boat Storage is a self-storage facility providing RV and Boat owners a secure location to store their recreation vehicles and boats. The facility may also store small recreational craft such as jet skis and wave runners. It will offer customers a sales office onsite in order to conduct the rental and business of the facility and offer a small retail sales area within the sales office in order to provide items in connection with RV and boat owner needs such as ice, and motor oil, etc. Also available exclusively to customers will be a sewage dump station, an RV/boat wash station propane refill sale onsite as well. There will be no onsite mechanic services available and will customers be prohibited from repairing and/or maintaining their recreational vehicles onsite.

Hours of operation are intended from 6:00 am to 10:00 pm with the office open from 8:00 am to 6:00pm daily. Customers will ingress and egress the site during the hours of operation through a security gate controlled by a key pad requiring an individual's code unique to each customer. The site will be monitored through a security camera system and each customer will only have access if they have registered their vehicle and shown proof of insurance with the office and are current with their payments.

### **Proposed Improvements:**

The developed site will include the construction of a ±1,000 s.f. managers office and covered storage for ±275 recreational vehicles depending on size. The canopies will have a maximum height of 19' and will be comprised of steel structural supports and sheet metal roofing. Additionally, a sewer dump station, wash bay and propane refill tank will be provided. The majority of the site will be paved and fenced with solid view fencing. There is a single point of access to the site. The entrance has been designed to allow for two RVs to be stacked in the inbound lane while a single exit lane has been provided. In order to allow for observation from the manager's office, the sewer dump station and RV wash bay are located near the main entrance.

Preliminary grading of the site has been completed with the goal of exporting material to the adjacent development to the north. Although portions of the development will be within the flood zone, all pavement will be elevated a minimum of 2.0' above the established base flood elevation protecting the improvements from flooding. The site will maintain existing drainage patterns sloping from north to south where flow will be collected and treated in a detention pond before being discharged to the detention facilities to the south. A majority of onsite stormflows are intended as surface drainage with only minimal underground storm drain improvements.

Water service will be provided by extending a water main from the existing main located near the intersection of Airport Road and Butti Lane. Sanitary sewer service will be provided by connecting to an existing manhole along the western property with a drop connection.

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### **Required Findings for Special Use Permit**

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#### *1. Will be consistent with the objectives of the Master Plan elements.*

*Explain how your project will further and be in keeping with, and not contrary to, the goals of the Master Plan elements. Turn to the Master Plan Policy Checklist included with this application. The Master Plan Policy Checklist for Special Use Permits and Major Project Reviews addresses five items that appear in the Carson City Master Plan. Each theme looks at how a proposed development can help achieve the goals of the Carson City Master Plan. Address each theme; a check indicates that the proposed development meets the applicable Master Plan Policy. Provide written support of the policy statement in your own words as a part of these findings. For additional guidance, please refer to the Carson City Master Plan document on our website at [www.carson.org/planning](http://www.carson.org/planning) or you may contact the Planning Division to review the document in our office or request a copy.*

*1. A Balanced Land Use Pattern*

- The proposed development will promote a balanced land use pattern by extending public trails & services, incorporating durable and sustainable building materials, providing generous setbacks & allowing for the expansion of drainage detention facilities to the south.***

*2. Equitable Distribution of Recreational Opportunities*

- The improvements will include extending a 10' pedestrian path across the frontage of the site extending an existing trail network.***

3. *Economic Vitality*
  - The proposed use will provide for offsite RV storage which will allow for more dense residential communities and expanded RV sales.
4. *Livable Neighborhoods and Activity Centers*
  - The proposed development will provide a sustainable project with landscape buffers and a screen wall surrounding the storage area.
5. *A Connected City*
  - The project will extend the 10' pedestrian path from the south across the full project frontage which will eventually lead to a community park.

2. *Will not be detrimental to the use, peaceful enjoyment, economic value, or development of surrounding properties or the general neighborhood; and is compatible with and preserves the character and integrity of adjacent development and neighborhoods or includes improvements or modifications either on-site or within the public right-of-way to mitigate development related to adverse impacts such as noise, vibrations, fumes, odors, dust, glare or physical activity.*

- a. *Describe the general types of land uses and zoning designations adjoining your property (for example: North: grocery store, Retail Commercial zoning)  
Reference Pre-Development Discussion above.*
- b. *Explain why your project is similar to existing development in the neighborhood, and why it will not hurt property values or cause problems, such as noise, dust, odors, vibration, fumes, glare, or physical activity, etc. with neighboring property owners. Have other properties in your area obtained approval of a similar request? How will your project differ in appearance from your neighbors? Your response should consider the proposed physical appearance of your proposal, as well as comparing your use to others in the area.*  
**The proposed use is consistent with the surrounding development and will not negatively impact property values. To the south and west are open detention ponds and the Carson City Public Works facility is to the East. To the north are a proposed park and residential development. The project will be fully screened and provide landscaping to address visual concerns.**
- c. *Provide a statement explaining how your project will not be detrimental to the use, peaceful enjoyment or development of surrounding properties and the general neighborhood.*  
**The project use will drive minimal traffic increases, generate minimal noise and will have set business hours therefore having no detrimental effect on the surrounding properties.**
- d. *If outdoor lighting is to be a part of the project, please indicate how it will be shielded from adjoining property and the type of lighting (wattage/height/placement) provided.*  
**All required lighting will be provided on the canopies and office building. At final building permit, a photometrics plan will be provided showing compliance with Carson City Development standards.**
- e. *Describe the proposed landscaping, including screening and arterial landscape areas (if required by the zoning code). Include a site plan with existing and proposed landscape shown on the plan which complies with City ordinance requirements.*  
**The proposed landscape improvements will provide evergreen screen buffers on the north and south borders, as requested by the Parks's Dept. review comments. There is a double row of street trees proposed along Airport Road and the project will have a Plant List loaded with pollinator friendly trees, shrubs, groundcovers, vines and perennials.**

f. Explain any short-range and long-range benefit to the people of Carson City that will occur if your project is approved.

**With the approval of the project, Carson City will see increased tax revenue as well as improved stormwater detention.**

3. Will have little or no detrimental effect on vehicular or pedestrian traffic.

Consider the pedestrian and vehicular traffic that currently exists on the road serving your project. What impact will your development have to pedestrian and vehicular traffic when it is successfully operating? Will additional walkways and traffic lights be needed? Will you be causing traffic to substantially increase in the area? State how you have arrived at your conclusions.

The Sierra Tahoe RV & Boat Storage will have a minimal impact on traffic generation and will improve pedestrian traffic conditions. RV & Boat Storage is a low impact use in regards to traffic generation. In accordance with the requirements of the Major Project Review, a 10' wide pedestrian trail has been extended along the project frontage, therefore improving pedestrian traffic.

4. Will not overburden existing public services and facilities, including schools, police and fire protection, water, sanitary sewer, public roads, storm drainage and other public improvements.

a. How will your project affect the school district? Will your project add to the student population or will it provide a service to the student population?

**The proposed project will have no impact on the school district with the exception of providing tax revenue.**

b. How will your project affect police and fire protection?

**The proposed project should have a minimal impact on both police and fire protection. The project will include a comprehensive security system and will provide fire hydrants and access in accordance with fire department requirements.**

c. Is the water supply serving your project adequate to meet your needs without degrading supply and quality to others in the area? Is there adequate water pressure? Are the lines in need of replacement? Is your project served by a well? Contact the Development Engineering Division at (775) 887-2300 for assistance with this item, if applicable.

**Based on discussions with Carson City Public Works, a water main extension will be required and will be extended from the intersection of Airport Road and Butti Lane. Additionally, based in preliminary discussions, there is ample pressure to serve the development.**

d. If your project will result in the covering of land area with paving or a compacted surface, how will drainage be accommodated? Contact the Development Engineering Division at (775) 887-2300 for assistance with this item, if applicable.

**The proposed development will result in an increase in stormwater runoff due to proposed impervious surfaces, however, based on correspondence with the Carson City Public Works department, no additional stormwater detention will be required due to the project's proximity to the detention ponds to the south and west of the site. In an effort to work with the City, the site will be graded in a manner to allow for additional stormwater detention to the south.**

e. Is there adequate capacity in the sewage disposal trunk line that you will connect to in order to serve your project, or is your site on a septic system? Contact the Development Engineering Division at (775) 887-2300 for assistance with this item, if applicable.

**The proposed sewer connection is to the City's main interceptor line which based on discussions with Carson City Public works has more than enough capacity to serve the project.**

**The project will have a minimal impact on the sanitary sewer infrastructure and does not meet the thresholds for a full sanitary sewer report.**

f. *What kind of road improvements are proposed or needed to accommodate your project? Contact the Development Engineering Division at (775) 887-2300 for assistance with this item, if applicable.*  
**No new roadway improvements are required to serve the project.**

g. *Indicate the source of the information that you are providing to support your conclusions and statements made in this application (private engineer, Development Engineering, Public Works, Transportation, title report or other sources).*  
**The conclusions above were made based on discussions with Carson City Public Works and Engineering Division as well as the Major Project Review comments.**

5. *Meets the definition and specific standards set forth elsewhere in Carson City Municipal Code, Title 18 for such particular use and meets the purpose statement of that district.*

*Explain how your project meets the purpose statement of the zoning district in which it is located and how it meets the specific standards that are set forth in that zoning district. In CCMC Section 18.04, Use Districts, find the zoning district where your property is located. Refer to the purpose statement at the beginning of the zoning district section and explain how your project meets the purpose statement of that district. In addition, find the specific Intensity and Dimensional Standards for your zoning district in either CCMC Section 18.04.190 (Residential) or CCMC Section 18.04.195 (Non-Residential) and explain how your project meets these specific standards. To access the Carson City Municipal Code, visit our website at [www.carson.org/planning](http://www.carson.org/planning)*

**The zoning is split between General Commercial (GC) and Public Regional (PR). The proposed use is allowed within the GC zoning district however it requires the SUP in the PR zoning district. This is an office and service use which has minimal impact on the surrounding land uses.**

6. *Will not be detrimental to the public health, safety, convenience and welfare.*

*Provide a statement explaining how your project will not be detrimental to the public health, safety, convenience and welfare. If applicable, provide information on any benefits that your project will provide to the general public.*

**The project will be designed and constructed in accordance with all applicable State and City codes. It is low impact in terms of water, sewer and traffic demand and will allow for increased stormwater detention to the ponds to the south.**

7. *Will not result in material damage or prejudice to other property in the vicinity, as a result of proposed mitigation measures.*

*Provide a statement explaining how your project will not result in material damage or prejudice to other property in the vicinity.*

**The project will provide no material damage to adjacent properties and will serve as a buffer between the residential uses to the north and public works facilities to the east.**

SIERRA TAHOE RV & BOAT STORAGE LLC  
892 Southwood Blvd  
Incline Village, NV 89451

**DATE:** May 22, 2019

**APPLICATION:** SUP-19-083

**APPLICANT/OWNER:** Sierra Tahoe RV & Boat Storage LLC

**AGENT:** Monte Vista Consulting, LTD

**PROJECT APN/ADDRESS:** 010-041-76/Butti Way and east side of Airport Road

**PLANNING COMMISSION MEETING DATE:** June 26, 2019

**RE:** Include text in the submission that the policies of the Specific Plan and Development Agreement have been reviewed and that this proposal has incorporated these requirements and is in compliance.

The policies of the Lompa Ranch North Specific Plan (SPA) and the Lompa Ranch Development Agreement (Ordinance No. 2017-25) have been reviewed and this proposal incorporates those requirements into the plan. The proposed development is in compliance with these governing documents in every respect.

The Development Agreement states that “the development of the plan area will be in compliance with the architectural and design standards incorporated into the Handbook.” (DA Section 1). The SPA along with the phasing plan are referenced as the Handbook.

Section 2.1 of the SPA (i.e. Handbook) deal with commercial uses. The proposed development complies with the Site Planning Standards discussed in section 2.1.1 by placing the lone building in the development in a way to create visual interest along the public street. The other requirements related to grading (section 2.1.2), parking (section 2.1.3), landscaping (2.1.4), lighting (2.1.5), signs (2.1.6), fencing (2.1.7), and trash (2.1.8) have been reviewed and incorporated into the design and is in compliance. Furthermore, the architecture of the building is designed to complement the intended residential design per section 2.4.3 to 2.4.6.

Property Information  
Property ID 01004176  
Location BUTTI WY  
Owner TERRASAS & TRIPP LLC  
Acres 7.28



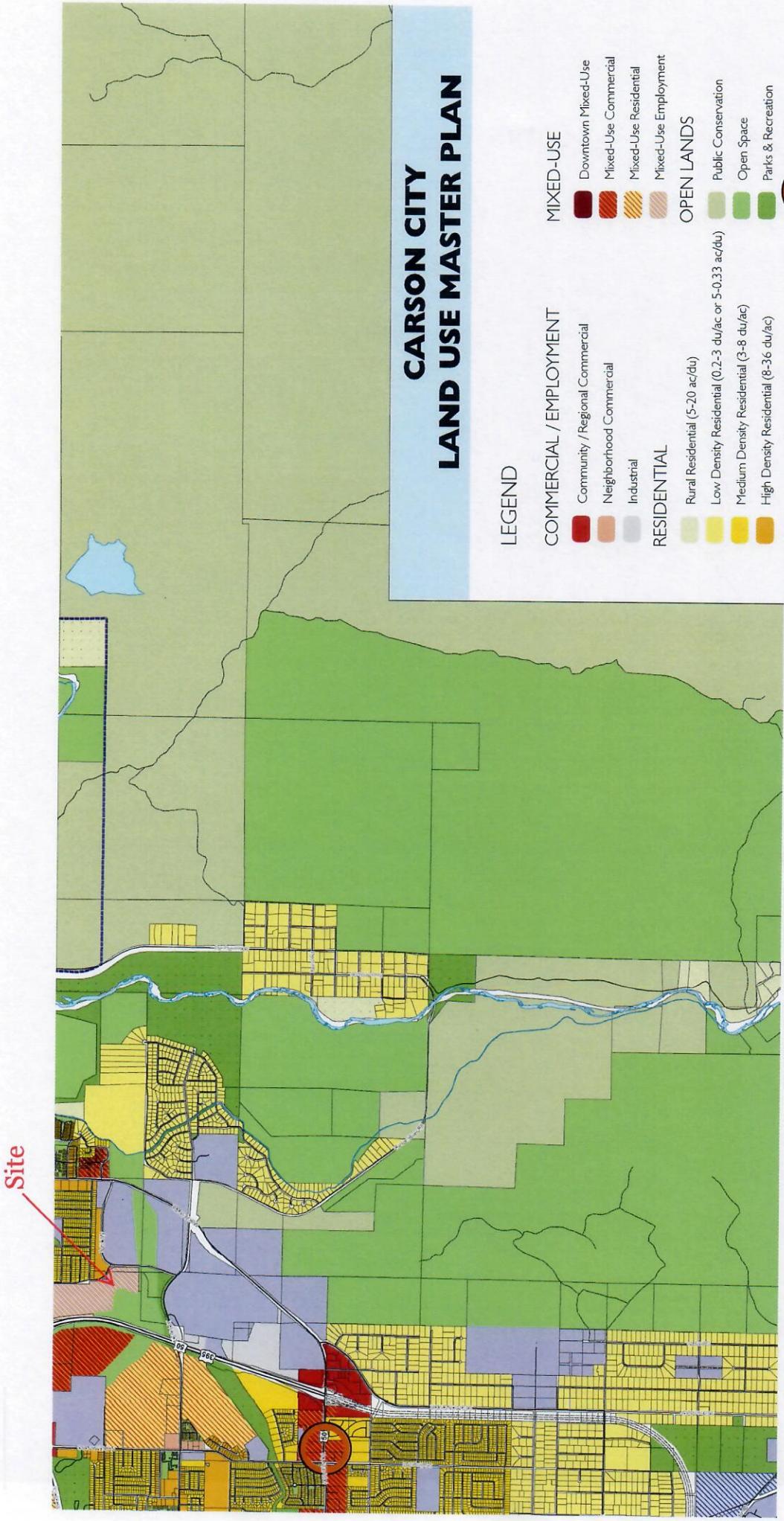
**MAP FOR REFERENCE ONLY  
NOT A LEGAL DOCUMENT**

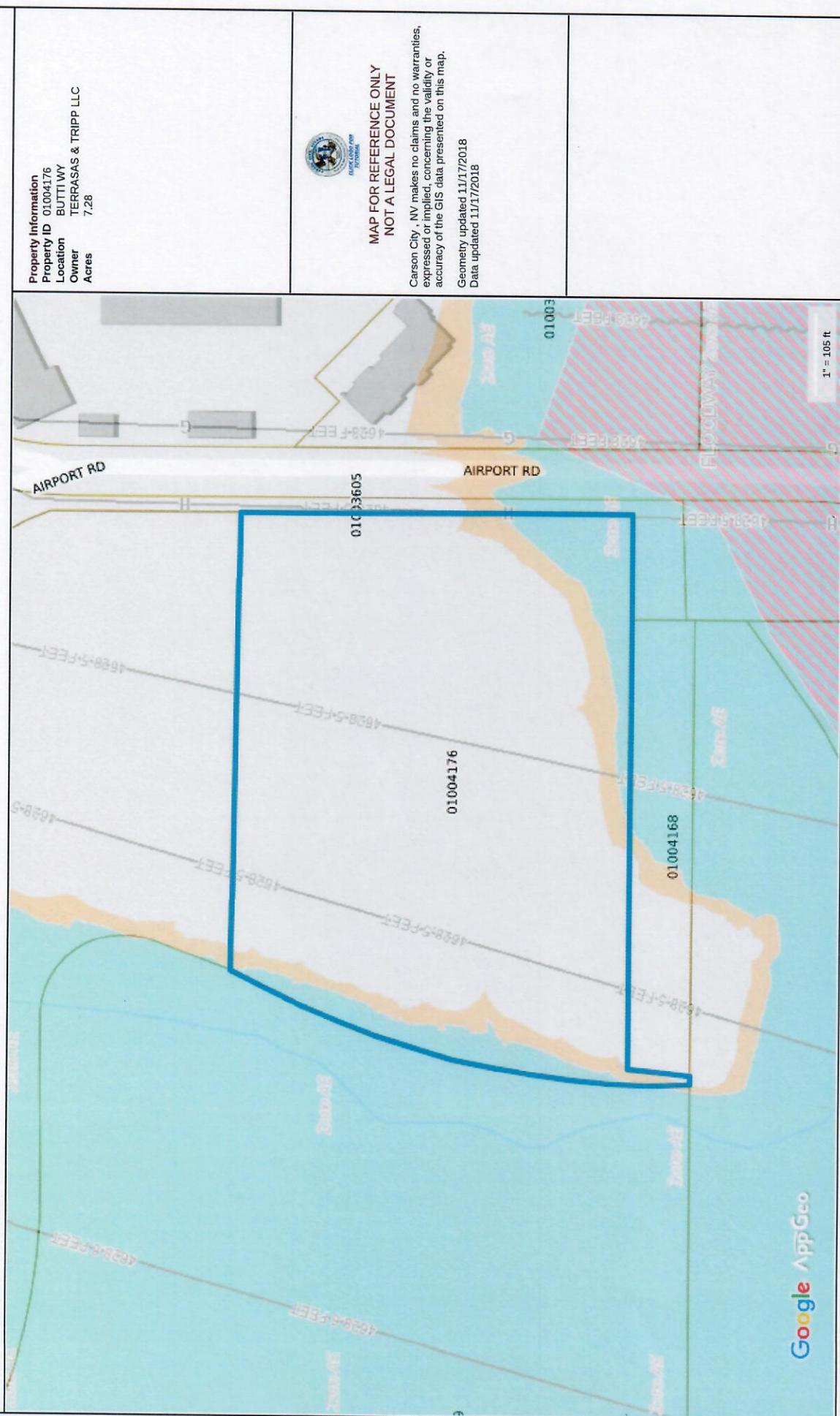
Carson City, NV makes no claims and no warranty is expressed or implied concerning the validity or accuracy of the GIS data presented on this map.

Geometry updated 11/17/2018  
Data updated 11/17/2018

$$1'' = 105 \text{ ft}$$







## Lompa Fault Line



## Property Information

Property ID 01004176  
Location BUTTI WY  
Owner SIERRA TAHOE RV & BOAT STORAGE  
Acres 7.28



[CLICK LOGO FOR  
TUTORIAL](#)

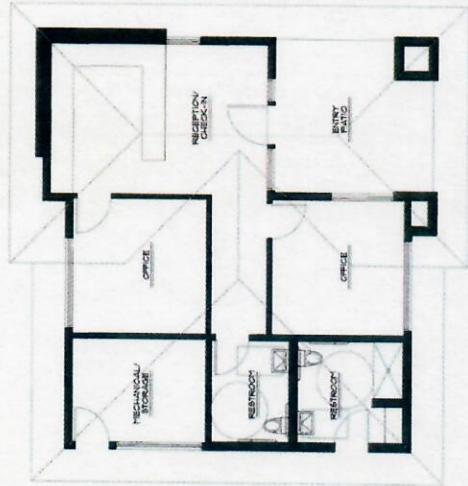
MAP FOR REFERENCE ONLY  
NOT A LEGAL DOCUMENT

Carson City , NV makes no claims and no warranties,  
expressed or implied, concerning the validity or accuracy of  
the GIS data presented on this map.

Geometry updated 11/17/2018  
Data updated 11/17/2018



LODGE PLAN & EXTERIOR BUILDING ELEVATIONS - SUP SUBMITTAL



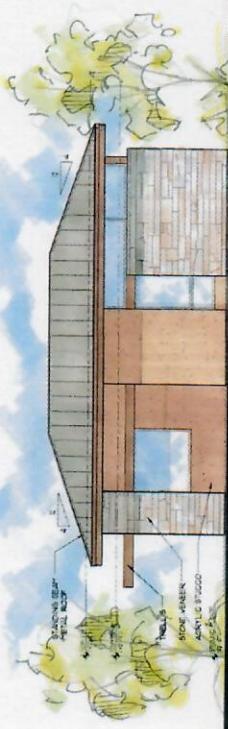
1 OFFICE FLOOR PLAN



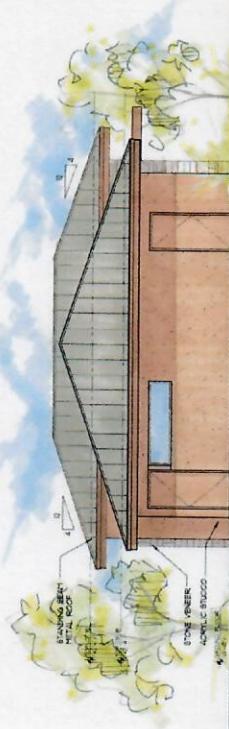
**2** **SOUTH ELEVATION**



**4** NORTH ELEVATION



3 WEST ELEVATION



5 EAST ELEVATION

### PLANT LIST:

#### LARGE/STREET TREES (2" Caliper)

BORNEA ALMOND (BUDDED) EFFICIENT NEW IRRIGATION SYSTEM.



#### ROCKFALL CLOTHESLINE

BLAKISTON

#### SMALL/ACCENT TREES:

BOHNEA

### LANDSCAPE LEGEND:

SHRUB PLANTING AREA (W/ 12" MULCH COVER) - (164,340 SF)  
SHRUB PLANTING AREA (W/ 12" MULCH COVER) - (164,340 SF)  
SHRUB PLANTING AREA (W/ 12" MULCH COVER) - (164,340 SF)

### LANDSCAPE CALCULATIONS:

#### ZONING:

GENERAL COMMERCIAL

#### GENERAL AREA:

278 AC.

#### LANDSCAPE AREA:

221,481 SF.

#### LANDSCAPE AREA REQUIRED:

65,288 SF. (20% of Impervious Area)

#### LANDSCAPE AREA PROVIDED:

65,282 SF. (20.04%)

#### TREE/HEDGE REQUIREMENTS:

1 TREE/40 SF. (= 400 TREES)

Including: 1 TREE/20 PARKING SPACES (7 SPACES = 14 TREES)

and 1 TREE/20' STREET FRONTAGE (40 FT. X 20' TREES = 8 TREES)

TOTAL TREES REQUIRED = 444 TREES

ALL TREES ARE RECOMMENDED

6 SPRINGS PER TREE REQUIRED = 2,664 SPRINGS



L.1

### PRELIMINARY LANDSCAPE PLAN

preliminary landscape plan for:  
SIERRA TAHOE RV and BOAT STORAGE  
Airport Road Carson City, NV  
APN: 010-041-76

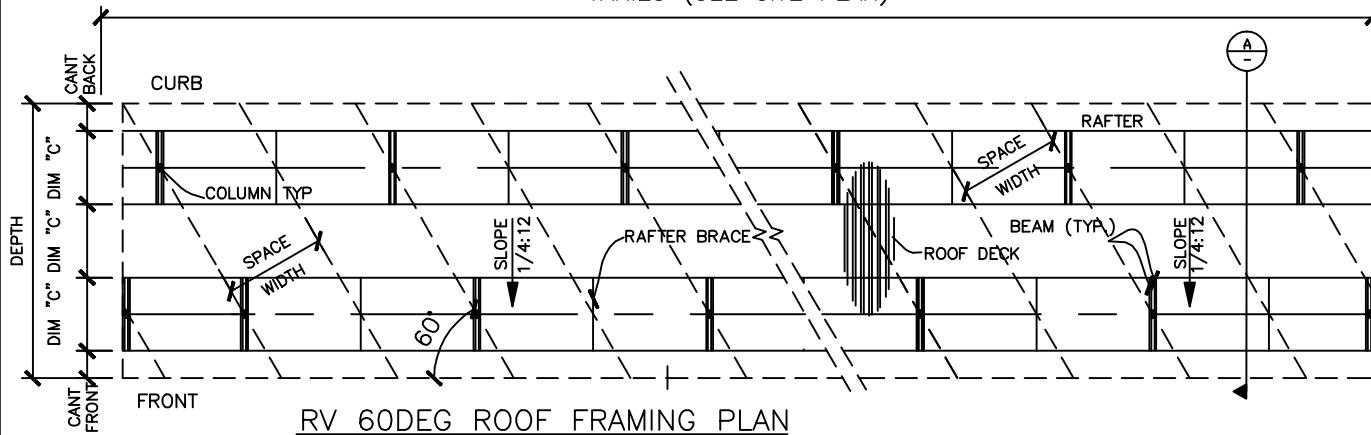


TreeHugger Studio, LLC  
landscape architecture  
and site planning services  
340 West Gaffey Drive  
Washoe Valley, NV 89704  
(775) 530-0655

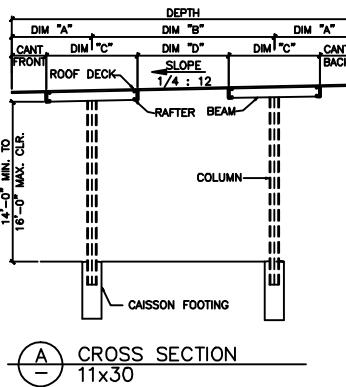
|            |    |
|------------|----|
| REVISIONS: | BY |
|            |    |



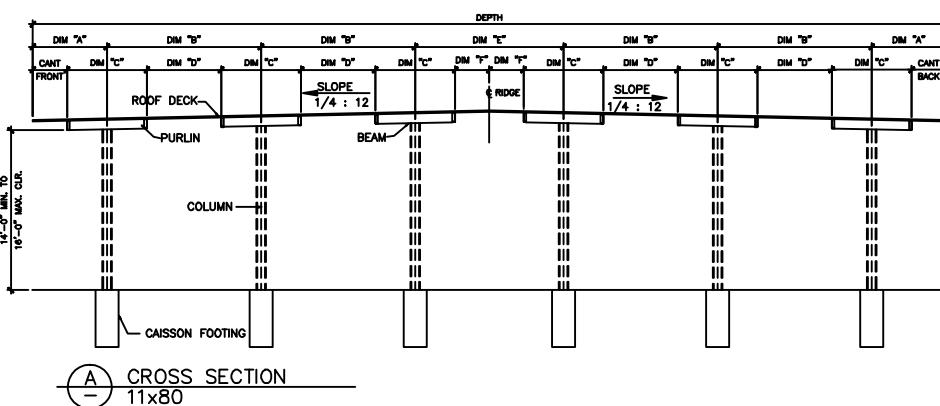
VARIES (SEE SITE PLAN)



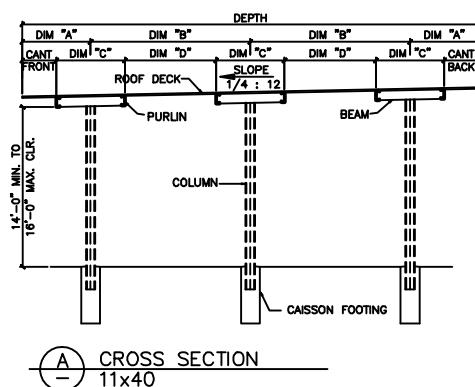
RV 60DEG ROOF FRAMING PLAN



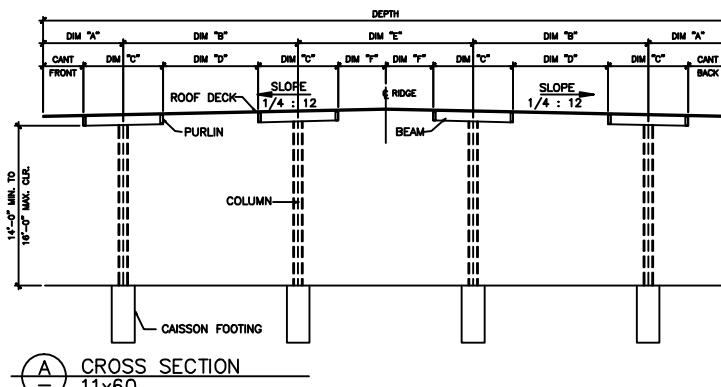
A- CROSS SECTION  
11x30



A- CROSS SECTION  
11x80



A- CROSS SECTION  
11x40



A- CROSS SECTION  
11x60

SCHEDULE

| SIZE    | DEPTH | SPACE WIDTH | "A" DIM | "B" DIM | "C" DIM | "D" DIM | "E" DIM | "F" DIM | CANT Front DIM | CANT Back DIM |
|---------|-------|-------------|---------|---------|---------|---------|---------|---------|----------------|---------------|
| 11'X30' | 30'   | 11'         | 7'      | 16'     | 8'      | 8'      |         |         | 3'             | 3'            |
| 11'X40' | 40'   | 11'         | 6'      | 14'     | 6'      | 6'      |         |         | 3'             | 3'            |
| 11'X60' | 60'   | 11'         | 7'      | 15'-4"  | 7'      | 8'-4"   | 15'-4"  | 4'-2"   | 3'-6"          | 3'-6"         |
| 11'X80' | 80'   | 11'         | 6'-6"   | 13'-6"  | 7'      | 6'-6"   | 13'-0"  | 3'      | 3'             | 3'            |

BAJA STANDARD

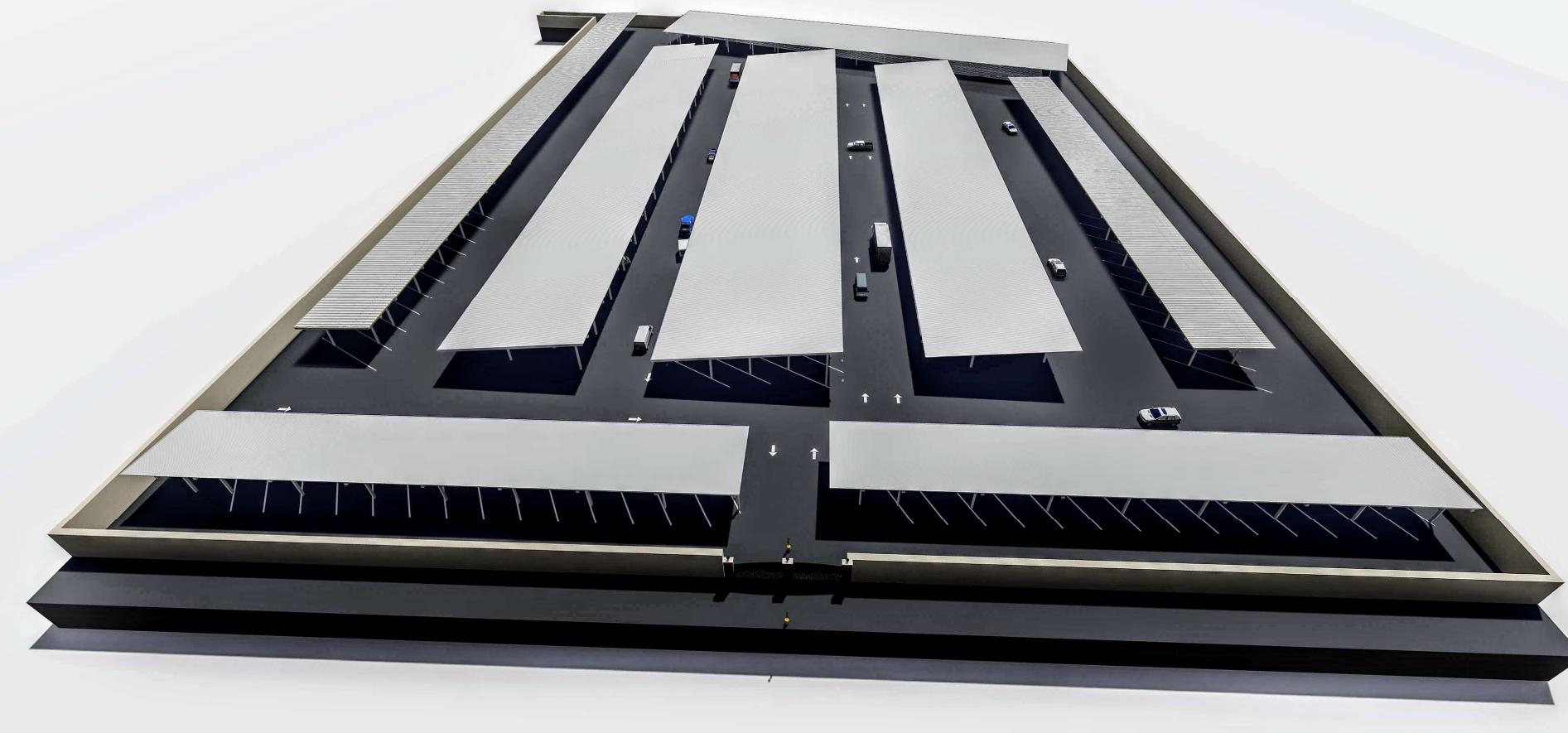
RV 60DEG STANDARD TYPES

BAJA

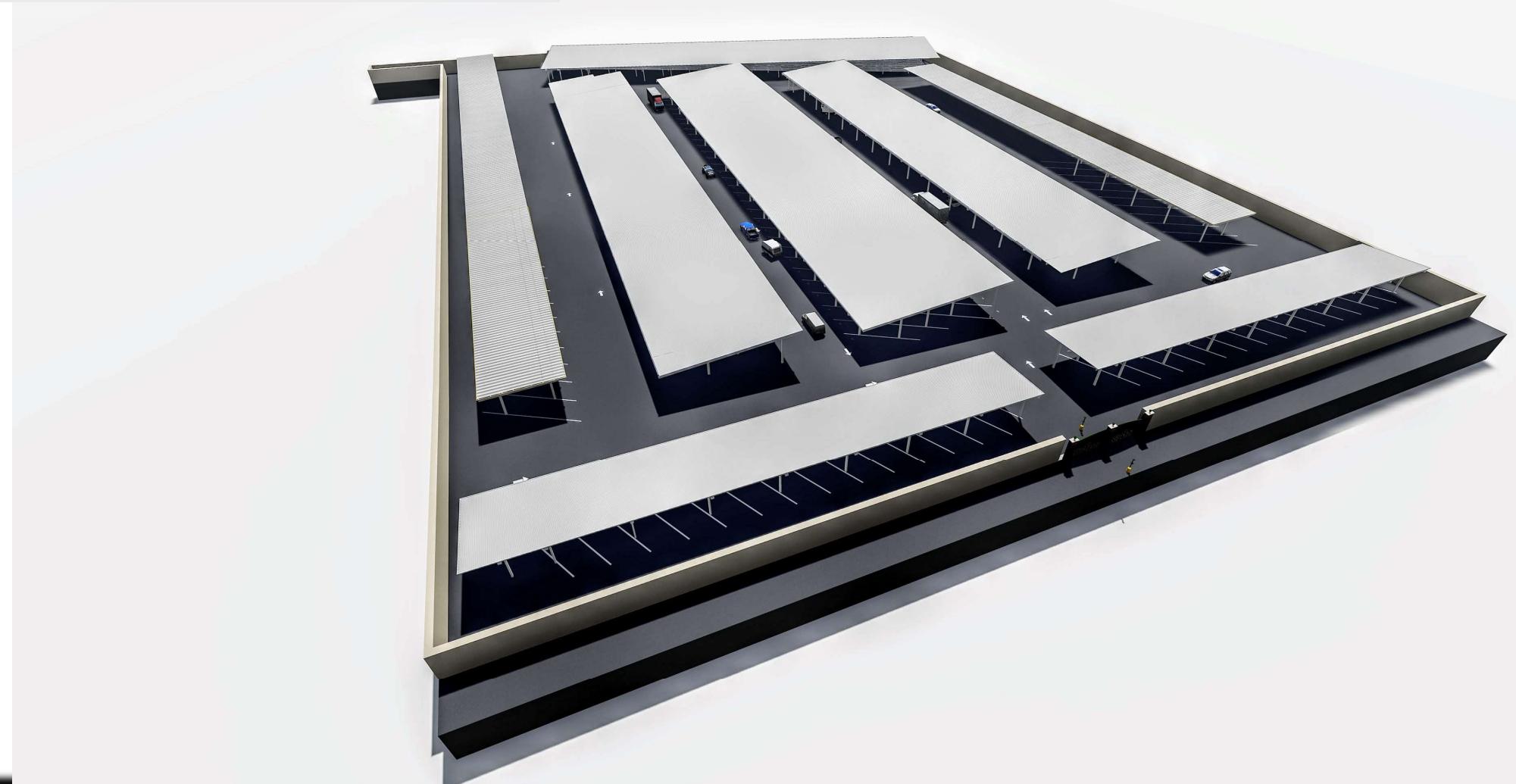
CONSTRUCTION CO., INC.  
223 FOSTER ST., MARTINEZ CA 94553  
1-800-366-9600 FAX: (925) 229-0161

SHEET

Page 1 of 2



3D RV LAYOUT (60 DEGREES)

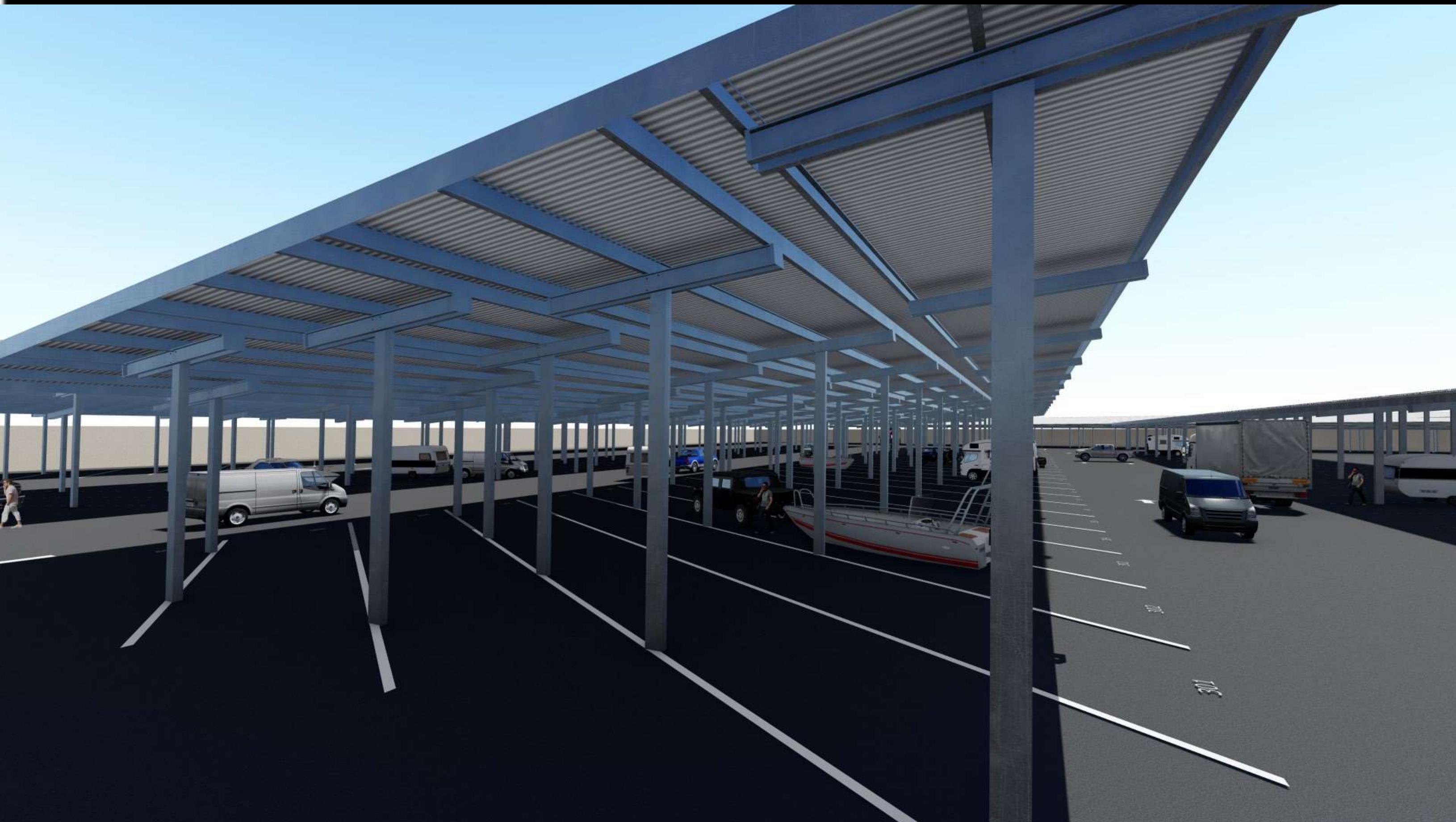


RV COVER NON SOLAR 60 DEGREES













800 Glendale Avenue • P.O. Box 855 • Sparks, Nevada 89432 • Fax #: (775)358-7197 • Telephone (775)358-8680

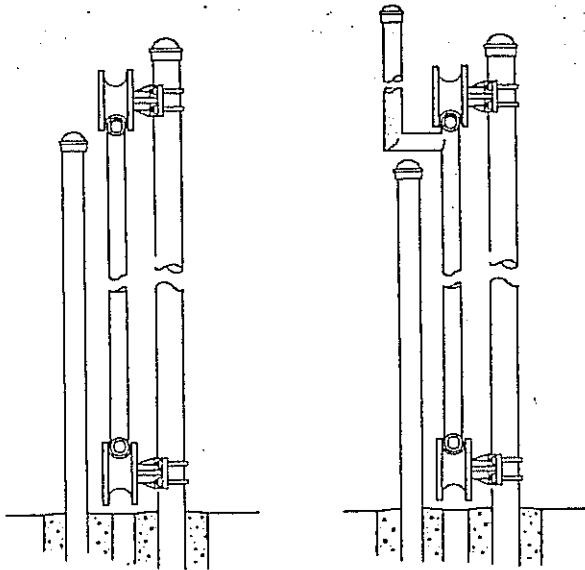
# CANTILEVER GATES

## Commercial & Industrial

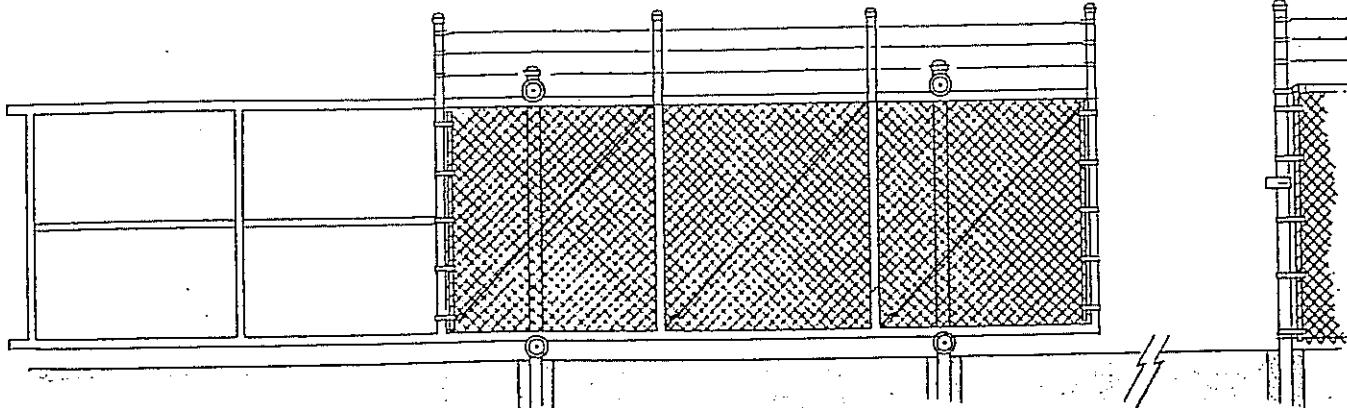
*(Made to customer specifications)*

CANTILEVER GATE PRICING BASED ON  
STYLES SHOWN BELOW AND DOES NOT  
INCLUDE HARDWARE.

| OPENING   | OVERALL<br>HEIGHT |
|-----------|-------------------|
| 5' - 8'   | 5' - 8'           |
| 8' - 12'  | 5' - 8'           |
| 12' - 15' | 5' - 8'           |
| 15' - 20' | 5' - 8'           |
| 20' - 24' | 5' - 8'           |



### SINGLE GATES TO 15' OPENING



CONSTRUCTION: 2" FW/DQ HORIZONTAL RAIL  
1½" FW/DQ VERTICAL BRACE  
1½" STRUCTURAL/CQ COUNTER BALANCE

TRUSS ROD DIAGONAL BRACE  
9 GAUGE FILL  
2" FW/DQ SAFETY POST



# VIEWGUARD® Slatted Fence

**V**iewguard® and Viewguard Plus® are nationally recognized as the premier privacy fencing products on the market today. An attractive, cost effective and extremely durable alternative to wood-slatted fencing, Viewguard® products are perfect for a wide variety of fencing needs. The High Density Polyethylene slats come in a wide range of colors. The colors shown in the brochure are stocked by Security Contractor Services. However, many more colors are available. See your SCS representative for a full line.

## All ViewGuard® and ViewGuard Plus® slatted fences include:

- *Comprehensive Manufacturing Process*

Each 2-3/8" slat is inserted vertically and individually secured to the wire with a stainless steel staple. This process ensures the stability and integrity of each individual component. VIEWGUARD® is also available using PVC coated Bonded (fusion adhered) wire to match the plastic slats.

- *Durable Quality Material*

Our standard VIEWGUARD® and VIEWGUARD PLUS® with galvanized wire is woven using 9 ga. weaving wire (.148" diam.) with an average of 1.2 oz. Zinc coating per square foot. The diamond size needed to accommodate the 2-3/8" slats is 3-1/2" x 5-1/2".

- *Weather Resistant*

Made from a high density polyethylene blend, our UV stabilized wide slats (2-3/8") are resistant to chemicals, salt and petroleum products.

- *Multiple Uses*

With the privacy that VIEWGUARD® and VIEWGUARD PLUS® have to offer, in addition to its durability, it is a natural application for commercial, industrial and residential environments.

- *Easy Maintenance*

Because of the VIEWGUARD® and VIEWGUARD PLUS® high density polyethylene finish, its natural, clean look can always be restored by rain or one quick rinse with a garden hose.

- *Made to Order*

8 ga. Vinyl Coated Fused Bonded Wire is available in Black, Green, Brown, Beige and White.

You can order any of our eight standard colors (featured below) or design your own color scheme by using an infinite number of combinations with our various colors.

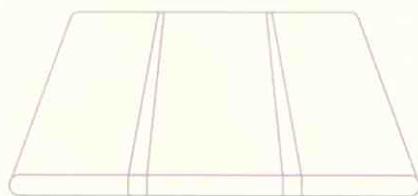


# VIEWGUARD

*fenning* *plus*

## VIEWGUARD

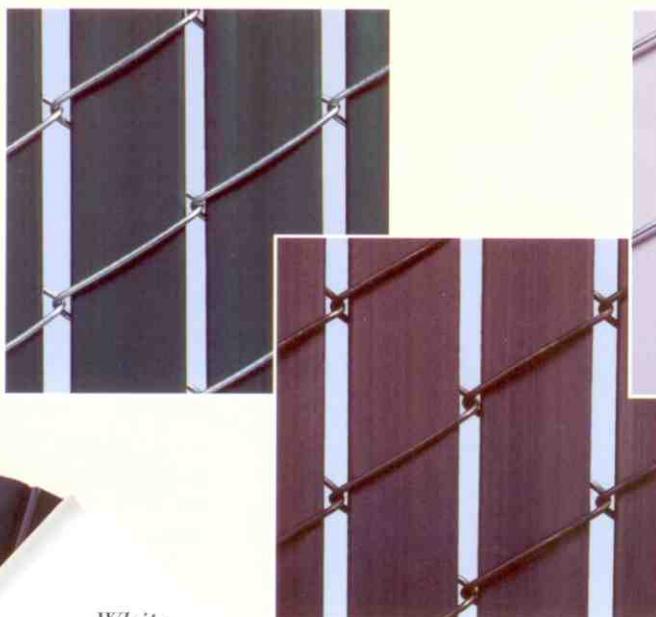
Isometric View



Section View



Galvanized Wire



*Black*

*White*

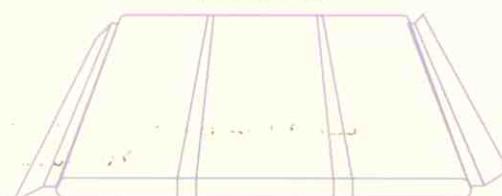
*Beige*

*Green*

Bonded Wire

## VIEWGUARD *plus*

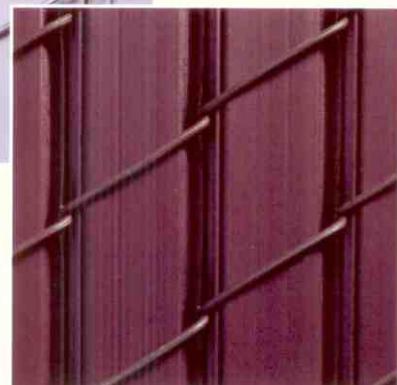
Isometric View



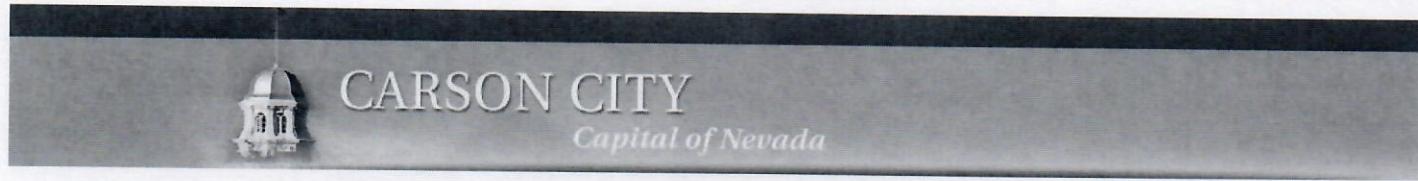
Section View



Galvanized Wire



Bonded Wire

[Treasurer Home](#)[Assessor Data Inquiry](#)[Back to Last Page](#)

### Secured Tax Inquiry Detail for Parcel # 010-041-76

Property Location: BUTTI WY  
 Billed to: TERRASAS & TRIPP LLC  
 250 GREG ST  
 SPARKS, NV 89431-0000

Tax Year: 2018-19  
 Roll #: 017689  
 District: 2.4  
 Tax Service:  
 Land Use Code: 160

[Code Table](#)

Current Owner: SIERRA TAHOE RV & BOAT STORAGE  
 892 SOUTHWOOD BLVD  
 INCLINE VILLAGE, NV 89451-0000

#### Outstanding Taxes:

| Prior Year          | Tax              | Penalty/Interest | Total            | Amount Paid      | Total Due |
|---------------------|------------------|------------------|------------------|------------------|-----------|
| <b>Current Year</b> |                  |                  |                  |                  |           |
| 08/20/18            | 3,005.29         |                  | 3,005.29         | 3,005.29         | .00       |
| 10/01/18            | 3,003.00         |                  | 3,003.00         | 3,003.00         | .00       |
| 01/07/19            | 3,003.00         |                  | 3,003.00         | 3,003.00         | .00       |
| 03/04/19            | 3,003.00         |                  | 3,003.00         | 3,003.00         | .00       |
| <b>Totals:</b>      | <b>12,014.29</b> | <b>.00</b>       | <b>12,014.29</b> | <b>12,014.29</b> |           |

[Payment Cart](#)[History](#)

#### Additional Information

|                 | 2018-19 | 2017-18 | 2016-17 | 2015-16 | 2014-15 |
|-----------------|---------|---------|---------|---------|---------|
| Tax Rate        | 3.5700  | 3.5700  | 3.5200  | 3.5200  | 3.5400  |
| Tax Cap Percent | 4.2     | 2.6     | .2      | 3.2     | 3.0     |

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**Reports**



Carson City  
Community Development Department  
108 E. Proctor Street  
Carson City, NV 89701

May 16, 2019

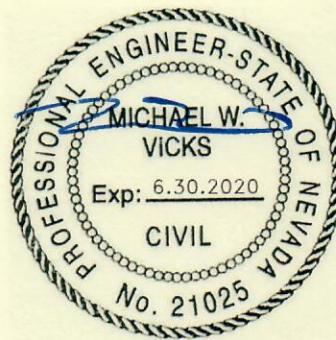
RE: Sierra Tahoe RV & Boat Storage – Traffic Letter

The Sierra Tahoe RV & Boat Storage is located on Airport Road (APN: 010-041-76). The site is currently undeveloped and is part of the Lompa Ranch Development. Site improvements will include covered recreational vehicle parking ( $\pm 252$  spaces), a manager's office, a sanitary waste dump station and a wash bay. The proposed use is not listed in the ITE Trip Generation Manual. In order to estimate trip generation, the use has been analyzed as a mini-warehouse which is believed to be a conservative estimate. Based on the proposed unit count the ITE Trip Generation Manual (9<sup>th</sup> Edition) the project will generate 63 daily trips with a peak of 5 trips in both the AM and PM periods falling below Carson City's threshold for a full traffic study. No additional traffic study has been performed.

Please contact Monte Vista Consulting if you have any questions or if there is anything else I can help with.

Sincerely,  
Monte Vista Consulting

Michael Vicks, P.E.  
Principal



MAY 15 2019



Carson City  
Community Development Department  
108 E. Proctor Street  
Carson City, NV 89701

May 23, 2019

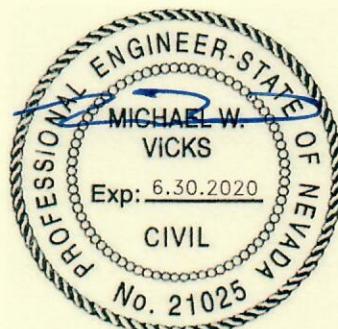
RE: Sierra Tahoe RV & Boat Storage – Parking Justification Letter

The Sierra Tahoe RV & Boat Storage is located on Airport Road (APN: 010-041-76). The site is currently undeveloped and is part of the Lompa Ranch Development. Site improvements will include covered recreational vehicle parking ( $\pm 252$  spaces), a manager's office, a sanitary waste dump station and a wash bay. The proposed use is not listed in the ITE Parking Generation Manual. In order to estimate trip generation, the use has been analyzed as a mini-warehouse which is believed to be a conservative estimate. Based on the proposed unit count the ITE Parking Generation Manual (4<sup>th</sup> Edition) the project will require 4 spaces. The proposed project will include 7 parking spaces and will also have oversized drive isles which would allow for additional temporary parking in the drive isles if required. No additional parking study has been performed. Based on similar operations, the proposed site layout provides more than adequate parking.

Please contact Monte Vista Consulting if you have any questions or if there is anything else I can help with.

Sincerely,  
Monte Vista Consulting

Michael Vicks, P.E.  
Principal



MAY 23 2019



Carson City  
Community Development Department  
108 E. Proctor Street  
Carson City, NV 89701

May 16, 2019

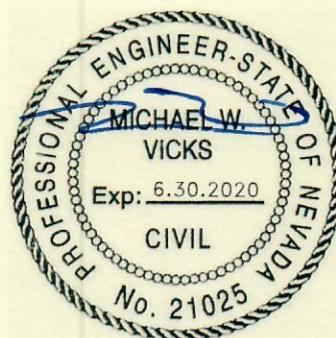
RE: Sierra Tahoe RV & Boat Storage – Sanitary Sewer Letter

The Sierra Tahoe RV & Boat Storage is located on Airport Road (APN: 010-041-76). The site is currently undeveloped and is part of the Lompa Ranch Development. Site improvements will include covered recreational vehicle parking ( $\pm 252$  spaces), a manager's office, a sanitary waste dump station and a wash bay (Both the dump station and wash bay will be restricted use for customers only). No sanitary sewer infrastructure is extended to the site, however, the main interceptor does run along the western boundary of the property. In order to provide sanitary sewer service, a new main will be extended from an existing manhole on the western side of the site and then a private lateral will be constructed. The threshold for a full sanitary sewer report is 200 drainage fixture units daily (5,000 gallons). The office will have (2) bathrooms and a shower which will equate to 10 fixture units. Based on discussions with Carson City Development Engineering, the average usage over a summer month would be utilized in determining the approximate peak contribution. The owners have reached out to several other facilities with a similar business model and estimated that they would anticipate 5.0 sewer dumps per day and a similar usage for the wash bay. Considering the average RV has a capacity of 100 gallons or less of waste water, it is estimated that the project is likely to contribute less than 500 gallons (25 fixture units) of waste water to the sanitary sewer system daily during summer months. No additional analysis of the existing offsite sanitary sewer system has been completed.

Please contact Monte Vista Consulting if you have any questions or if there is anything else I can help with.

Sincerely,  
Monte Vista Consulting

Michael Vicks, P.E.  
Principal



MAY 15 2019



Carson City  
Community Development Department  
108 E. Proctor Street  
Carson City, NV 89701

May 16, 2019

RE: Sierra Tahoe RV & Boat Storage – Drainage Letter

The Sierra Tahoe RV & Boat Storage is located on Airport Road (APN: 010-041-76). The site is currently undeveloped and is part of the Lompa Ranch Development. Site improvements will include covered recreational vehicle parking ( $\pm 252$  spaces), a manager's office, a sanitary waste dump station and a wash bay. The site is located adjacent to community detention ponds which are directly to the south and west of the site. Portions of the development are located in FEMA flood Zone AE. Those portions of the project will be elevated in a manner to bring them out of the flood plain and will be officially removed from the flood plain with a LOMA prior to Certificate of Occupancy.

Based on the findings of the Major Project Review and subsequent meetings with Carson City Public Works, it has been determined no additional onsite detention will be required for the development and it can drain directly into the ponds to the south. The development will increase impervious coverage by approximately 233,000 s.f. (5.34 acres). Utilizing the Rational Method the increased impervious area results in anticipated additional site contributions to the adjacent detention facilities of:

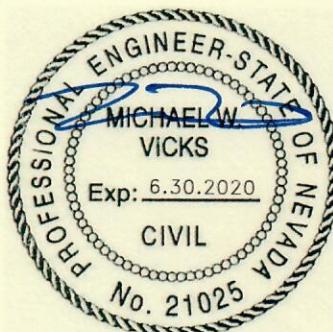
5-Year Storm Event = 7.34 cfs  
100-Year Storm Event = 13.50 cfs

This flow will be treated through a rock lined swale prior to entering the ponds, however little infiltration is anticipated. In order expand the available storage in the adjacent ponds, the grading of the site has been modified to allow for deeper cuts on the adjacent property which is owned by Carson City.

Please contact Monte Vista Consulting if you have any questions or if there is anything else I can help with.

Sincerely,  
Monte Vista Consulting

Michael Vicks, P.E.  
Principal



MAY 15 2019

March 13, 2017

# Feasibility Geotechnical Investigation

## LOMPA RANCH BOAT AND RV STORAGE PROJECT Carson City, Nevada



### Prepared For:

Terasas and Tripp  
250 Greg Street  
Reno, Nevada 89431

### Prepared By:

Resource Concepts, Inc.  
340 N. Minnesota St.  
Carson City, Nevada 89703



March 13, 2017

Frank Terasas  
Terasas and Tripp  
250 Greg Street  
Reno, NV 89431

**Subject: Feasibility Geotechnical Investigation for the Lompa Ranch Boat and RV Storage Project,  
Butti Way, Carson City, Nevada APN 010-041-76**

Dear Mr. Terasas:

In accordance with your authorization, we have prepared our "Feasibility Site Geotechnical Investigation" report for the subject site titled "Lompa Ranch and RV Storage Project", located on Butti Way, Carson City, Nevada APN 010-041-76. The purpose of our investigation was to provide information regarding the constructability of the proposed project on the site from geotechnical and geo-hazards perspectives.

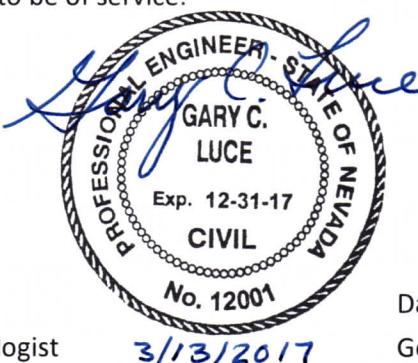
The accompanying report presents our findings and conclusions from our investigation. The project site appears to be generally well suited, except as noted below, to the proposed commercial use as described herein. Liquefaction potential has been indicated on previous geologic mapping reviewed for this investigation and should be further assessed by a site specific geotechnical investigation. Liquefaction potential is common throughout Carson City and is routinely addressed during investigations. This report is not intended to take the place of a site specific geotechnical investigation.

Please contact us if you have any questions concerning the contents of this report. We look forward to reviewing the project plans as they develop further and providing additional geotechnical engineering consultation as needed.

Thank you for the opportunity to be of service.

Resource Concepts, Inc.

Gary C. Luce, PE  
Sr. Geotechnical Engineer/Geologist



  
David Edgington, EI  
Geotechnical Engineering Intern

Enclosures: Figures 1 Through 5  
Appendix A- Field Investigation and Laboratory Tests Results  
Sample Grading Specification

**CARSON CITY**  
340 North Minnesota St.  
Carson City, NV 89703-4152  
775 / 883-1600 • fax: 775 / 883-1656

**Engineering • Surveying • Water Rights  
Resource & Environmental Services**  
[www.rci-nv.com](http://www.rci-nv.com)

**ZEPHYR COVE**  
212 Elks Pt. Rd., Ste. 443 • PO Box 11796  
Zephyr Cove, NV 89448-8020  
775 / 588-7500 • fax: 775 / 589-6333

# FEASIBILITY GEOTECHNICAL INVESTIGATION LOMPA RANCH BOAT AND RV STORAGE PROJECT

**APN 010-041-76**

**Butti Way  
Carson City, Nevada**

## **1.0 PURPOSE AND SCOPE**

This report presents the results of Resource Concepts Inc. (RCI) Feasibility Site Geotechnical Investigation for the proposed Lompa Ranch and RV Storage Project. The subject parcels are identified by the Carson City County Assessor as APN 010-041-76. The proposed development area totals approximately 7.25 acres located on Butti Way in Carson County, Nevada. It is our understanding that the parcel will have vehicle access from Butti Way as shown on the Vicinity Map, Figure 1. The topography of the site can be generally characterized as flat lying with gentle to moderate slopes to the south and west. The property has an average elevation of approximately 4630 feet. The preliminary layout of the proposed development is presented on Figure 2, the project Site Plan.

For the preparation of this report, we reviewed the following documents:

- *BAJA Construction Company Inc., Preliminary Site Plan, RV Enclosed and Canopy January 17, 2017*
- *Black Eagle Consultants Inc., Geotechnical Investigation, (July 2005), Carson City Freeway, Phase 2A.*
- *Geocon Consultants Inc., (October 2010), Robinson Transmission Main Geotechnical Investigation.*
- *Katzer, T. (1980), Carson City Quadrangle, General Groundwater Map, Nevada Bureau of Mines and Geology, Scale 1:24,000.*
- *Katzer, T. and Schroer, C.V (1981), Carson City Quadrangle, Flood and Related Debris Flow Hazards Map, Nevada Bureau of Mines and Geology, Scale 1:24,000.*
- *Natural Resources Conservation Service Website, Soil Survey of Carson City Area, Nevada, (<http://websoilsurvey.sc.egov.usda.gov/App/HomePage.htm>).*

- *Resource Concepts Inc., Geotechnical Investigation for the Carson City Animal Shelter, Butti Way, Carson City, Nevada, January 20, 2015*
- *Trexler, D.T. (1977), Carson City Folio Geologic Map, Nevada Bureau of Mines and Geology, Carson City 7.5' Minute Quadrangle, Nevada, Scale 1:24,000.*
- *Trexler, D.T. and Bell, J.W., (1979), Carson City Quadrangle, Earthquake Hazards Map, Nevada Bureau of Mines and Geology, Scale 1:24,000.*

RCI's scope of work consisted of a limited field investigation and summarizing published information regarding soil conditions, geologic hazards, and site geology. The published NRCS soils information included in our site review only extends to five feet below the surface. Our limited field investigation included a limited number of exploration test pits across the site to provide additional confidence in the reliability of the published soil information and to extend the soils information depth to approximately 10 feet for consideration of footing bearing and settlement concerns and utility installation criteria.

Resource Concepts, Inc. (RCI) performed the following scope of geotechnical services:

- Observed the excavation of five exploratory test pits (TP-1 through TP-5) at the site. The test pits were excavated using a Cat Backhoe equipped with a 24-inch bucket to depths of approximately ten feet below the existing ground surface (bgs). The approximate locations of the exploratory test pits are depicted on the Site Plan, Figure 2.
- Logged the test pits in general accordance with the Unified Soil Classification System (USCS).
- Obtained bulk samples from the test pits. Logs of the exploratory test pits and other details of the field investigation are included in Appendix A.
- Submitted selected soil samples for geotechnical laboratory testing.

## 2.0 REGIONAL GEOLOGY

The geology of the site is referenced from Carson City Folio Geologic Map (Trexler, 1977). Carson City lies within a large fault bounded valley referred to as Eagle Valley. The valley area is typical of the western edge of the Great Basin geomorphic province. The geologic map indicates the project site area is predominantly underlain by Quaternary alluvial plain deposits. These deposits are described by Trexler (1977) as yellowish brown to gray, unstratified to poorly bedded, poorly to moderately sorted fine silty sand, sandy silt, granular clayey coarse sand, and minor sandy gravel. The alluvial plain deposits are on the order of 2,000 feet deep in the Eagle Valley basin based on geophysical data.

Intrusive and extrusive igneous rocks form the mountains that surround the valleys in the Carson City region. These mountains provide the source for sediments that form the alluvial deposits underlying the valley and that form the modern alluvial fans that ring the valleys. The geology of the site and surrounding areas is depicted on the Geology Map, Figure 3.

### **3.0 SOIL CONDITIONS**

The soil conditions observed in the five exploratory test pits excavated at the site are relatively consistent with the Natural Resources Conservation Service (NRCS) mapping and related data. The NRCS data is limited to the top five feet of soil. The soil units mapped by NRCS on the site are shown on the Soil Map, Figure 4. The soils observed in our test pits to a depth of five feet bgs consist of alluvial deposits of loose to medium dense, moist to wet, interbedded poorly graded sand (SP), Silty Sand (SM) and clayey sand (SC). Soils observed and sampled in the five test pits from approximately five to eleven feet below ground surface (bgs) consisted of loose to medium dense, moist to wet, poorly to well graded, Silty Sand with gravel (SM, SM-SW and SP-SM). Test results on representative samples are attached in Appendix A.

### **4.0 GROUNDWATER CONDITIONS**

At the time of our investigation, March, 2017, Carson City has experienced a record amount precipitation. Precipitation records indicate over 25 inches since October 1, 2016 which is almost double the yearly average. Our field measurements are likely to reflect the approximate high groundwater level on the site. During the spring melting period these levels may be anticipated to continue to rise by a foot or more.

Groundwater depth in two of our five test pits (TP-01 and TP-02) ranged from 8 to 10 feet. The groundwater in this interval is contained in medium to coarse grained sand aquifers. Review of the General Groundwater Map of the Carson City Quadrangle indicates permanent groundwater levels of 20 to 30 feet below existing grade for the May to June 1976 period. Variations in rainfall, snowmelt, temperature, and other factors can cause fluctuations in the level of groundwater. Groundwater flow in the project site area is generally to the southeast toward the Carson River.

### **5.0 GEOLOGIC HAZARDS**

#### **Faulting**

The project site as all of Carson City is located near active faults which are considered capable of producing significant ground motions due to seismic events. Based on the Nevada Bureau of Mines and Geology and the USGS Quaternary Fault Fold Database, there are no known active faults (Holocene-age, exhibiting displacement within the last 11,000 years) crossing the subject site. Therefore the risk of fault ground rupture at the site is considered low.

A single fault is mapped across the west site of the site. The mapped fault is considered older than would be considered active for the proposed development. Unoccupied projects such as proposed are recommend uses in areas subject to potential ground ruptures and therefore we do not recommend a site specific investigation. The Fault Map, Figure 5 is attached showing the distribution of faults in the vicinity of the site and the Carson City area.

#### **Seismic Hazard Analysis**

The Nevada Bureau of Mines has evaluated faulting along the Carson Range and Geology to be capable of producing earthquake Richter Magnitudes of approximately 7.3 with peak ground

accelerations of approximately 2.0g. These values are equivalent to Modified Mercalli Intensities of X (ten) or greater. The seismic risk at the site is not significantly greater than that of the surrounding developments and the Carson City area in general. We recommend that seismic design of the structures be in accordance with the latest version of the International Building Code (IBC) and the American Society of Civil Engineers (ASCE) Standard 7-10. For preliminary design and cost estimations an IBC Site Class of "D" or stiff soil should be employed. Final design should be based on a site specific geotechnical investigation.

### **Liquefaction**

No specific investigation for the possibility of liquefaction at the site was performed. Liquefaction of granular soils can be caused by strong vibratory motion due to earthquakes. Soils that are highly susceptible to liquefaction are loose, granular and saturated. Liquefaction of soils may cause surface distress, loss of bearing capacity, and settlement of structures.

Mapping by the Nevada Bureau of Mines and Geology shows the site to be in an area of moderate severity of shaking where groundwater is generally greater than ten feet below the surface. Areas such as this may be susceptible to liquefaction due to strong shaking caused by large seismic events.

We know of no residential or commercial buildings in this portion of Carson City to have deep foundations due to liquefaction concerns. To our knowledge the buildings surrounding the site are built on conventional shallow foundations.

### **Flooding and Debris Flow**

Review of the FIRM map 320001011G issued on December 22, 2016 indicates that the site is bound by areas within the 0.2 percent annual chance of flooding. The site itself is listed on the FIRM map as an area of minimal flooding. The scope of our work does not include any evaluation or validity of the map. The owner will need to further evaluate the site conditions and determine if the project will need to have additional design or drainage requirements for the project.

## **5.0 CONCLUSIONS AND RECOMMENDATIONS**

### **General**

Based on the results of our preliminary investigation, site soils are generally granular, cohesionless varieties and are well suited for the support of the type of commercial structures we would anticipate being constructed on the site. A thin (< 1 foot) clay layer was identified on the upper portion of the site. This soils should be avoided for direct support of foundations or pavements. Due to the general location and thickness of the clay layer, it will likely be largely removed during clearing and grubbing as well as mass grading of the site. We anticipate that to level the site approximately one to three feet of cut is likely. Deposition of the clay materials in the lower portions of fills or outside of building areas is an effective means of mitigation.

The only potential geologic hazard identified on the site is liquefaction which should be further addressed by a site specific geotechnical investigation. Liquefaction hazards are common in the

Carson City area on low lying sites. Determination of the liquefaction potential and the need for any mitigation can be determined through drilling and subsequent routine geotechnical analyses.

### **Site Grading**

Site grading should be able to be accomplished with moderate size equipment suitable for handling medium dense to dense sandy soils.

Earthwork operations should be observed and compacted fill tested for density. All fill placed within the structural areas should meet the structural fill requirements of the most recent edition of the *Standard Specification for Public Works*.

Attached to this report is a sample grading specification which is intended for planning and cost estimating purposes only. Specific grading recommendations should be developed as part of a site specific geotechnical investigation.

### **Preliminary Foundation Design Criteria**

Conventional foundations are likely to consist of continuous perimeter spread footings and or column footings. Frost depth in Carson City is 24 inches below adjacent exterior grade. For preliminary cost estimates the IBC presumptive bearing capacity of 1,500 psf with lateral bearing of 150 psf/ft below natural grade and a coefficient of friction of 0.25 are recommended. A site specific investigation in our opinion may result in some increase to these values.

### **Slabs on Grade and Pavements**

Site soils, except clay soils as noted above, are well suited to the support of slab on grade and flexible pavement construction. It should be noted that placement conditions of concrete are often adverse due to the dry climate, propensity for winds and hot and cold seasons. Mitigations measures have been developed by local contractors but caution should be used especially if using contractors from outside of the area. Due to freeze thaw extremes at the elevation of Carson City, exterior concrete should be air entrained, as is required in the Standard Specifications for Public Works Construction, with from 4.5% to 7.0% entrained air content. The water cement ratio for all exterior concrete should be 0.45 or less. A mid-range plasticizer is commonly used to facilitate the finishing process while maintaining the desired water cement ratio. Exterior concrete should be placed and finished in accordance with American Concrete Institute (ACI) recommendations for concrete placed in areas subject to freeze-thaw environments.

## **6.0 LIMITATIONS AND UNIFORMITY OF CONDITIONS**

The recommendations of this report pertain only to the limits of the site as identified on the Site Plan and are based upon the assumption that the soil conditions do not significantly deviate from those disclosed in the investigation. This report is not intended to take the place of a design specific geotechnical investigation for the project area. The evaluation or identification of the potential presence of hazardous or corrosive materials was not part of the scope of services provided by RCI.

The findings of this report are valid as of the present date. However, changes in the conditions of a property can occur with the passage of time, whether they be due to natural processes or the works

of man on this or adjacent properties. In addition, changes in applicable or appropriate standards may occur, whether they result from legislation or the broadening of knowledge. Accordingly, the findings of this report may be invalidated wholly or partially by changes outside our control. Therefore, this report is subject to review and should not be relied upon after a period of three years or after the acceptance of a newer version of the building code by Carson City.

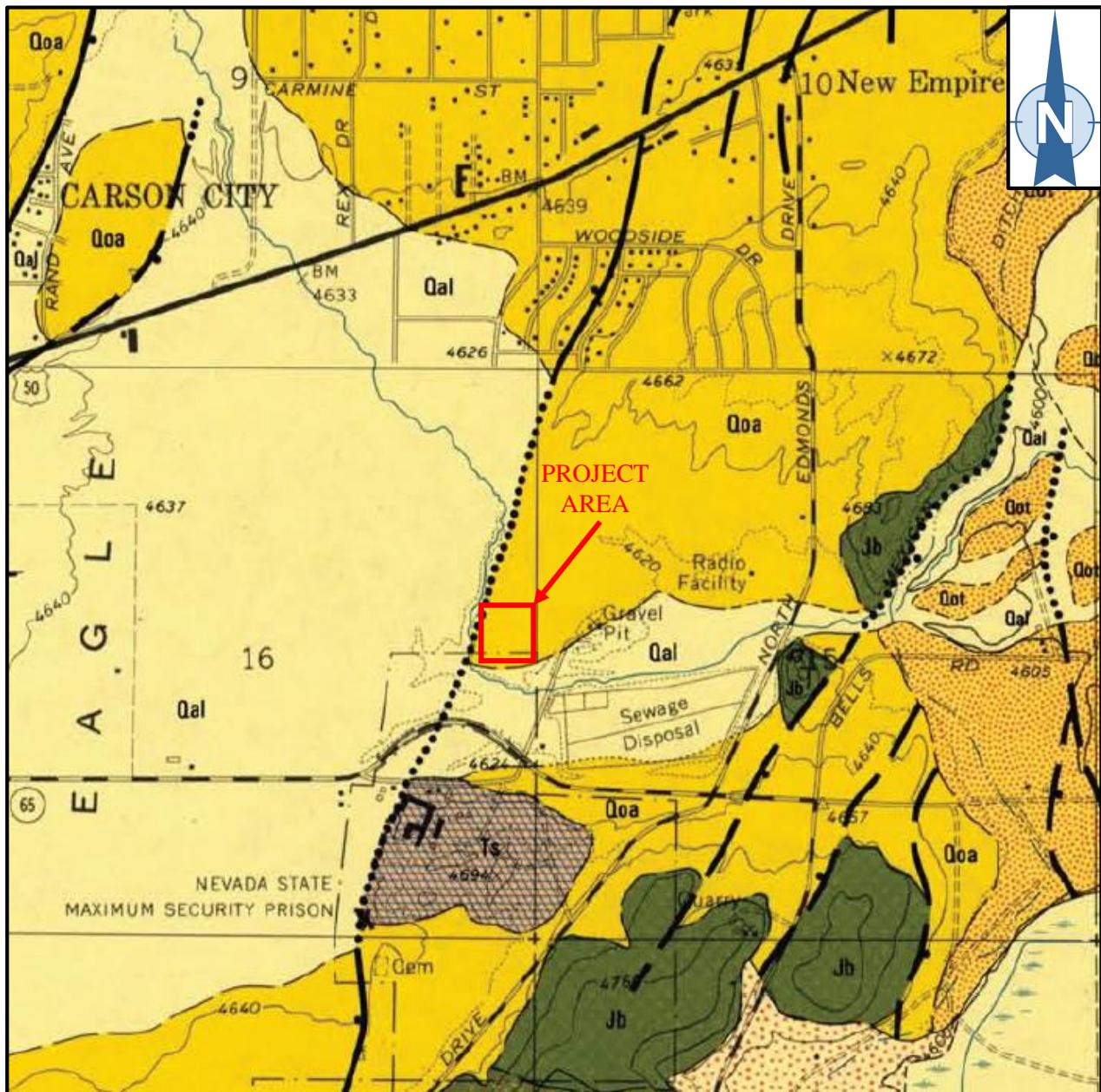
Our professional services were performed, our findings obtained, and our recommendations prepared in accordance with generally accepted geotechnical engineering principles and practices used in Carson City, Nevada in March of 2017. This warranty is in lieu of all other warranties, either expressed or implied.



340 N. Minnesota St.  
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**FIGURE 1 VICINITY MAP**  
**LOMPA RANCH**  
**BOAT AND RV STORAGE PROJECT**  
**CARSON CITY, NV**  
PROJECT NO. 16-302.1



Qoa- Older Alluvial-Plain Deposits

Qal- Alluvial Plain Deposits

Ts- Sedimentary Rocks

Jb- Metavolcanic Breccia

FAULT

INFERRED FAULT LOCATION

Map Reference: *Nevada Bureau of Mines and Geology, 1977*  
*U.S. Geological Survey, New Empire 7 1/2' quadrangle*



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**FIGURE 3 GEOLOGIC MAP**  
**LOMPA RANCH**  
**BOAT AND RV STORAGE PROJECT**  
**CARSON CITY, NV**

PROJECT NO. 16-302.1



■ TEST PIT LOCATIONS OBSERVED BY RESOURCE CONCEPTS, INC. ON MARCH 7, 2017

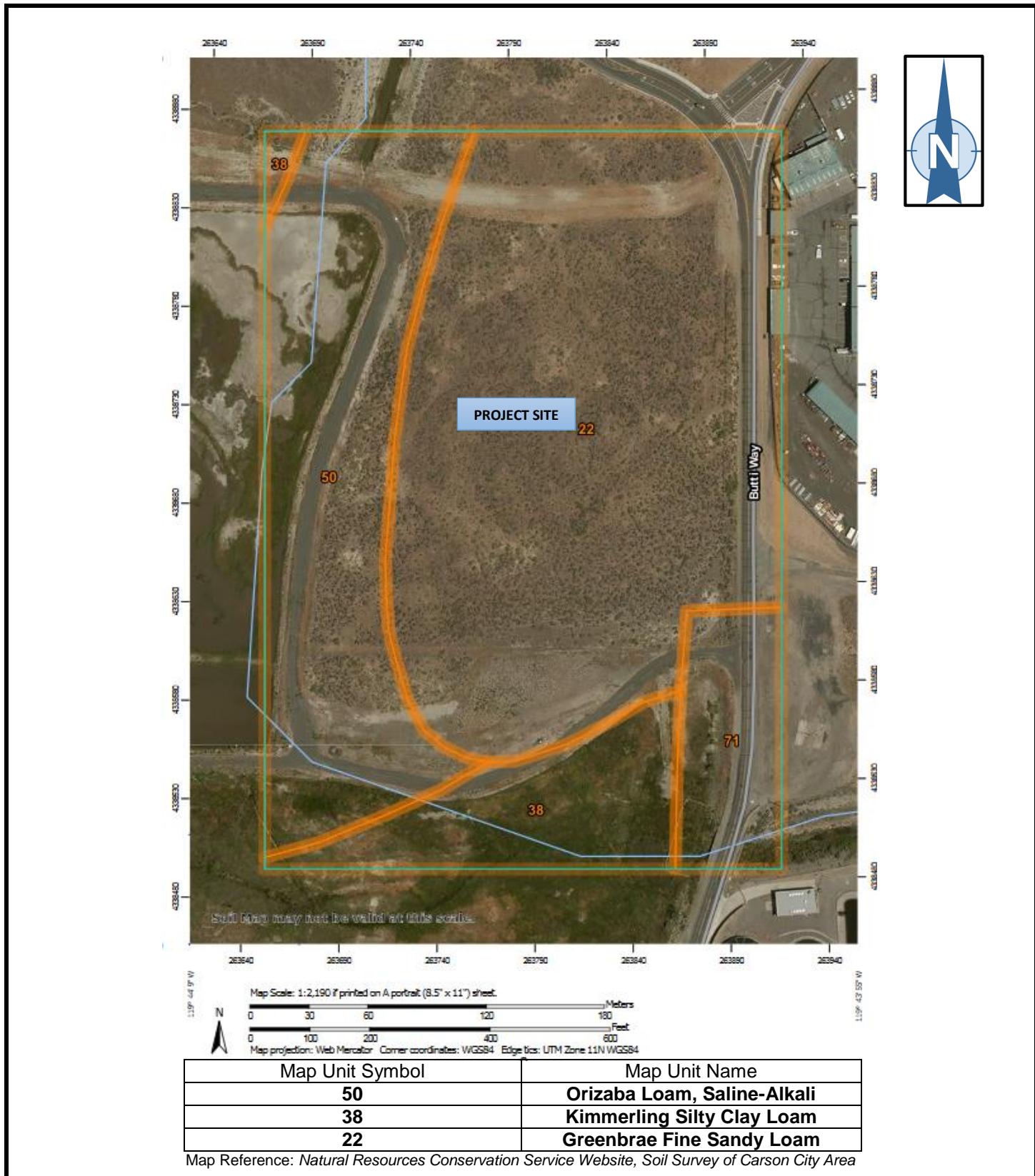


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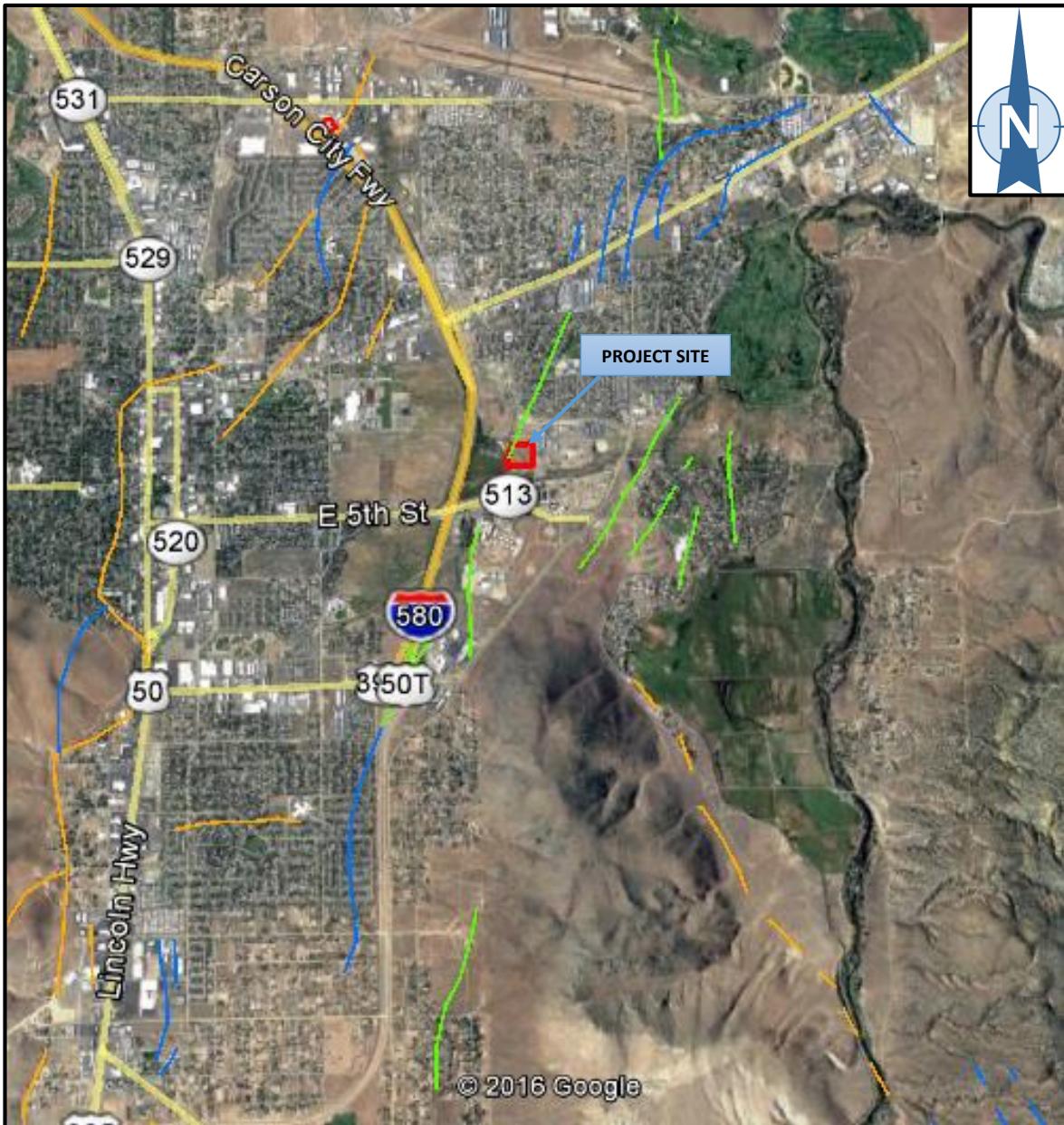
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**FIGURE 2 SITE PLAN**  
**LOMPA RANCH**  
**BOAT AND RV STORAGE PROJECT**  
**CARSON CITY, NV**

PROJECT NO. 16-302.1



**FIGURE 4 SOILS MAP**  
**LOMPA RANCH**  
**BOAT AND RV STORAGE PROJECT**  
**CARSON CITY, NV**  
**PROJECT NO. 16-302.1**



**Quaternary Fault (Inactive)**  
**Late Quaternary Fault (Inactive)**  
**Active Holocene Fault**



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**FIGURE 5 FAULT MAP**  
**LOMPA RANCH**  
**BOAT AND RV STORAGE PROJECT**  
**CARSON CITY, NV**  
**PROJECT NO. 16-302.1**

## APPENDIX A

### Field Investigation and Laboratory Tests Results

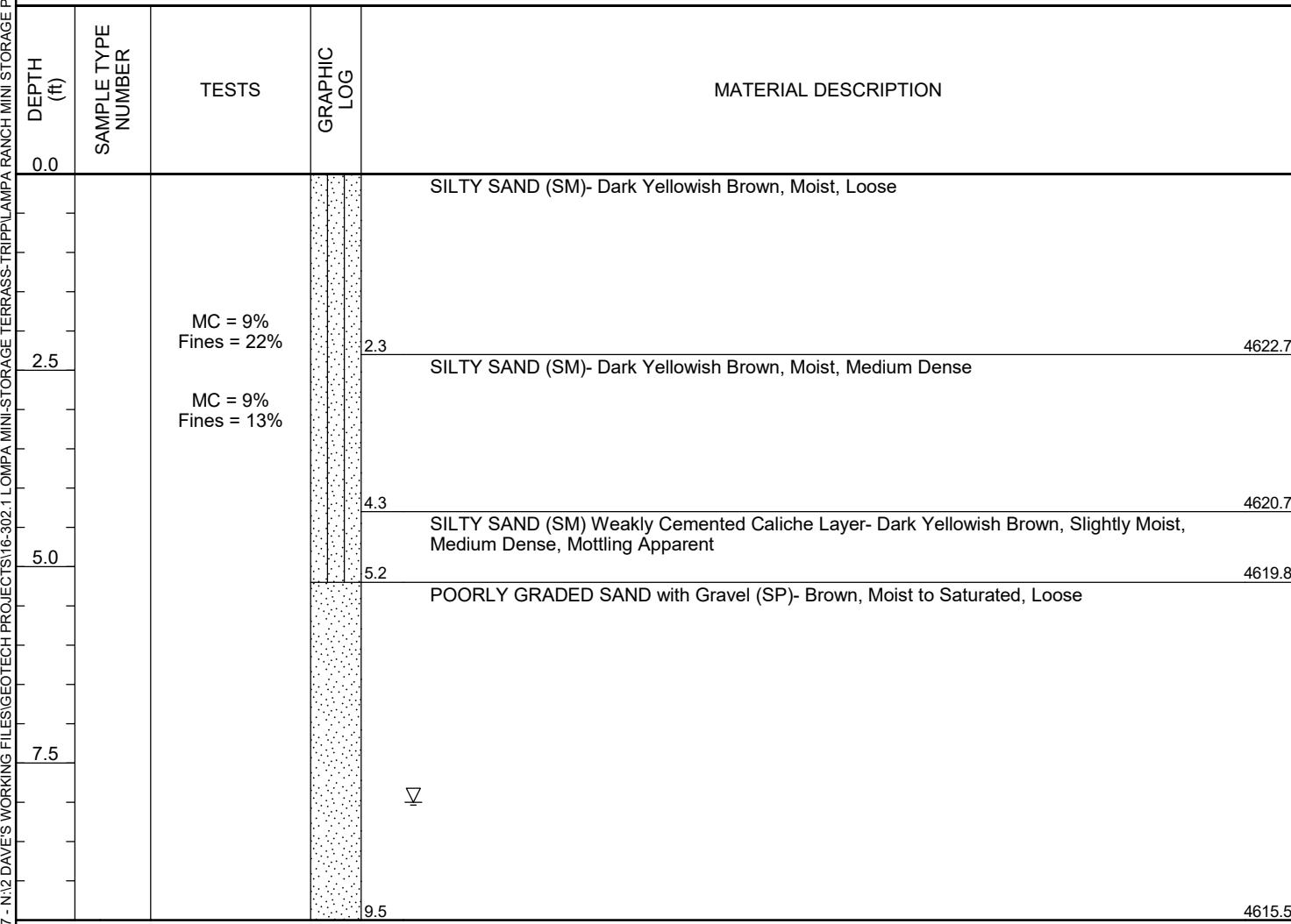


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# TEST PIT NUMBER TP-01

PAGE 1 OF 1

CLIENT Terrasas and Tripp PROJECT NAME Lompa Ranch Mini Storage Project  
PROJECT NUMBER 16-302.1 PROJECT LOCATION Carson City Nevada  
DATE STARTED 3/7/17 COMPLETED 3/7/17 GROUND ELEVATION 4625 ft TEST PIT SIZE inches  
EXCAVATION CONTRACTOR Armac GROUND WATER LEVELS:  
EXCAVATION METHOD Backhoe  
LOGGED BY GL CHECKED BY GL  
NOTES    AT TIME OF EXCAVATION 8.00 ft / Elev 4617.00 ft  
AT END OF EXCAVATION ---  
AFTER EXCAVATION ---





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# TEST PIT NUMBER TP-02

PAGE 1 OF 1

**CLIENT** Terrasas and Tripp  
**PROJECT NUMBER** 16-302.1  
**DATE STARTED** 3/7/17      **COMPLETED** 3/7/17  
**EXCAVATION CONTRACTOR** Armac  
**EXCAVATION METHOD** Backhoe  
**LOGGED BY** GL      **CHECKED BY** GL  
**NOTES**

**PROJECT NAME** Lompa Ranch Mini Storage Project  
**PROJECT LOCATION** Carson City Nevada  
**GROUND ELEVATION** 4636 ft      **TEST PIT SIZE** inches  
**GROUND WATER LEVELS:**  
 **AT TIME OF EXCAVATION** 9.80 ft / Elev 4626.20 ft  
**AT END OF EXCAVATION** ---  
**AFTER EXCAVATION** ---

GENERAL BH / TP / WELL / LAB / GDT - 3/10/17 09:07 - N:\\2 DAVES WORKING FILES\\GEOOTECH PROJECTS\\16-302.1 LOMPA MINI-STORAGE TERRASS-TRIPPI-LAMPA RANCH MINI STORAGE PROJECT.GPJ

| DEPTH<br>(ft) | SAMPLE TYPE<br>NUMBER  | TESTS | GRAPHIC<br>LOG | MATERIAL DESCRIPTION  |
|---------------|------------------------|-------|----------------|---|
| 0.0           |                        |       |                | SILTY SAND with Fine Sand (SM)- Dark Brown, Moist, Loose  |
|               |                        |       | 1.0            | SILTY SAND (SM)- Yellowish Brown, Moist, Medium Dense   |
| 2.5           |                        |       | 2.4            | SANDY SILT (ML)- Gray, Moist, Medium Dense  |
|               |                        |       | 4.0            | SILTY SAND with Fine to Coarse Sand (SM)- Grayish Brown, Moist, Loose to Medium Dense                               |
| 5.0           |                        |       |                |   |
| 7.5           |                        |       | 7.4            | SILTY SAND (SM)- Grayish Brown, Moist, Medium Dense   |
|               | MC = 14%<br>Fines = 6% |       | 9.0            | POORLY GRADED SAND with Fine Sand (SP)- Yellowish Brown, Moist to Saturated, Medium Dense, Strong Mottling Apparent |
| 10.0          |                        |       | 10.4           | Bottom of test pit at 10.4 feet.  |



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## TEST PIT NUMBER TP-03

PAGE 1 OF 1

**CLIENT** Terrasas and Tripp

**PROJECT NUMBER** 16-302.1

**DATE STARTED** 3/7/17 **COMPLETED** 3/7/17

EXCAVATION CONTRACTOR Armac

## EXCAVATION METHOD Backhoe

LOGGED BY GL CHECKED BY GL

## NOTES

**PROJECT NAME** Lompa Ranch Mini Storage Project

**PROJECT LOCATION** Carson City Nevada

**GROUND ELEVATION** 4635 ft      **TEST PIT SIZE** inches

## GROUND WATER LEVELS:

**AT TIME OF EXCAVATION** ---

#### AT END OF EXCAVATION ---

## AFTER EXCAVATION ---

Bottom of test pit at 11.0 feet.



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# TEST PIT NUMBER TP-04

PAGE 1 OF 1

CLIENT Terrasas and Tripp PROJECT NAME Lompa Ranch Mini Storage Project  
PROJECT NUMBER 16-302.1 PROJECT LOCATION Carson City Nevada  
DATE STARTED 3/7/17 COMPLETED 3/7/17 GROUND ELEVATION 4635 ft TEST PIT SIZE inches  
EXCAVATION CONTRACTOR Armac GROUND WATER LEVELS:  
EXCAVATION METHOD Backhoe AT TIME OF EXCAVATION ---  
LOGGED BY GL CHECKED BY GL AT END OF EXCAVATION ---  
NOTES AFTER EXCAVATION ---

GENERAL BH / TP / WELL - GINT STD US LAB GDT - 3/10/17 09:07 - N:\\2 DAVE'S WORKING FILES\\GEOTECH PROJECTS\\16-302.1 LOMPA MINI-STORAGE TERRASS-TRIPLA RANCH MINI STORAGE PROJECT.GPJ

| DEPTH<br>(ft) | SAMPLE TYPE<br>NUMBER | GRAPHIC<br>LOG | MATERIAL DESCRIPTION   |        |
|---------------|-----------------------|----------------|--|--------|
|               |                       |                |  |        |
| 0.0           |                       |                | 0.3 SILTY SAND with Rootlets (SM)- Dark Brown, Moist, Loose                          | 4634.7 |
|               |                       |                | CLAYEY SAND (SC)- Dark Yellowish Brown, Moist, Loose                                 | 4634.2 |
|               |                       |                | 0.8 SILTY SAND (SM)- Gray, Moist, Medium Dense                                       |        |
| 2.5           |                       |                | 3.1 SILTY SAND with Fine Sand (SM)- Gray, Moist, Medium Dense                        | 4631.9 |
|               |                       |                | 4.0 POORLY GRADED SAND with Gravel (SP)- Grayish Brown, Moist, Loose to Medium Dense | 4631.0 |
| 5.0           |                       |                |  |        |
| 7.5           |                       |                |  |        |
| 10.0          |                       |                | 10.6   | 4624.4 |

Bottom of test pit at 10.6 feet.

GENERAL BH / TP / WELL - GINT STD US LAB GDT - 3/10/17 09:07 - N:\\2 DAVE'S WORKING FILES\\GEOTECH PROJECTS\\16-302.1 LOMPA MINI-STORAGE TERRASS-TRIPLA RANCH MINI STORAGE PROJECT.GPJ

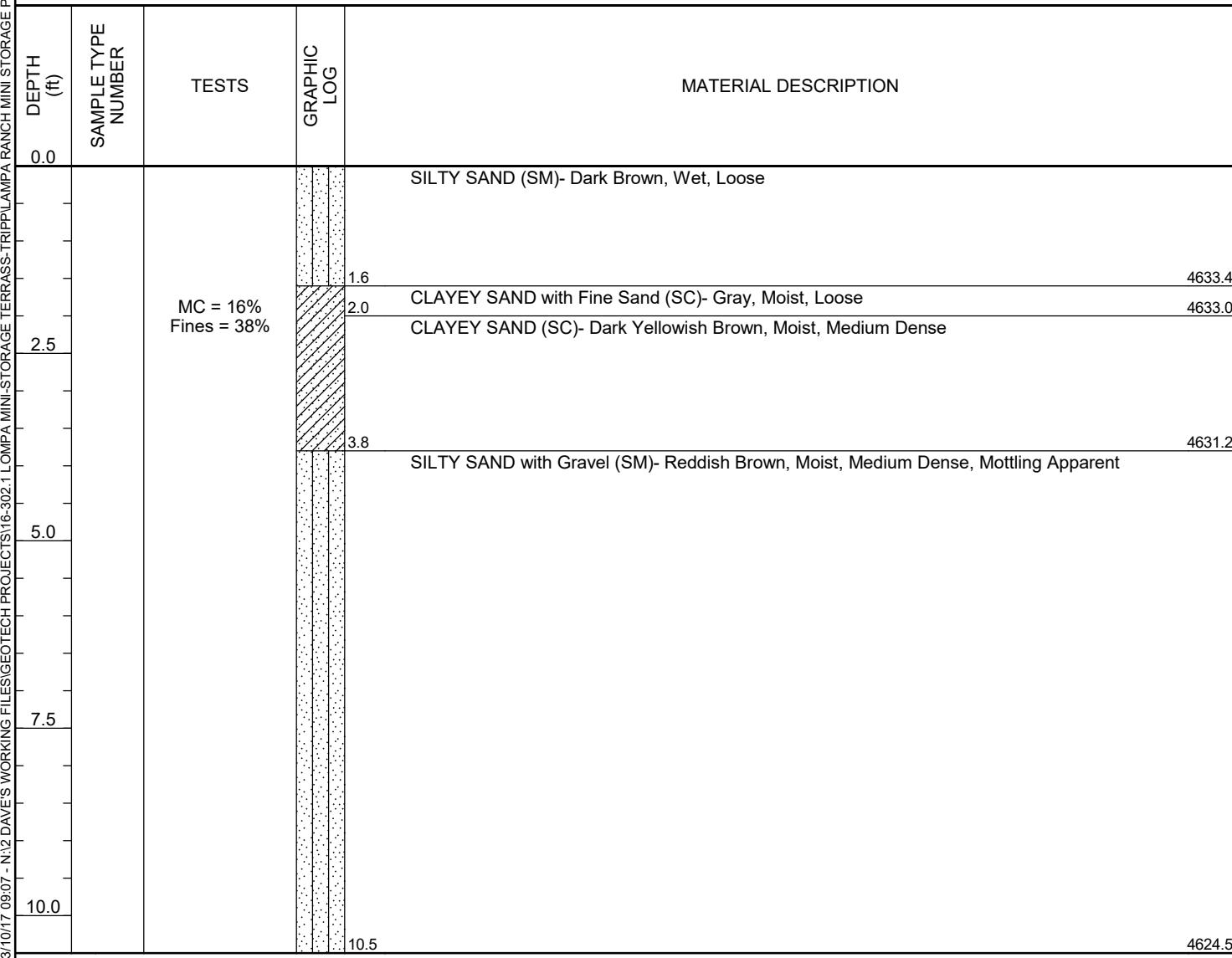


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# TEST PIT NUMBER TP-05

PAGE 1 OF 1

CLIENT Terrasas and Tripp PROJECT NAME Lompa Ranch Mini Storage Project  
PROJECT NUMBER 16-302.1 PROJECT LOCATION Carson City Nevada  
DATE STARTED 3/7/17 COMPLETED 3/7/17 GROUND ELEVATION 4635 ft TEST PIT SIZE inches  
EXCAVATION CONTRACTOR Armac GROUND WATER LEVELS:  
EXCAVATION METHOD Backhoe AT TIME OF EXCAVATION ---  
LOGGED BY GL CHECKED BY GL AT END OF EXCAVATION ---  
NOTES AFTER EXCAVATION ---





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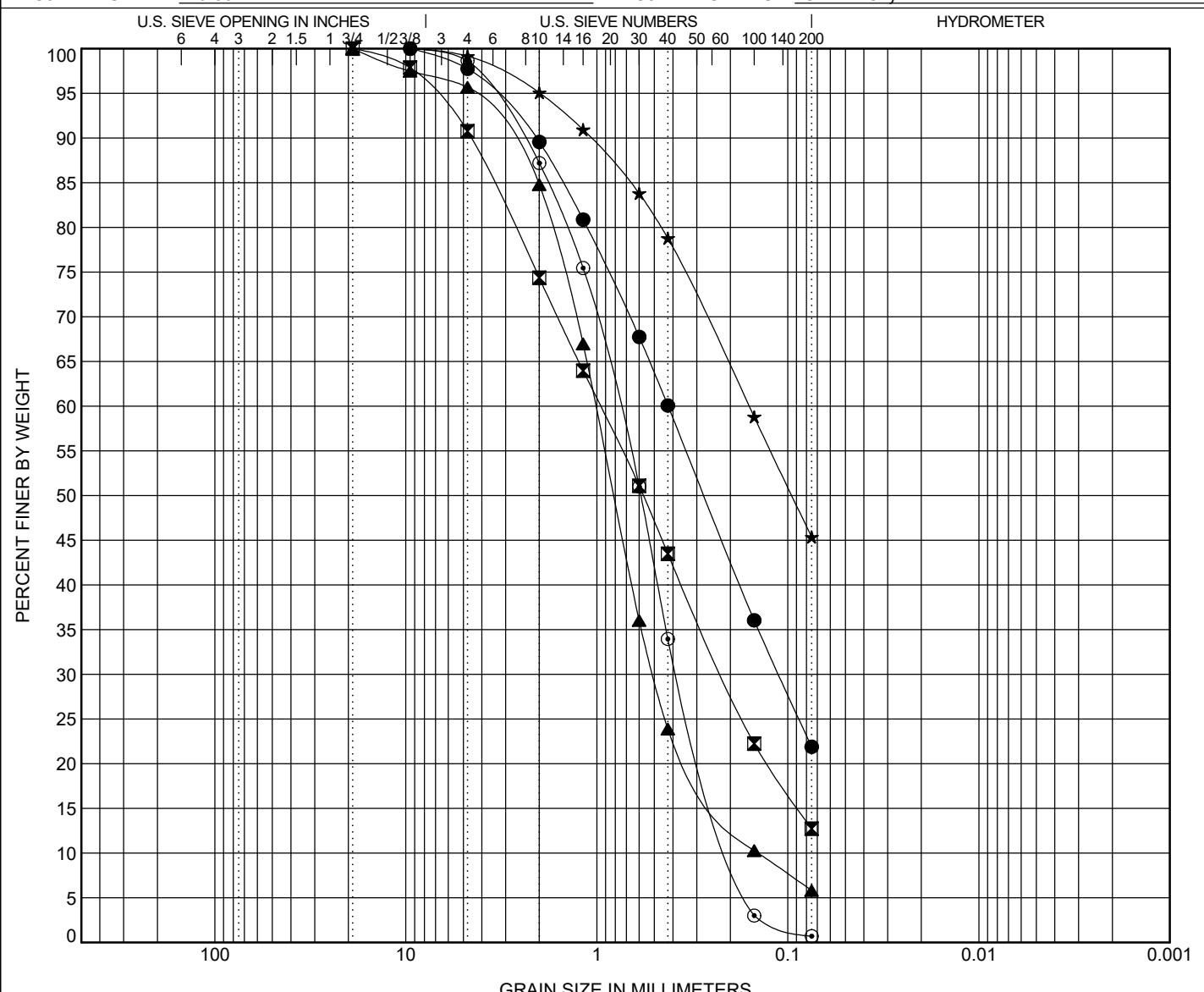
## GRAIN SIZE DISTRIBUTION

CLIENT Terrasas and Tripp

PROJECT NAME Lompa Ranch Mini Storage Project

PROJECT NUMBER 16-302.1

PROJECT LOCATION Carson City Nevada



| COBBLES | GRAVEL |      | SAND   |        |      | SILT OR CLAY |  |  |
|---------|--------|------|--------|--------|------|--------------|--|--|
|         | coarse | fine | coarse | medium | fine |              |  |  |

| BOREHOLE | DEPTH | Classification                   |       |       |       |         | LL    | PL    | PI    | Cc   | Cu   |
|----------|-------|----------------------------------|-------|-------|-------|---------|-------|-------|-------|------|------|
| ● TP-01  | 2.0   | SILTY SAND (SM)                  |       |       |       |         |       |       |       |      |      |
| ☒ TP-01  | 3.0   | SILTY SAND with Gravel (SM)      |       |       |       |         |       |       |       |      |      |
| ▲ TP-02  | 9.0   | POORLY GRADED SILTY SAND (SM-SP) |       |       |       |         |       |       |       | 1.75 | 7.04 |
| ★ TP-03  | 1.0   | CLAYEY SAND (SC)                 |       |       |       |         | 35    | 21    | 14    |      |      |
| ○ TP-03  | 3.0   | POORLY GRADED SAND (SP)          |       |       |       |         |       |       |       | 0.95 | 4.05 |
| BOREHOLE | DEPTH | D100                             | D60   | D30   | D10   | %Gravel | %Sand | %Silt | %Clay |      |      |
| ● TP-01  | 2.0   | 9.5                              | 0.424 | 0.112 |       | 2.3     | 75.9  |       | 21.9  |      |      |
| ☒ TP-01  | 3.0   | 19                               | 0.958 | 0.219 |       | 9.2     | 78.0  |       | 12.7  |      |      |
| ▲ TP-02  | 9.0   | 19                               | 1.013 | 0.506 | 0.144 | 4.3     | 89.9  |       | 5.8   |      |      |
| ★ TP-03  | 1.0   | 9.5                              | 0.16  |       |       | 0.8     | 53.8  |       | 45.4  |      |      |
| ○ TP-03  | 3.0   | 9.5                              | 0.769 | 0.372 | 0.19  | 1.3     | 98.0  |       | 0.7   |      |      |



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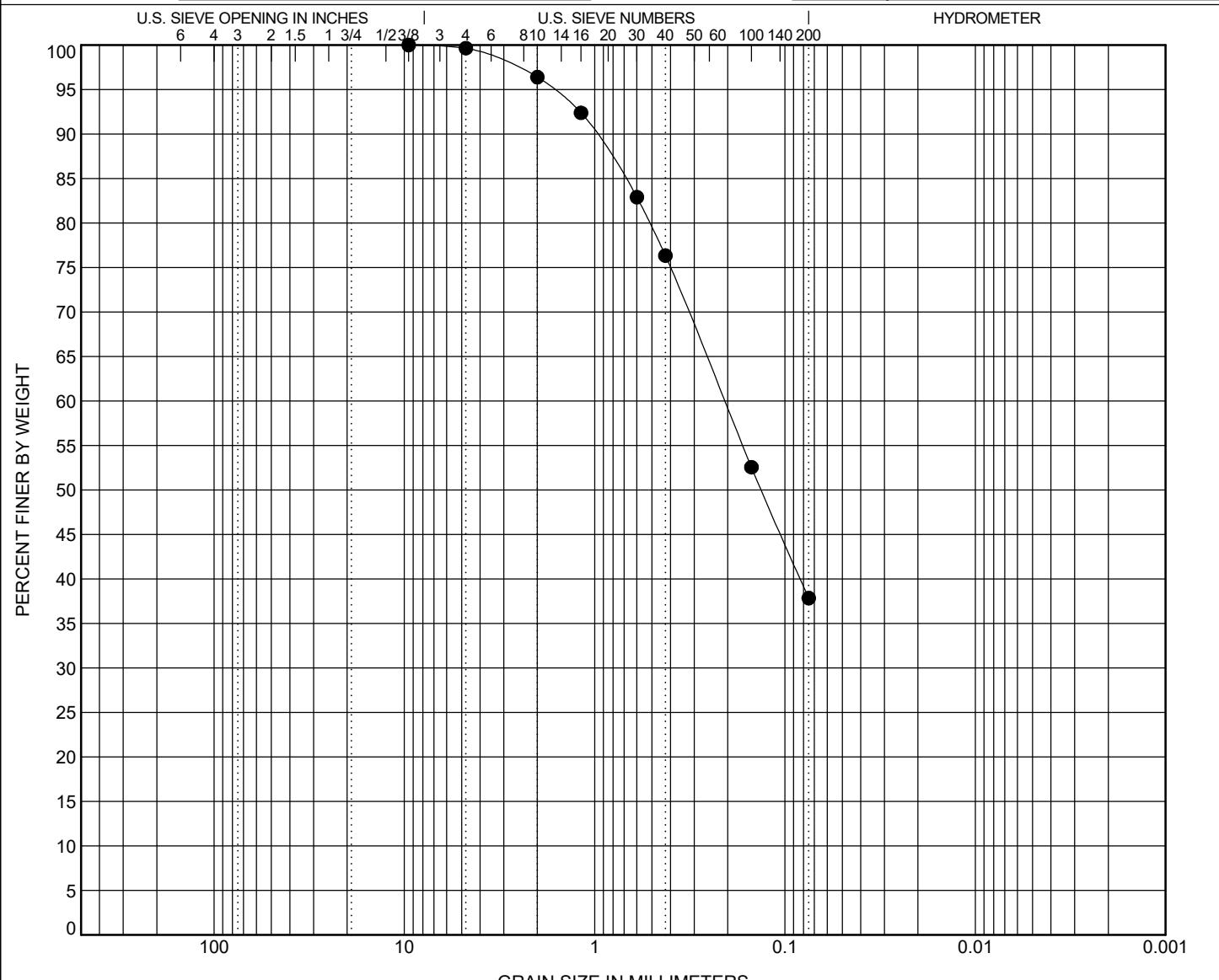
## GRAIN SIZE DISTRIBUTION

**CLIENT** Terrasas and Tripp

**PROJECT NAME** Lompa Ranch Mini Storage Project

**PROJECT NUMBER** 16-302.1

**PROJECT LOCATION** Carson City Nevada





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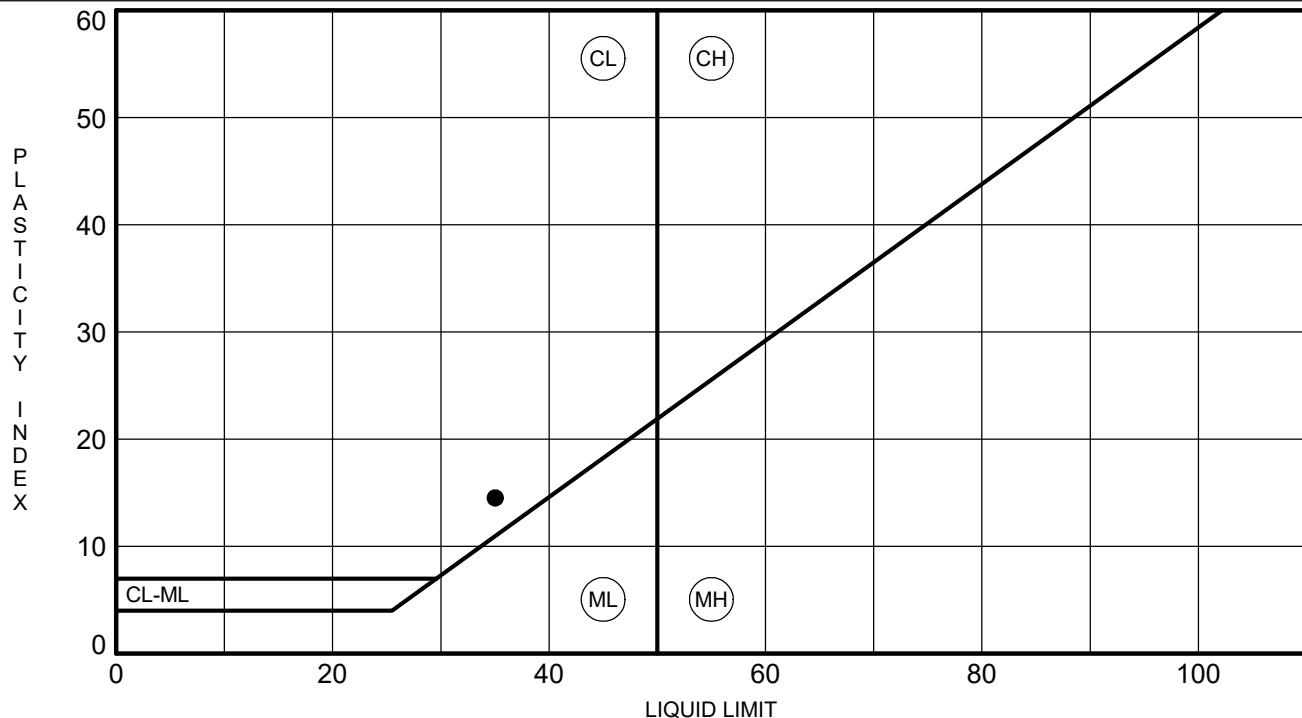
## ATTERBERG LIMITS' RESULTS

**CLIENT** Terrasas and Tripp

**PROJECT NAME** Lompa Ranch Mini Storage Project

**PROJECT NUMBER** 16-302.1

**PROJECT LOCATION** Carson City Nevada





## SAMPLE EARTHWORK SPECIFICATIONS

### General

The geotechnical report is available to bidders as a general guide to project and site conditions. The geotechnical report is not a part of the contract documents and is not a warranty as to site conditions. Bidders are encouraged to pothole the site as changes in site conditions between exploration points or due to weather or man-made influences can impact the site. Access to the site to perform independent evaluations should be provided upon reasonable request. Contract documents should address access and independent evaluations as to site conditions at the time of bidding.

The specifications for earthwork, flatwork and pavements should conform to the Standard Specifications for Public Works Construction (the “Orange Book”) except as modified herein or as specified in the contract documents (including drawings).

### 1.1 Soil Handling and Excavation Characteristics

- 1.1.1 In our opinion, grading and excavations **may/may not** be accomplished with light to moderate effort with conventional heavy-duty grading/excavation equipment. Excavations in native soils **are/are not** anticipated to generate significant quantities of oversized material (greater than six inches in dimension) that would require special handling or exporting from the site.
- 1.1.2 Excavated native soils generated from cut operations at the site **are/are not** anticipated to be suitable for re-use as engineered fill.
- 1.1.3 Imported structural fill material may be necessary. If needed, imported materials should meet the Standard Specifications for Public Works specifications (304.03). Structural fill is defined herein as all fill placed within two feet of foundations and all fill placed beneath pavement sections. Import structural fill material should be sampled and approved by us prior to its transportation to the site.
- 1.1.4 Temporary excavations, such as utility trench sidewalls excavated within undisturbed, unsaturated native soils or structural fill should remain near-vertical to depths of **XX** feet. Some minor sloughing should be expected within some of cleaner sand lenses or during periods of high precipitation. Native soils within **XX** feet of the existing surface should be considered **Type X** by OSHA Standards. It is the contractor’s responsibility to provide sufficient and safe excavation support per OSHA Standards as well as protecting nearby utilities, structures, and other improvements, which may be damaged by earth movements.

## **1.2 Grading – General, Site Preparation**

- 1.2.1 A preconstruction conference should be held at the site prior to the beginning of grading operations with the owner, contractor, civil engineer and geotechnical engineer in attendance. Soil handling and grading requirements can be discussed at that time. Earthwork operations should be observed and compacted fill tested by our representative.
- 1.2.2 All references to relative compaction and optimum moisture content in this report are based on the newest version ASTM D1557 Test Procedure.
- 1.2.3 Site preparation should begin with the removal of vegetation and debris if any. The depth of removal should be such that material exposed in the cut areas or soils to be used as fill is relatively free of organic matter. This will likely result in removal depths ranging from approximately **XX** inches, depending on location. Material generated during stripping should be disposed of as directed by the Engineer.
- 1.2.4 During or immediately following wet weather, the near-surface soil may deflect or pump under heavy equipment loads. Yielding soil conditions can typically be stabilized using one of the methods listed below. However, soil conditions and mitigation methods should be reviewed and approved by us when encountered.
  - **Option 1.** Deeply scarify (10 to 12 inches) allow to air dry to near optimum moisture content and re-compact.
  - **Option 2.** Remove unstable (wet) soils to a firm base and allow the wet subgrade soil to dry to near optimum moisture content and re-compact. Replace the removed soils with drier soil meeting the structural fill specifications.
  - Other stabilization alternatives may be appropriate depending on the situation. Consultation with us is crucial for expedient and appropriate mitigation.

## **1.3 Grading – Building Pad Preparation**

- 1.3.1 For the purposes of this report, structural building pad areas should be considered areas extending a minimum of **XX** feet beyond the outside dimensions of the building or any foundation element.
- 1.3.2 Cut surfaces should be scarified at least X inches and compacted to **XX** % of maximum dry density.
- 1.3.3 Areas to receive fill should be prepared as described above for cut surfaces. Fill should be placed in X inch lifts, moisture conditioned to within **XX**% of optimum moisture content and compacted to a minimum of **XX**% of maximum dry density.

## **1.4 Grading – Underground Utilities**

- 1.4.1 Underground utility trenches within structural areas (building pads, parking lots, and streets) should be backfilled with properly compacted material. The material excavated from the trenches

**will/will not** be adequate for on-site use as backfill provided it **does/does not** contain deleterious matter, vegetation or rock larger than six inches in maximum dimension. Trench backfill should be placed in loose lifts not exceeding eight inches. The lifts should be compacted to a minimum of 90% relative compaction at or near optimum moisture content.

- 1.4.2 Site soils **will/will not** be suitable for utility trench pipe bedding. Importation of bedding materials should be planned for accordingly.
- 1.4.3 Bedding and pipe zone backfill should extend from the bottom of the trench excavation to a minimum of 6 inches above the crown of the pipe. Pipe bedding material should consist of Class A Backfill material as defined by the Standard Specifications for Public Works (Orange Book). Bedding and pipe zone material should be compacted in 6 inch maximum lifts.

## **1.5 Grading – Pavement and Flatwork Areas**

- 1.5.1 In pavement and flatwork subgrade areas the subgrade (whether fill or undisturbed native soil) should be scarified to a depth of 8 to 10 inches and moisture conditioned at or near optimum moisture content. The upper 6 inches of pavement subgrade soils should be compacted to a minimum of 90% relative compaction at or near optimum moisture content. If the compacted surface becomes dry and loose after it has been tested for compaction, it should be moisture conditioned and lightly compacted to a firm surface prior to the placement of additional fill or aggregate base.
- 1.5.2 The subgrade soils for pavements should be finished to a compacted smooth unyielding surface. We recommend proof-rolling the subgrade with a loaded water truck (or similar equipment) to verify the stability of the subgrade prior to placing aggregate base.
- 1.5.3 Aggregate base used to support pedestrian and vehicular pavements should be compacted to a minimum of 95% relative compaction.

## **1.6 Slabs-on-Grade**

- 1.6.1 Earthwork below conventional concrete slab-on-grade floors should be prepared as recommended for cuts or fills in building pad areas. A vapor barrier should be provided consisting of a minimum of 10 mil polyethylene material or other approved material. The vapor barrier should be lapped and sealed in accordance with the manufacturer's recommendations. At a minimum, joint laps should be at least six inches in width. In any case, care should be taken to avoid any disturbance or rupture to the vapor barrier measures throughout the construction process.
- 1.6.2 Slabs should be underlain by a minimum of **X** inches of compacted (95% minimum relative density) aggregate base. Slab thickness and reinforcement should be determined by the structural engineer based on the anticipated loading.



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- 1.6.3 If a significant amount of time has passed since building pad grading and the soil surface of the building pad has become dry, then it should be re-moistened prior to placing the moisture retarding system. The building pad should be moistened by soaking or sprinkling such that the upper 12 inches of soil is near optimum moisture, as determined by our representative at least 48 hours before concrete placement.
- 1.6.4 Some floor coverings, such as tile or linoleum, are sensitive to moisture that can be transmitted from and through the slab. Slab floors should be moist cured for a minimum of 7 days prior to placing any floor coverings. Floor coverings should be installed in accordance with the manufacturer's recommendations including any moisture transmissivity testing requirements.
- 1.6.5 Crack control spacing should be determined by the project structural engineer based on slab thickness and intended usage.
- 1.6.6 All exterior concrete should be air entrained with from 4.5% to 7.0% air content. The water cement ratio for all exterior concrete should be 0.45 or less. The use of mid-range plasticizer is recommended to facilitate the finishing process while maintaining the desired water cement ratio.
- 1.6.7 Exterior concrete should be placed and finished in accordance with American Concrete Institute (ACI) recommendations for concrete placed in areas subject to freeze-thaw environments.

Recommendations presented herein are intended to reduce the potential for cracking of slabs as a result of differential movement. However, even with the incorporation of the recommendations presented herein, slabs-on-grade will still exhibit some cracking. The occurrence of concrete shrinkage cracks is independent of the soil supporting characteristics. Their occurrence may be reduced and/or controlled by limiting the slump of concrete, the use of crack control joints and proper concrete placing and curing. Adherence to ACI and Portland Concrete Association (PCA) recommendations including those for low humidity and wind, if applicable, should be incorporated into project construction practices.

## **1.7 Flexible Pavements**

- 1.7.1 Earthwork below conventional flexible pavements should be prepared as recommended for cuts or fills in building pad areas. Aggregate base should consist of Type 2 Class B and be compacted to 95% of maximum dry density.
- 1.7.2 Asphalt concrete should conform Section 320.02 of the Standard Specifications for Public Works Construction ("Orange Book"). We recommend that the asphalt consist of polymerized oil type PG64-28NV as it substantially reduces maintenance and extends pavement life in our experience.
- 1.7.3 The asphalt concrete should consist of Type II or Type III of Section 200.02.02 of the Standard Specifications for Public Works Construction ("Orange Book").



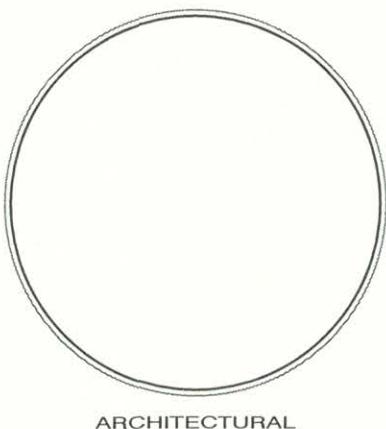
Resource Concepts Inc

## 1.8 Site Drainage

- 1.8.1 Adequate drainage is crucial to reduce the potential for differential soil movement, erosion and subsurface seepage. Under no circumstances should water be allowed to pond adjacent to footings. The site should be graded and maintained such that surface drainage is directed away from structures and the top of slopes into swales or other controlled drainage devices. The percent fall of slopes around structures should be as per the most current version of the IBC as adopted by the local governing agency.
- 1.8.2 Roof and pavement drainage should be directed into conduits to carry runoff away from the structures. Landscape irrigation should be kept at least three feet away from all foundations. We recommend that drip irrigation be installed within six feet of foundations wherever feasible.

| REVISIONS | DATE |
|-----------|------|
|           |      |
|           |      |
|           |      |
|           |      |

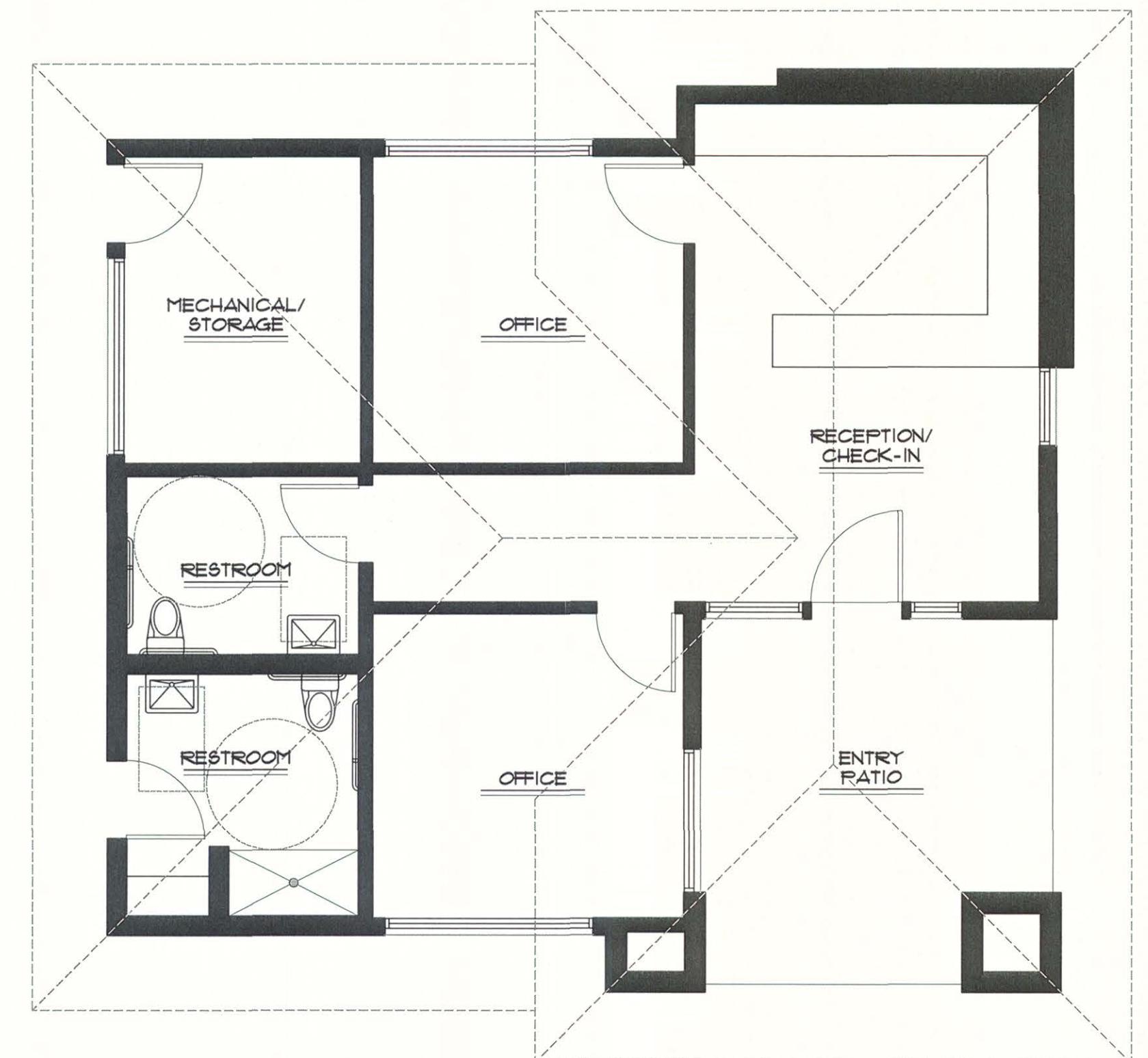
**ECJ**  
ARCHITECT  
ERIC JOHNSON  
ARCHITECT  
PO BOX 34282  
RENO NEVADA 89523  
775-750-1133  
ECJARCH@CHARTER.NET



ARCHITECTURAL  
ONLY

**SIERRA TAHOE RV & BOAT STORAGE**  
NEW RV & BOAT STORAGE FACILITY FOR:  
BUTTI WAY | CARSON CITY | NEVADA | APN 010-041-766  
FLOOR PLAN & EXTERIOR BUILDING ELEVATIONS - SUP SUBMITTAL

|              |          |
|--------------|----------|
| DRAWN BY     |          |
| CHECKED      | -        |
| DATE         | 12/05/13 |
| SCALE        | AS SHOWN |
| JOB NUMBER   | STRB     |
| SHEET NUMBER | A4.1     |



|           |    |
|-----------|----|
| REVISIONS | BY |
|           |    |
|           |    |
|           |    |
|           |    |

TreeHugger Studio, LLC  
landscape architecture  
and site planning services  
140 West Guffey Drive  
Washoe Valley, NV 89704  
(775) 530-0665



preliminary landscape plan for:  
SIERRA TAHOE RV and BOAT STORAGE  
Airport Road  
Carson City, NV  
APN: 010-041-76

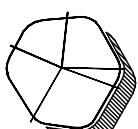
## PRELIMINARY LANDSCAPE PLAN

SHEET INFORMATION

ISSUE DATE:  
May 2019  
SCALE:  
1" = 30'-0"  
DRAWN BY:  
R. SHOCK  
CHECKED BY:  
PROJECT NUMBER:  
228-19-006

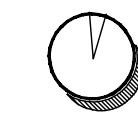
L.1

### PLANT LIST:



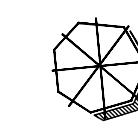
LARGE/STREET TREES: (2" Caliper)  
IRRIGATE ALL PLANTINGS W/RESOURCE EFFICIENT Drip IRRIGATION SYSTEM.

| BOTANICAL NAME       | COMMON NAME            |
|----------------------|------------------------|
| RED MAPLE            | ACER RUBRUM            |
| BLACK GUM            | NYSSA SYLVATICA        |
| GLDTISIA TRIACANTHOS | SHADEMASTER HONELOCUST |
| QUERCUS PALUSTRIS    | PIN OAK                |
| QUERCUS RUBRA        | RED OAK                |



### SMALL/ACCENT TREES:

| BOTANICAL NAME              | COMMON NAME          |
|-----------------------------|----------------------|
| ACER GINNALA                | AMUR MAPLE           |
| CERCIS CANADENSIS           | EASTERN REDBUD       |
| LIQUIDAMBAR STYACIFLUA      | SWEETGUM             |
| MALUS PRIERIFIRE            | PRIERIFIRE CRABAPPLE |
| PYRUS CALLERYANA 'REDSPINE' | REDSPINE PLUM        |



### EVERGREEN TREES: (6' Min. Height)

| BOTANICAL NAME                 | COMMON NAME       |
|--------------------------------|-------------------|
| JUNIPERUS CHINENSIS 'TORULOSA' | HOLLYWOOD JUNIPER |
| CALOCEDRUS DECURRENS           | INCENSE CEDAR     |
| PICEA PUNGENS                  | COLORADO SPRUCE   |
| PINUS FLEXILIS 'VANDERWOLF'    | VANDERWOLF'S PINE |
| PINUS NIGRA                    | AUSTRIAN PINE     |

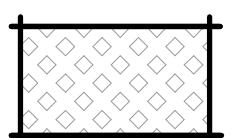
### SHRUBS/GROUNDCOVERS/VINES: (5 Gal. Min.)

| BOTANICAL NAME   | COMMON NAME      |
|------------------|------------------|
| BUDDLEIA DAVIDII | BUTTERFLY BUSH   |
| CAMPIS RADICANS  | RED TRUMPET VINE |
| GENISTA LYDIA    | DAWRF BROOM      |
| PHOTINIA FRASERI | REDTWIG PHOTINIA |
| SYRINGA VULGARIS | COMMON LILAC     |

### PERENNIALS: (1 Gal.)

| BOTANICAL NAME         | COMMON NAME              |
|------------------------|--------------------------|
| ECHINACEA PURPUREA     | PURPLE CONEFLOWER        |
| GAILLARDIA ARISTATA    | INDIAN BLANKET FLOWER    |
| PENSTEMON STRICTUS     | ROCKY MOUNTAIN PENSTEMON |
| CERATHEUM TOMENTOSUM   | SNOW-IN-SUMMER           |
| PEROVSKIA ATRIPLIFOLIA | RUSSIAN SAGE             |

### LANDSCAPE LEGEND:



SHRUB PLANTING AREA w/ROCK MULCH COVER - (56,340 SF)  
Shrub sizes, varieties and quantities to meet code requirements.  
Place mulch @ 4" min. depth, over weed barrier fabric

### LANDSCAPE CALCULATIONS:

#### ZONING:

SITE AREA=

IMPERVIOUS AREA =

LANDSCAPE SUMMARY:

LANDSCAPE AREA REQUIRED =

LANDSCAPE AREA PROVIDED =

#### TREE/SHRUB REQUIREMENTS:

1 TREE/400 S.F. (= 141 TREES)

Includes: 1 TREE/10 PARKING SPACES (7 SPACES = 1 TREE)

and 1 TREE/30 LF STREET FRONTAGE (497 LF. = 17 TREES)

TOTAL TREES REQUIRED =

TREES PROVIDED =

6 SHRUBS per TREE REQUIRED =

#### GENERAL COMMERCIAL

7.28 AC.

231,341 S.F.

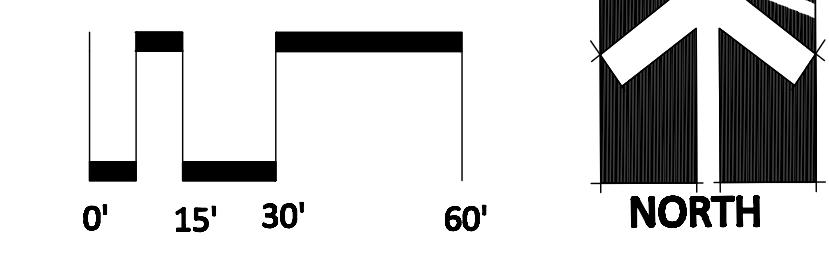
46,268 S.F. (20% of Impervious Area)

56,925 S.F. (24.6 %)

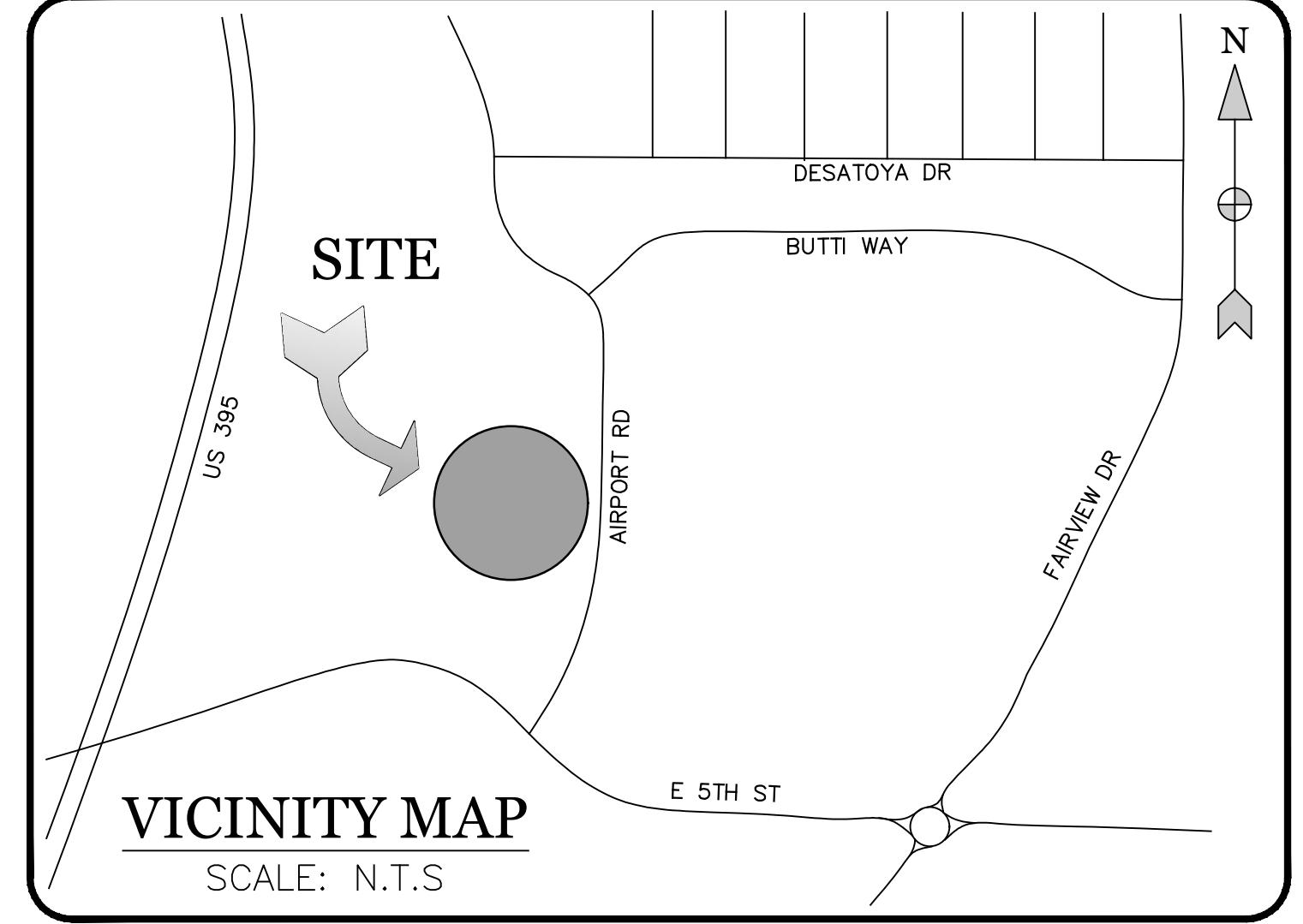
141 TREES

141 TREES

846 SHRUBS



| SITE & GRADING LEGEND |  |
|-----------------------|--|
|                       | A.C. PAVEMENT AREA                                   |
|                       | CONCRETE AREA  |
|                       | ACCESSIBLE PARKING SPACE W. SIGN & PAVEMENT MARKINGS |
|                       | PEDESTRIAN ACCESS RAMP                               |
|                       | ACCESSIBLE ROUTE                                     |
|                       | PARKING SPACE COUNT                                  |
|                       | KEYNOTE (REF. CORRESPONDING LEGEND)                  |
|                       | UTILITY  |
|                       | EXISTING UTILITY LINE W. DESCRIPTION                 |
|                       | MAHOLE W. DESCRIPTION (EXISTING/PROPOSED)            |
|                       | CLEANOUT (EXISTING/PROPOSED)                         |
|                       | CATCH BASIN/DROP INLET                               |
|                       | YARD DRAIN   |
|                       | DIRECTIONAL FLOW LINE                                |
|                       | GRADE BREAK  |
|                       | PROPOSED CONTOUR LINE                                |
|                       | EXISTING CONTOUR LINE                                |
|                       | SPOT ELEVATION (EXISTING/PROPOSED)                   |



#### NOTES

1. THE FIELD SURVEY PREPARED BY WOOD RODGERS IS THE BASIS OF THIS DESIGN. MVC TAKES NO RESPONSIBILITY FOR THE ACCURACY OF THE SURVEY.
2. BASIS OF BEARINGS WAS DERIVED FROM NATIONAL GEODETIC SURVEY MONUMENT "V 357" DESCRIBED AS A DISC SET IN CONCRETE STAMPED "V 357 1953. THE MONUMENT IS LOCATED ON THE SOUTH SIDE OF U. S. HIGHWAY 50, SOUTHEAST OF THE INTERSECTION OF THE HIGHWAY AND AKRON WAY, 101 FEET SOUTHEAST OF THE CENTER LINE OF THE HIGHWAY, 54 FEET NORTHEAST OF THE CENTER OF A WOODEN CATTLE GUARD, PUBLISHED ELEVATION = 4633.40.
3. BASIS OF ELEVATION IS THE NEVADA STATE PLANE COORDINATE SYSTEM, WEST ZONE, NORTH AMERICAN DATUM OF 1983/1994, HIGH ACCURACY REFERENCE NETWORK (NAD 83/94-HARN), AS DETERMINED USING REAL TIME KINEMATIC (RTK) GPS OBSERVATIONS OF CARSON CITY CONTROL MONUMENTS "CC038" AND "V357". THE BEARING BETWEEN CARSON CITY CONTROL MONUMENT "CC038" AND "V357" IS TAKEN AS SOUTH 31°48'44" WEST. ALL DIMENSIONS SHOWN ARE GROUND DISTANCES. COMBINED GRID-TO-GROUND FACTOR = 1.0002000.
4. ALL WORK WITHIN THE CARSON CITY RIGHT-OF-WAY SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE LATEST CODES, STANDARDS, SPECIFICATIONS & DETAILS.
5. REFERENCE ARCHITECTURAL PLANS FOR ALL EXACT DIMENSIONS.
6. ALL DIMENSIONS ARE FROM THE FACE OF CURB, FACE OF BUILDING, FACE OF WALL, CENTER OF PIPE, CENTER OF MANHOLE OR PROPERTY LINE UNLESS OTHERWISE NOTED.
7. ALL PERMANENT STRIPING, SIGNAGE & TRAFFIC CONTROL IMPROVEMENTS SHALL BE INSTALLED IN ACCORDANCE WITH CURRENT "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) REQUIREMENTS.
8. THE ACCESSIBILITY ROUTE SHALL NOT HAVE A RUNNING SLOPE EXCEEDING 5% OR A CROSS SLOPE EXCEEDING 2%. REFERENCE SITE PLAN FOR LOCATION.
9. THIS SITE LIES IN FEMA FLOOD ZONE X AND ZONE AE (320001011116). ZONE AE IS DEFINED AS AREAS WITHIN THE 1-PERCENT ANNUAL CHANCE FLOOD, ALSO REFERRED TO AS THE BASE FLOOD OR 100-YEAR FLOOD. MODERATE FLOOD HAZARD AREAS, ZONE X (SHADED) IS DEFINED AS A MODERATE FLOOD HAZARD AREA AND IS BETWEEN THE LIMITS OF THE BASE FLOOD AND THE 0.2-PERCENT-ANNUAL-CHANCE (OR 500-YEAR) FLOOD. ZONE X (UNSHADED) IS DEFINED AS AN AREA OF MINIMAL FLOOD HAZARD, WHICH ARE THE AREAS OUTSIDE THE 0.2-PERCENT-ANNUAL-CHANCE FLOODPLAIN.

| UTILITY PROVIDERS  |                                  |
|--------------------|----------------------------------|
| DOMESTIC WATER     | CARSON CITY UTILITIES DEPARTMENT |
| IRRIGATION WATER   | CARSON CITY UTILITIES DEPARTMENT |
| SANITARY SEWER     | CARSON CITY UTILITIES DEPARTMENT |
| STORM DRAIN        | CARSON CITY UTILITIES DEPARTMENT |
| NATURAL GAS        | SOUTHWEST GAS CORPORATION        |
| ELECTRICITY        | NV ENERGY                        |
| TELECOMMUNICATIONS | AT&T / CHARTER COMMUNICATIONS    |
| FIRE               | CARSON CITY FIRE DEPARTMENT      |
| POLICE             | CARSON CITY SHERIFFS OFFICE      |

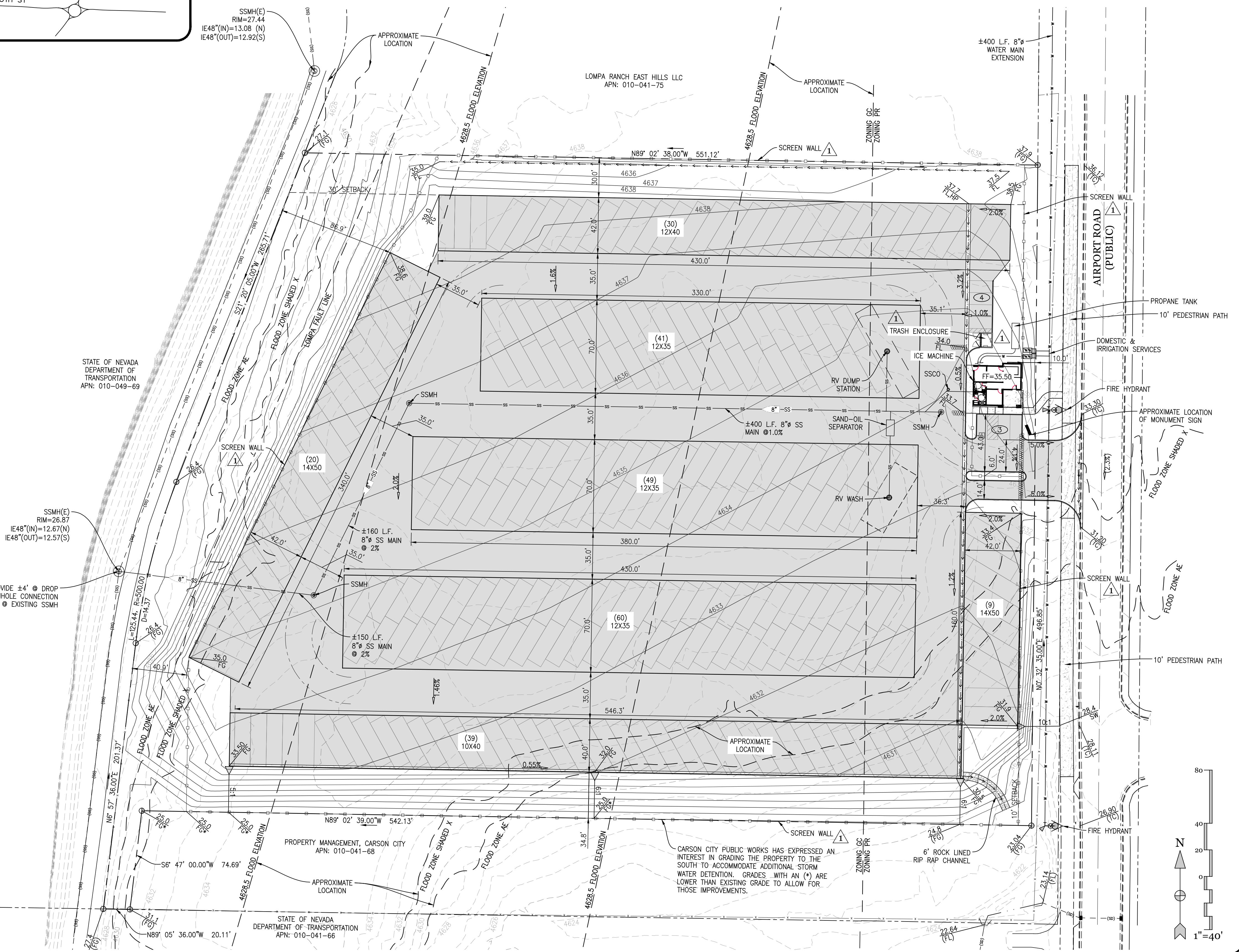
| SITE ANALYSIS      |                        |
|--------------------|------------------------|
| AREA               | 7.28 AC                |
| ZONING             | GENERAL COMMERCIAL(GC) |
| SETBACKS (F/R/S)   | 0/0/0(FT)*             |
| BUILDING FOOTPRINT | ±1,000 S.F. 0.3%       |
| PAVED/IMPERVIOUS   | ±232,629 S.F. 73%      |
| LANDSCAPED         | ±63,423 S.F. MIN. 20%  |
| UNDEVELOPED        | ±20,065 S.F. 6.7%      |

\*30' MIN. SETBACK FROM RESIDENTIAL & 10' MIN. LANDSCAPE SETBACK ALONG AIRPORT ROAD\*

| EARTHWORK ANALYSIS |                             |                   |                                    |
|--------------------|-----------------------------|-------------------|------------------------------------|
| SITE AREA          | 7.28 AC                     | CRITERIA          | REQUIRED SPACES                    |
| SITE DISTURBANCE   | ±7.0 AC                     | OFFICE            | ±975 1/325 3.0                     |
| PROPOSED CUT       | ±19,500 YD <sup>3</sup>     | TOTAL REQUIREMENT | SPACES PROVIDED ADA VAN ACCESSIBLE |
| PROPOSED FILL      | ±5,000 YD <sup>3</sup>      | 3 SPACES          | 7 1 1                              |
| NET EARTHWORK      | ±14,500 YD <sup>3</sup> CUT |                   |                                    |

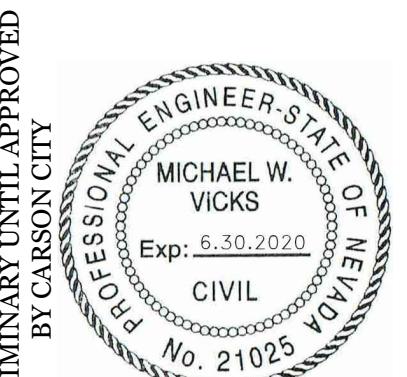
NOTES:  
1. ALL BUILDING INFORMATION PROVIDED BY THE ERIC JOHNSON ARCHITECT.  
2. REQUIREMENTS DETERMINED USING CMC 18A2.2.

| PARKING ANALYSIS |                         |          |                 |
|------------------|-------------------------|----------|-----------------|
| USE              | AREA (FT <sup>2</sup> ) | CRITERIA | REQUIRED SPACES |



|             |                          |
|-------------|--------------------------|
| Revisions   | Initial Review Responses |
| Date        | 5.23.19                  |
| Drawn       | MWV                      |
| Checked     | MWV                      |
| Project No. | 18.041                   |

Preliminary Site Plan  
C1.0



PROFESSIONAL ENGINEER STATE OF NEVADA  
MICHAEL W. VICKS  
CIVIL  
No. 21025  
Exp. 6/30/2020

Sierra Tahoe RV & Boat Storage  
Butti Way | APN: 010-041-76 | Carson City | Nevada

