

STAFF REPORT FOR PLANNING COMMISSION MEETING OF JANUARY 27, 2021

FILE NO: SUP-2020-0050

AGENDA ITEM: E-7

STAFF CONTACT: Hope Sullivan, Planning Manager

AGENDA TITLE: For Possible Action: Discussion and possible action regarding a Special Use Permit for outdoor storage with personal storage within an enclosed building on property zoned General Commercial and Public Regional, located on the west side of Airport Road, south of Butti Way, APN 010-041-76. (Hope Sullivan, hsullivan@carson.org).

STAFF SUMMARY: The applicant is proposing a storage facility with a 1,000 square foot manager's office, 54,400 square feet of enclosed personal storage, covered storage space for 180 recreational vehicles, and a sanitary waste dump station. As the property has two zoning districts, and the use is a conditional use in one of the two zoning districts, a Special Use Permit is required. The Planning Commission is authorized to approve a Special Use Permit.

RECOMMENDED MOTION: "I move to approve SUP-2020-0050 based on findings and subject to conditions of approval contained in the staff report."

VICINITY MAP:



RECOMMENDED CONDITIONS OF APPROVAL:

1. The applicant must sign and return the Notice of Decision for conditions for approval within 10 days of receipt of notification. If the Notice of Decision is not signed and returned within 10 days, then the item may be rescheduled for the next Planning Commission meeting for further consideration.
2. All development shall be substantially in accordance with the development plans approved with this application, except as otherwise modified by these conditions of approval.
3. All on and off-site improvements shall conform to City standards and requirements.
4. The use for which this permit is approved shall commence within 12 months of the date of final approval. A single, one year extension of time may be requested in writing to the Planning Division thirty days prior to the one year expiration date. Should this permit not be initiated (obtain a Building Permit) within one year and no extension granted, the permit shall become null and void.
5. All requirements of the Lompa Ranch North Specific Plan development agreement must be met.
6. The LP dispenser should be shown on the building permit and tank size must be indicated. Installation must be approved by the NV LPG Board.
7. A site specific geotechnical investigation must be performed as recommended by the feasibility geotechnical investigation, and construction recommendations be incorporated into the design.
8. The multi-use path must be extended along the frontage as shown.
9. The project must meet the Carson City Municipal Code, Development Standards, and Standard Detail requirements including:
 - The project must balance flood zone cut and fill quantities, or may have a net gain of cut to increase storm water detention. There may not be a net gain of fill.
 - The project must provide funds to the City to process a CLOMR.
 - The water main must be extended along the frontage as shown.
10. The project site is adjacent to a future three acre dog park associated with the proposed Lompa Ranch East development. The applicant will be required to install the screen wall upslope from the park's south property line and adjacent to the storage facility. The landscape design for the screening buffer between the proposed park and project site will be reviewed and approved by the Parks, Recreation & Open Space Department.
11. The Unified Pathway Master Plan identifies Airport Road as an on-street bicycle facility. The applicant will be required to protect the existing bike lanes during the project's construction and repair lanes/restripe the pavement markings at their expense, if damage occurs. Any repairs and restriping to the bike lanes must be approved by Development Engineering.
12. Design all project driveway approaches that cross the 10' pedestrian multi-use path along Airport Road to meet ADA standards.

13. The applicant will be required to maintain all common landscape and open space areas on the project site in perpetuity, including any landscaping in the road right-of-way.
14. Incorporate “best management practices” into the applicant’s construction documents and specifications to reduce the spread of noxious weeds. The Parks, Recreation & Open Space Department is willing to assist the applicant with this aspect of their project.
15. The applicant will be required to remove any graffiti on the perimeter screening wall, per Carson City Municipal Code.
16. A detailed landscaping plan is required at the time of site improvement permit. This plan shall demonstrate that disturbed areas are either landscaped or otherwise improved, shall utilize drought tolerant plants, shall incorporate a buried irrigation system, and shall incorporate a minimum 15 foot wide landscape strip along Airport Road planted with 1 tree per 25 lineal feet of street frontage, and six shrubs per tree.
17. A detailed lighting plan is required at the time of site improvement permit and shall include a photometric survey demonstrating even lighting throughout the site, with no spillover lighting.
18. Proposed signage requires a sign permit and shall meet the standards of Section 2.1.6 of the Lompa Ranch North Specific Plan.
19. The site must be fully enclosed with six foot solid fencing consistent with fencing proposed, acknowledging that the masonry rear of the storage buildings may function as the screening device in lieu of the addition of fencing.
20. Site improvement plans shall show the location of the wash area and dumpster locations. These areas must be inside the six foot fence.
21. Site improvement plans must include a detail of the dumpster enclosures and placement consistent with Section 2.1.8 of the Lompa Ranch North Specific Plan.
22. A landscape plan meeting the requirements of Division 4 of the Development Standards is required at the time of building permit. The plan shall include trees in the parking aisle planters. Plantings along the right-of-way shall be provided at a rate of 1 tree per 25 lineal feet of street frontage with a minimum of six shrubs per tree.

LEGAL REQUIREMENTS: CCMC 18.02.080 (Special Use Permits), 18.04.135 General Commercial (GC); 18.04.185 Public Regional (PR); 18.04.020 Determination of Districts

MASTER PLAN DESIGNATION: Mixed Use Residential and Mixed Use Employment, Lompa Ranch North Specific Plan

PRESENT ZONING: General Commercial and Public Regional

KEY ISSUES: Does the proposed storage facility meet the required findings for approval of a Special Use Permit? Is the proposed use compatible with surrounding properties?

SURROUNDING ZONING AND LAND USE INFORMATION:

NORTH: Single Family 6,000 / vacant (tentative map approved)

EAST: Public Regional / Animal Facility

SOUTH: Agriculture / Drainage Facility

WEST: Agriculture / Drainage Facility

ENVIRONMENTAL INFORMATION:

FLOOD ZONE: Zone AE (100 year flood plain), XS (Between 100 year and 500 year flood plain), and X (areas of minimal flooding)

EARTHQUAKE FAULT: Severe, Zone I

SLOPE/DRAINAGE: Flat

SITE DEVELOPMENT INFORMATION:

LOT SIZE: 7.28 acres

EXISTING LAND USE: Vacant

VARIANCES REQUESTED: None

DISCUSSION:

A Special Use Permit is required for the following reason:

- According to CCMC Title 18.04.020.6.d where a parcel of land is divided by a zoning district boundary, the following shall apply: "To utilize the entire parcel with a use that is only allowed in 1 of the 2 zoning districts, a zoning map amendment or special use permit is required for that portion of the property not zoned for the use."

The vacant property is split zoned General Commercial and Public Regional. Outside storage and personal storage uses are allowed in General Commercial zoning, but not allowed in Public Regional zoning.

At its meeting of June 26, 2019, the Planning Commission approved a Special Use Permit for a covered RV and boat storage facility (outside storage) for approximately 275 vehicles.

At its meeting of November 19, 2019, the Planning Commission approved a modification to the Special Use Permit to allow 30,000 square feet of personal storage in an enclosed building.

As construction has not commenced, the special use permit has expired. The subject application is substantially similar to the special use permit and modification approved in 2019. Difference are that the RV storage along Airport Road will be covered, but not enclosed; the wash bay has been eliminated, and the RV dump station has been relocated.

The facility will include 54,400 square feet (approximately 300 units) of enclosed personal storage, covered storage for approximately 180 recreation vehicles, a 1000 square foot sales office on site that will include a small retail component with items related to RVs and boats. The site will also have a dumping station and a propane refill area. No mechanic services will be on site, and customers will be prohibited from repairing or maintaining their vehicles on site.

Enclosed personal storage buildings will be located along the north, south, and west perimeters of the property. The rear of the personal storage buildings will consist of masonry walls with a height

of eight and a half feet. The wall will include a split faced masonry pier and accent strip. A corrugated metal roof panel will be utilized, and the front of the building, which face interior to the site, will utilize metal wall panels with metal overhead doors.

Hours of operation are still proposed to be 6:00 AM to 10:00 PM, with the office open from 8:00 AM to 6:00 PM daily. The site will be gated. Customers will have a gate code. The site will be fully enclosed with solid six foot fencing, recognizing that where the exterior wall of the personal storage building creates a barrier, additional fencing will not be required.

Per the provisions of 18.02.080, the Planning Commission has the authority to approve a Special Use Permit upon making each of the seven required findings in the affirmative.

PUBLIC COMMENTS: Public notices were mailed to 42 property owners within 950 feet of the subject property on January 14, 2021. As of the writing of this report, no comments have been received. Any comments that are received after this report is completed will be submitted to the Planning Commission prior to or at the meeting on January 27, 2021, depending on the date of submittal of the comments to the Planning Division.

OTHER CITY DEPARTMENTS OR OUTSIDE AGENCY COMMENTS: The following comments were received by various city departments. Recommendations that are not code requirements have been incorporated into the recommended conditions of approval, where applicable.

Fire Department:

Project must comply with the International Fire Code and northern Nevada fire code amendments as adopted by Carson City.

Engineering Department:

The Engineering Division has no preference or objection to the special use request provided that the following conditions are met:

- The conditions of SUP-19-083 must be met.

The Engineering Division has reviewed the application within our areas of purview relative to adopted standards and practices and to the provisions of CCMC 18.02.080, Conditional Uses. The Engineering Division offers the following discussion:

C.C.M.C. 18.02.080 (5a) - Master Plan

The request is not in conflict with any Engineering Master Plans.

C.C.M.C. 18.02.080 (5b) – Use, Peaceful Enjoyment, Economic Value, Compatibility

Development Engineering has no comment on this finding.

C.C.M.C. 18.02.080 (5c) - Traffic/Pedestrians

The existing infrastructure and proposed drive isles are sufficient to provide safe access and circulation if conditions are met. Current and projected traffic volumes in the area have not changed to a degree that would warrant new mitigation measures or further study for the subject project.

C.C.M.C. 18.02.080 (5d) - Public Services

The existing sewer, water, and storm drain infrastructure are sufficient to provide service to the project. Current and projected demand on these utilities in the area has not changed to a degree

that would warrant new mitigation measures or further study for the subject project.

C.C.M.C. 18.02.080 (5e) – Title 18 Standards

Development Engineering has no comment on this finding.

C.C.M.C. 18.02.080 (5f) – Public health, Safety, Convenience, and Welfare

The project meets engineering standards for health and safety.

FINDINGS: Staff's recommendation is based upon the findings as required by CCMC Section 18.02.080 (Special Use Permits) enumerated below and substantiated in the public record for the project.

1. Will be consistent with the objectives of the Master Plan elements.

The subject property is part of the Lompa Ranch North Specific Plan, and is designated as Mixed Use Residential. This is the only non-residential component in the portion of the Lompa Ranch North Specific Plan east of the freeway.

Section 2.1 of the Specific Plan provides design standards and guidelines for the commercial uses, as follows.

2.1 Commercial Uses

2.1.1 Commercial Site Planning Standards

a) Building placement and orientation shall be designed to create visual interest along public streets. Multiple buildings in a single project shall demonstrate a positive functional relationship to one another.

Two buildings are proposed along Airport Road, an office building and the rear of a roofed RV storage building. In addition, a six foot solid fence utilizing vertical board and dense landscaping will be located along Airport Road. The parking to serve the building is to the side of the building as opposed to between the street and the building. Staff finds this design to be consistent with the Master Plan.

b) To the extent possible, buildings located within a single project shall be clustered. Plazas and pedestrian areas shall also be an important element in the design of clustered buildings. When clustering is impractical, a visual link should be established between buildings through the use of architectural features, landscaping, etc.

The storage buildings on the site will be on the perimeter, and the office building is along Airport Road. Given the secured nature of the use, it does not lend itself to plazas and the like.

c) For general commercial uses, a minimum of 15 percent of the building area should be located at or near the front setback line. This minimizes large, continuous areas of parking and encourages

active streetscapes.

Both buildings along Airport Road are located at the front setback line.

d) Buildings shall be oriented so that public access or windows face adjoining streets.

The office building is oriented so that windows and a stone facade face the street. Fencing with landscaping on the exterior will be providing a visual screen between the Airport Road and the RV storage canopy.

e) Plazas or common areas within a project shall be located near building entrances or areas of high pedestrian traffic to ensure their use.

Given the proposed use, a plaza is not recommended as it will not be a use where people congregate or linger.

f) To the extent possible, areas between buildings shall be utilized for plazas, outdoor seating, or landscape features in order to eliminate "dead zones" of underutilized space.

Given the proposed use, a plaza is not recommended as it will not be a use where people congregate or linger.

g) Bicycle racks shall be provided within all commercial centers.

The proposed use is not a commercial center.

2.1.2 Commercial Grading and Drainage

a) Design of commercial uses shall be sensitive to the natural terrain, and structures should be located to minimize necessary grading and preserve natural site features such as drainageways, wetlands, etc. Grading of commercial sites should blend with the natural topography of the site.

The subject property has drainage facilities on two sides. The site will maintain existing drainage patterns sloping from north to south where flow will be collected and treated in a detention pond before being discharged to the detention facilities to the south.

b) Grading within commercial areas shall be designed to complement the architectural and landscape design character of the center and surrounding area. Grading techniques can be used to screen parking and service areas, reduce the perception of height and mass on larger buildings, and provide reasonable transitions between uses.

The site is being graded in a manner to allow for additional stormwater detention to the south.

c) Graded slopes should properly transition to existing natural terrain at project borders.

The proposed grading is intended to allow for additional stormwater detention.

d) Man-made slopes shall not exceed an average of 3:1 slope and turf areas shall not exceed an average 4:1 slope.

Slopes will be 6:1 adjacent to the drainage basins in order to provide for additional stormwater detention.

e) Areas disturbed by grading activities shall be revegetated prior to the issuance of a certificate of occupancy. If climatic conditions or other circumstances prevent planting at the time of occupancy, a bond shall be provided for landscaping during the subsequent growing season. Drought tolerant plant species shall be utilized to help minimize erosion.

A detailed landscaping plan will be obtained at the time of site improvement permit to ensure disturbed areas are either landscaped or otherwise improved.

f) New commercial developments must include a final hydrology report to be reviewed and approved by the Carson City Engineering Department prior to the issuance of a building permit.

The applicant has submitted a drainage letter prepared by a licensed engineer. A detailed drainage report will be required at the time of site improvement permit review.

g) An erosion control plan shall be included with each grading permit.

A detailed erosion control plan will be required at the time of site improvement permit review.

2.1.3 Commercial Parking Lots

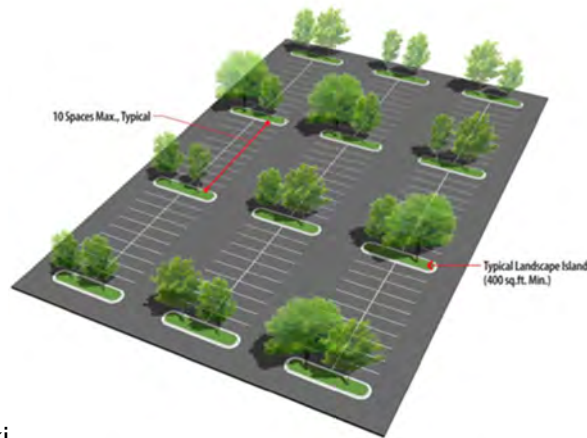
a) A minimum of 10 feet of landscaping shall be provided between parking lots and the public streets.

A ten foot landscape area is provided along Airport Road.

b) A minimum 400 square foot interior planter shall be provided at the end of parking aisles (refer to example to the right). Planters shall include a minimum of one deciduous tree (min. two inch caliper).

Landscaping is located at each end of the parking lot. Trees will be required to be added at the ends of the parking aisles.

c) *Landscape islands (minimum of 400 square feet) shall be provided for every 10 spaces in large parking fields and shall include a minimum of one deciduous tree (two inch caliper minimum). See example below.*



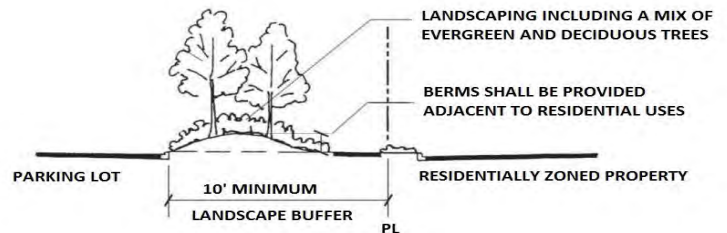
The parki

ng lot only has three spaces.

d) *Pedestrian connections between parking lots and buildings shall be provided along with connections to sidewalks along adjoining public streets.*

The project will include the extension of the multi-use path along the frontage of the property.

e) *Parking should be located to the side and rear of a project site where feasible. However, no more than 10 percent of the required parking shall be in the rear service area (typically not used for general public access) of a project site.*



Parking is located to the side of the building.

f) *Parking areas shall be screened from adjoining residential areas through the use of landscaping and berming. This buffer shall be a minimum of 10 feet in width (see example to right).*

The subject property has drainage facilities on two sides, a proposed dog park to the north, and the road to the east.

g) *Commercial centers that include tenants that utilize shopping carts shall provide a "cart corral" within 150 feet of 85 percent of their parking stalls.*

The proposed use will not utilize shopping carts.

h) For commercial centers exceeding 5 acres, a maintenance plan shall be required for parking lots that includes regular sweeping and a snow removal/storage plan for winter weather events.

The proposed use is not a commercial center.

i) For commercial centers adjoining residential areas, parking lot sweeping shall be limited to the hours between 8:00 am and 9:00 pm.

The proposed use is not commercial center.

j) Parking lot design, including space dimensions, aisle widths, etc. shall comply with the provisions of the Carson City Municipal Code.

The proposed parking lot design complies with the Municipal Code.

k) Outdoor sales or special events may not reduce parking past minimum requirements mandated in the Carson City Municipal Code.

Outdoor sales and special events are not proposed on the subject property.

2.1.4 Commercial Landscaping

a) Landscaping, including plant materials and themes shall be consistent throughout the Lompa Ranch North SPA.

It is anticipated that this will be the first project built on the east side of Lompa Ranch North Specific Plan Area. The plants will establish the theme for the balance of the east side.

b) Landscaping standards contained in the Carson City Development Standards shall apply within Lompa Ranch North.

Proposed landscaping will be consistent with the City's landscaping standards.

c) Within commercial centers, areas not utilized for parking, buildings, plazas, or access/circulation shall be landscaped to the back of curb. Unbuilt pad areas shall be excluded from this standard.

The proposed use is not a commercial center.

d) Drought tolerant plantings shall be used in conjunction with low water demand principles and techniques.

A detailed landscape plan will be submitted with the site improvement permit. Staff will verify landscaping is comprised of drought tolerant plants.

e) All landscaped areas shall be irrigated with permanent automatic irrigation systems. All irrigation systems shall be placed underground.

A detailed landscape plan will be submitted with the site improvement permit. Staff will verify the plans call for a buried irrigation system.

f) Landscape maintenance within commercial areas shall be the responsibility of individual property owners or completed through a private maintenance association.

All landscaping shall be the responsibility of the property owner.

g) Landscaping along adjoining rights-of-way shall be a minimum width of 15 feet and provide a mix of trees, shrubs, and living groundcover. Trees shall be provided at a rate of 1 tree per 25 lineal feet of street frontage with a minimum of six shrubs per tree.

The plans call for a 30 foot landscaped area along the right-of-way. A detailed landscape plan will be submitted with the site improvement permit. Staff will verify that the tree and shrubs meet the required numbers.

2.1.5 Commercial Lighting

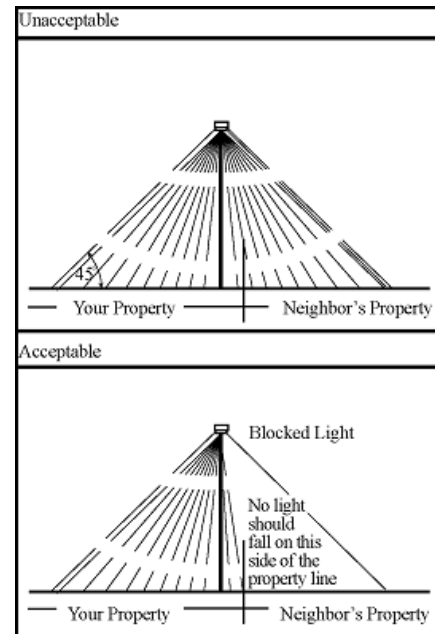
a) Adequate lighting shall be provided to ensure a safe pedestrian environment.

A detailed lighting plan will be submitted at the time of building permit. Staff will ensure the adequacy of the plan at that time.

b) Parking lot lighting within 75 feet of residential areas shall be limited to 12 feet in height and shall incorporate shielded fixtures. Additional height limitations for parking lot lighting within certain distances of residential areas are identified in the Carson City Development Standards.

The parking lot is not within 75 feet of residential areas.

c) Parking lot lighting shall use shielded/directed fixtures to ensure that spill-over and glare do not occur on adjoining properties.



A detailed lighting plan will be submitted at the time of building permit. Staff will ensure the adequacy of the plan at that time.

d) The use of bollard lighting is encouraged in pedestrian areas.

Given the use, there are not identified pedestrian areas.

e) Exterior lighting shall be used for purposes of illumination and safety only, and shall not be designed for, or used as, an advertising display.

A detailed lighting plan will be submitted at the time of building permit. Staff will ensure the adequacy of the plan at that time.

2.1.6 Commercial Signs

Signs and their integration into the project is a critical element in the design of Lompa Ranch North. Careful use of forms, styles, materials, and colors will establish continuity throughout the community. Signs are intended to be utilized only where necessary, and in an understated manner, emphasizing an image of permanence and quality.

a) Signs shall be included on facades or entry canopies of buildings and illuminated or backlit with indirect lighting. All tenant identification signs shall be consistently located and integrated into the architectural design of the building entry. Storefront signs shall be proportional with the building architecture.

The applicant has identified the location of a sign along Airport Road, but a specific sign has not been proposed. A sign permit will be required prior to erecting any signs. Compliance will be determined at that time.

b) Flashing or animated signs are prohibited.

A sign permit will be required prior to erecting any signs. Compliance will be determined at that time.



c) Building signs that project more than 4 inches beyond the wall façade are prohibited, unless incorporated as an architectural element.

A sign permit will be required prior to erecting any signs. Compliance will be determined at that time.

d) *Hanging signs may be included under eaves above walkways and shall maintain a minimum of 8 feet of clearance. These signs shall be architecturally compatible with the building they serve.*

A sign permit will be required prior to erecting any signs. Compliance will be determined at that time.



2.1.7 Commercial Fencing

a) *Walls and fences shall be utilized to provide a buffer between incompatible uses. It is important, however, that walls are appropriately integrated into each project.*

The applicant has provided a wall detail for a six foot tall fence. The use is required to utilize a six foot solid wall of fence to screen the use from adjacent properties. Also, the personal storage buildings will be located along the exterior of the site. The masonry rear of the buildings will also provide for buffering.

b) *Solid fencing (6 foot minimum) shall be installed between commercial uses within Lompa Ranch North and adjoining residential uses. This can include wood or vinyl fencing, concrete block walls, pre-cast wall systems, or similar.*

The applicant has provided a wall detail for a six foot tall fence. The use is required to utilize a six foot solid wall of fence to screen the use from adjacent properties. Also, the personal storage buildings will be located along the exterior of the site. The masonry rear of the buildings will also provide for screening.

c) *Chain link fencing shall be prohibited within commercial centers.*

The applicant's wall detail is not chain link fencing.

2.1.8 Commercial Trash and Utility Areas

a) *Service and maintenance areas shall be screened from adjacent public right-of-ways, pedestrian plazas or adjacent residential uses with landscaped berms, walls or plantings. Storage areas shall be enclosed by a 100% site obscuring fence or wall, permanently installed and maintained at a minimum height of six feet.*

A dumpster location is proposed adjacent to the office, and will be screened by landscaping and fencing.

b) All trash and garbage bins shall be stored in an enclosure that includes solid screening, to the approval of the Carson City Community Development Department.

At the time of building permit, staff will verify dumpster enclosures.

c) Trash enclosures shall incorporate building materials, colors, etc. that are complementary to the overall project architecture. Gates shall be constructed of durable building materials that screens at a minimum 80% of the view into the trash enclosure. Wood or chain link gates are not allowed (see example to left).

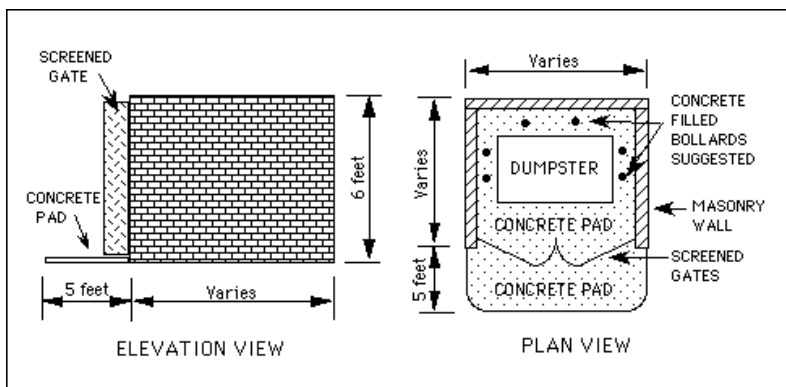
At the time of building permit, staff will verify dumpster enclosures.

d) Trash enclosures must include provisions for concrete pads or appropriately designed asphalt sections in front of the enclosure. The area in front of the trash enclosure shall be a minimum of six (6) feet to reduce pavement damage from disposal trucks.

At the time of building permit, staff will verify the provision of a concrete pad or appropriately designed asphalt section in front of the trash enclosure, as well as a minimum of six feet to reduce pavement damage.

2. Will not be detrimental to the use, peaceful enjoyment, economic value, or development of surrounding properties or the general neighborhood; and is compatible with and preserves the character and integrity of adjacent development and neighborhoods or includes improvements or modifications either on-site or within the public right-of-way to mitigate development related to adverse impacts such as noise, vibrations, fumes, odors, dust, glare or physical activity.

The subject property is across the street from the animal shelter, to the south of the proposed dog park, and to the east and north of drainage facilities. The proposed



personal storage use is a quiet use, and will likely establish before any of the houses in Lompa East. Staff does not find that the proposed use will be detrimental to the use, peaceful enjoyment, economic value or development of surrounding properties or the general neighborhood.

3. Will have little or no detrimental effect on vehicular or pedestrian traffic.

Based on the Institute of Traffic Engineer's Trip Generation Manual, the project will generate 63 daily trips. The existing roadway network can accommodate these trips. The addition of outdoor and personal storage uses is not anticipated to have a detrimental effect on pedestrian traffic. In fact, due to the extension of the multi-use trail, the use will have a positive effect on pedestrian traffic.

4. Will not overburden existing public services and facilities, including schools, police and fire protection, water, sanitary sewer, public roads, storm drainage, and other public improvements.

The proposed outdoor and personal storage uses will not overburden existing public services. It is not residential, so there will be no students. The existing sewer, water and storm drain infrastructure are sufficient to provide service to the project.

5. Meets the definition and specific standards set forth elsewhere in this Title for such particular use and meets the purpose statement of that district.

Although the outdoor and personal storage uses are not allowed in the Public Regional zoning district, as the site is split zoned, the use may establish subject to a Special Use Permit.

6. The use will not be detrimental to the public health, safety, convenience and welfare.

The proposed outdoor and personal storage use will not be detrimental of public health, safety, convenience and welfare. It is a fairly passive use.

7. Will not result in material damage or prejudice to other property in the vicinity, as a result of proposed mitigation measures.

The proposed outdoor and personal storage use will not result in material damage or prejudice to other property in the vicinity.

Attachment: Application LU-2020-0050

Special Use Permit
FOR
Sierra Tahoe
RV, Boat & Self Storage

Prepared For:

Sierra Tahoe RV & Boat Storage
Attn: Rick Ardizzone
892 Southwood Blvd.
Incline Village, NV 89451
408.891.8149

Prepared By:



575 E. Plumb Lane, Suite 101
Reno, NV 89502
775.636.7905

December 2020

18.041

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Carson City
Community Development Department
108 E. Proctor Street
Carson City, NV 89701

December 18, 2020

RE: Sierra Tahoe RV & Boat Storage (SUP-19-083) – Special Use Permit Modification

The owners of the proposed Sierra Tahoe RV & Boat Storage did not formally request an extension of their previous special use permit approval which has now expired and therefore are resubmitting for a new Special Use Permit. The original Special Use Permit application was approved at the June 26, 2019 Planning Commission Meeting. A modification to that application was subsequently approved at the November 19, 2019 Planning Commission Meeting. The project has since been submitted to Carson City for building permit approval. The following application includes minor modifications to the previously approved Special Use Permit applications based on the building permit documents as well as revisions requested by the various reviewing agencies. The overall form of the project remains essentially the same as proposed in the modified special use permit application which includes the construction of a $\pm 1,000$ s.f. managers office, $\pm 54,400$ s.f. (± 300 units) of enclosed personal storage, covered storage spaces for ± 180 recreational vehicles and a sanitary waste dump station. The notable changes are the replacement of the enclosed RV storage building along Airport Road with covered RV storage and the omission of the previously proposed wash bay and the relocation of the RV dump station. We believe these proposed modifications are relatively minor and that the overall scope of the project is consistent with previous approvals.

The following is a modified application which addresses the proposed project specifics. Again, only minor modifications are proposed with this application when compared to the original approvals. Those modifications have been made to ensure consistency between the special use permit application and construction documents which are completed at this time. Please contact Monte Vista Consulting if you have any questions or if there is anything else I can help with.

Sincerely,
Monte Vista Consulting

Michael Vicks, P.E.
Principal



Carson City Planning Division

108 E. Proctor St.

Carson City, Nevada 89701

(775) 887-2180

Planning@carson.org

www.carson.org

★ CLERK ★
FILED

Time 4:46pm

PLANNING COMMISSION

November 19, 2019

NOV 26 2019

By Lena E. Resch
Deputy
Carson City, Nevada

NOTICE OF DECISION – SUP-19-083-1

An application was received regarding an amendment to a Special Use Permit for Outdoor Storage to allow for personal storage within an enclosed building on property zoned General Commercial and Public Regional, located on the west side of Airport Road, south of Buttl Way, APN 010-041-76.

The Planning Commission conducted a public hearing on November 19, 2019, in conformance with City and State legal requirements and approved SUP-19-083-1 based on the findings contained in the staff report and subject to the following conditions of approval.

CONDITIONS OF APPROVAL:

The below conditions are the original conditions of approval. Proposed additions appear in bold and underlined.

1. The applicant must sign and return the Notice of Decision for conditions for approval within 10 days of receipt of notification. If the Notice of Decision is not signed and returned within 10 days, then the item may be rescheduled for the next Planning Commission meeting for further consideration.
2. All development shall be substantially in accordance with the development plans approved with this application, except as otherwise modified by these conditions of approval.
3. All on- and off-site improvements shall conform to City standards and requirements.
4. The use for which this permit is approved shall commence within 12 months of the date of final approval. A single, one year extension of time may be requested in writing to the Planning Division thirty days prior to the one year expiration date. Should this permit not be initiated (obtain a Building Permit) within one year and no extension granted, the permit shall become null and void.
5. All requirements of the Lompa Ranch North Specific Plan development agreement must be met.
6. The LP dispenser should be shown on the building permit and tank size must be indicated. Installation must be approved by the NV LPG Board.
7. A site specific geotechnical investigation must be performed as recommended by the feasibility geotechnical investigation, and construction recommendations be incorporated into the design.
8. The multi-use path must be extended along the frontage as shown.
9. The project must meet the Carson City Municipal Code, Development Standards, and Standard Detail requirements including:
 - The project must balance flood zone cut and fill quantities, or may have a net gain of cut to increase storm water detention. There may not be a net gain of fill.
 - The project must provide funds to the City to process a CLOMR.
 - The water main must be extended along the frontage as shown.

10. The project site is adjacent to a future three acre dog park associated with the proposed Lompa Ranch East development. The applicant will be required to install the screen wall upslope from the park's south property line and adjacent to the storage facility. The landscape design for the screening buffer between the proposed park and project site will be reviewed and approved by the Parks, Recreation & Open Space Department.
11. The Unified Pathway Master Plan identifies Airport Road as an on-street bicycle facility. The applicant will be required to protect the existing bike lanes during the project's construction and repair lanes/restripe the pavement markings at their expense, if damage occurs. Any repairs and restriping to the bike lanes has to be approved by Development Engineering.
12. Design all project driveway approaches that cross the 10' pedestrian multi-use path along Airport Road to meet ADA standards.
13. The applicant will be required to maintain all common landscape and open space areas on the project site in perpetuity, including any landscaping in the road right-of-way.
14. Incorporate "best management practices" into the applicant's construction documents and specifications to reduce the spread of noxious weeds. The Parks, Recreation & Open Space Department is willing to assist the applicant with this aspect of their project.
15. The applicant will be required to remove any graffiti on the perimeter screening wall, per Carson City Municipal Code.
16. A detailed landscaping plan is required at the time of site improvement permit. This plan shall demonstrate that disturbed areas are either landscaped or otherwise improved, shall utilize drought tolerant plants, shall incorporate a buried irrigation system, and shall incorporate a minimum 15 foot wide landscape strip along Airport Road planted with 1 tree per 25 lineal feet of street frontage, and six shrubs per tree.
17. A detailed lighting plan is required at the time of site improvement permit and shall include a photometric survey demonstrating even lighting throughout the site, with no spillover lighting.
18. Proposed signage requires a sign permit and shall meet the standards of Section 2.1.6 of the Lompa Ranch North Specific Plan.
19. The site must be fully enclosed with six foot solid fencing consistent with fencing proposed, **acknowledging that the masonry rear of the storage buildings may function as the screening device in lieu of the addition of fencing.**
20. Site improvement plans shall show the location of the wash area and dumpster locations. These areas must be inside the six foot fence.
21. Site improvement plans must include a detail of the dumpster enclosures and placement consistent with Section 2.1.8 of the Lompa Ranch North Specific Plan.
22. **A landscape plan meeting the requirements of Division 4 of the Development Standards is required at the time of building permit. The plan shall include trees in the parking aisle planters. Plantings along the right-of-way shall be provided at a rate of 1 tree per 25 lineal feet of street frontage with a minimum of six shrubs per tree.**

This decision was made on a vote of 7 ayes, 0 nays, 0 absent.

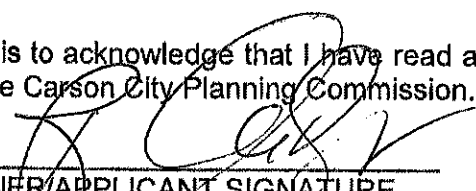

Hope Sullivan, AICP
Planning Manager

Emailed on: 11/27/19

By: UR

**PLEASE SIGN AND RETURN THIS NOTICE OF DECISION WITHIN
TEN DAYS OF RECEIPT**

This is to acknowledge that I have read and will comply with the Conditions of Approval as approved
by the Carson City Planning Commission.


OWNER/APPLICANT SIGNATURE

DATE

Dec 2, 2019

RICK ARDIZZONE
PLEASE PRINT YOUR NAME HERE

RETURN VIA:

Email to: lreseck@carson.org

Fax to: (775) 887-2278

Mail to: Carson City Planning Division
108 E. Proctor St.
Carson City, NV 89701



Carson City Planning Division

108 E. Proctor St.
Carson City, Nevada 89701
(775) 887-2180
Planning@carson.org
www.carson.org

★ CLERK ★
FILED
Time 1:45pm

PLANNING COMMISSION
June 26, 2019

JUL 10 2019

By Lena E. Besack
Deputy
Carson City, Nevada

NOTICE OF DECISION – SUP-19-083

An application was received for Outdoor Storage on property zoned General Commercial and Public Regional, located on the west side of Airport Road, south of Butti Way, APN 010-041-76.

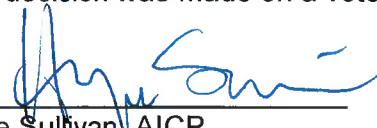
The Planning Commission conducted a public hearing on June 26, 2019, in conformance with City and State legal requirements and approved SUP-19-083 based on the findings contained in the staff report and subject to the following conditions of approval.

CONDITIONS OF APPROVAL:

1. The applicant must sign and return the Notice of Decision for conditions for approval within 10 days of receipt of notification. If the Notice of Decision is not signed and returned within 10 days, then the item may be rescheduled for the next Planning Commission meeting for further consideration.
2. All development shall be substantially in accordance with the development plans approved with this application, except as otherwise modified by these conditions of approval.
3. All on- and off-site improvements shall conform to City standards and requirements.
4. The use for which this permit is approved shall commence within 12 months of the date of final approval. A single, one year extension of time may be requested in writing to the Planning Division thirty days prior to the one year expiration date. Should this permit not be initiated (obtain a Building Permit) within one year and no extension granted, the permit shall become null and void.
5. All requirements of the Lompa Ranch North Specific Plan development agreement must be met.
6. The LP dispenser should be shown on the building permit and tank size must be indicated. Installation must be approved by the NV LPG Board.
7. A site specific geotechnical investigation must be performed as recommended by the feasibility geotechnical investigation, and construction recommendations be incorporated into the design.
8. The multi-use path must be extended along the frontage as shown.
9. The project must meet the Carson City Municipal Code, Development Standards, and Standard Detail requirements including:
 - The project must either balance flood zone cut and fill quantities or may have a net gain of cut to increase stormwater detention. There may not be a net gain of fill.
 - The project must provide funds to the City to process a CLOMR.
 - The water main must be extended along the frontage as shown.
10. The project site is adjacent to a future three acre dog park associated with the proposed Lompa Ranch East development. The applicant will be required to install the screen wall upslope from the park's south property line and adjacent to the storage facility. The landscape design for the screening buffer between the proposed park and project site will be reviewed and approved by the Parks, Recreation & Open Space Department.

11. The Unified Pathway Master Plan identifies Airport Road as an on-street bicycle facility. The applicant will be required to protect the existing bike lanes during the project's construction and repair lanes/restripe the pavement markings at their expense, if damage occurs. Any repairs and restriping to the bike lanes has to be approved by Development Engineering.
12. Design all project driveway approaches that cross the 10' pedestrian multi-use path along Airport Road to meet ADA standards.
13. The applicant will be required to maintain all common landscape and open space areas on the project site in perpetuity, including any landscaping in the road right-of-way.
14. Incorporate "best management practices" into the applicant's construction documents and specifications to reduce the spread of noxious weeds. The Parks, Recreation & Open Space Department is willing to assist the applicant with this aspect of their project.
15. The applicant will be required to remove any graffiti on the perimeter screening wall, per Carson City Municipal Code.
16. A detailed landscaping plan is required at the time of site improvement permit. This plan shall demonstrate that disturbed areas are either landscaped or otherwise improved, shall utilize drought tolerant plants, shall incorporate a buried irrigation system, and shall incorporate a minimum 15 foot wide landscape strip along Airport Road planted with 1 tree per 25 lineal feet of street frontage, and six shrubs per tree.
17. A detailed lighting plan is required at the time of site improvement permit and shall include a photometric survey demonstrating even lighting throughout the site, with no spillover lighting.
18. Proposed signage requires a sign permit and shall meet the standards of Section 2.1.6 of the Lompa Ranch North Specific Plan.
19. The site must be fully enclosed with six foot solid fencing consistent with fencing proposed.
20. Site improvement plans shall show the location of the wash area and dumpster locations. These areas must be inside the six foot fence.
21. Site improvement plans must include a detail of the dumpster enclosures and placement consistent with Section 2.1.8 of the Lompa Ranch North Specific Plan.

This decision was made on a vote of 6 ayes, 0 nays, 1 absent, 0 recusal.



Hope Sullivan, AICP
Planning Manager

HS:lr
Emailed on: 7/10/19

By: lp

**PLEASE SIGN AND RETURN THIS NOTICE OF DECISION WITHIN
TEN DAYS OF RECEIPT**

This is to acknowledge that I have read and will comply with the Conditions of Approval as approved by the Carson City Planning Commission.

OWNER/APPLICANT SIGNATURE

DATE

PLEASE PRINT YOUR NAME HERE

RETURN VIA:

Email to: lreseck@carson.org
Fax to: (775) 887-2278
Mail to: Carson City Planning Division
108 E. Proctor St.
Carson City, NV 89701

Carson City Planning Division
108 E. Proctor Street • Carson City NV 89701
 Phone: (775) 887-2180 • E-mail: planning@carson.org

FOR OFFICE USE ONLY:

CCMC 18.02.080

SPECIAL USE PERMIT

FEE*: \$2,450.00 MAJOR
\$2,200.00 MINOR (Residential
zoning districts)
+ noticing fee

*Due after application is deemed complete by staff

☐ **SUBMITTAL PACKET – 4 Complete Packets (1 Unbound Original and 3 Copies) including:**

- ☐ Application Form
- ☐ Detailed Written Project Description
- ☐ Site Plan
- ☐ Building Elevation Drawings and Floor Plans
- ☐ Special Use Permit Findings
- ☐ Master Plan Policy Checklist
- ☐ Applicant's Acknowledgment Statement
- ☐ Documentation of Taxes Paid-to-Date
- ☐ Project Impact Reports (Engineering)

☐ **CD or USB DRIVE with complete application in PDF**

Application Received and Reviewed By: _____

Submittal Deadline: Planning Commission application submittal schedule.

Note: Submittals must be of sufficient clarity and detail for all departments to adequately review the request. Additional information may be required.

FILE

APPLICANT PHONE #
 Seirra Tahoe RV & Boat Storage LLC 408.891.8149

MAILING ADDRESS, CITY, STATE, ZIP
 561 San Benito Avenue, Los Gatos, CA 95030

EMAIL ADDRESS
 rick.ardizzone@compass.com

PROPERTY OWNER PHONE #
 Sierra Tahoe RV & Boat Storage LLC 408.891.8149

MAILING ADDRESS, CITY, STATE, ZIP
 561 San Benito Avenue, Los Gatos, CA 95030

EMAIL ADDRESS
 rick.ardizzone@compass.com

APPLICANT AGENT/REPRESENTATIVE PHONE #
 Monte Vista Consulting, Ltd 775.636.7905

MAILING ADDRESS, CITY STATE, ZIP
 720 Tahoe Street, Suite 1, Reno NV 89509

EMAIL ADDRESS

mike@montevistaconsulting.com

Project's Assessor Parcel Number(s):

010-041-76

Street Address

556 Airport Rd

Project's Master Plan Designation

Mixed-Use Employment/Open Space

Project's Current Zoning

PR/GC

Nearest Major Cross Street(s)

Airport Rd/Butte Way

Please provide a brief description of your proposed project and/or proposed use below. Provide additional pages to describe your request in more detail. The proposed project will a private RV & Boat, and Mini Storage Facility. Please see narrative for details

PROPERTY OWNER'S AFFIDAVIT

I, **Rick Ardizzone**, being duly deposed, do hereby affirm that I am the record owner of the subject property, and that I have knowledge of, and I agree to, the filing of this application.

Signature _____

561 San Benito Avenue, Los Gatos, CA 95030

Address

12/16/2020

Date

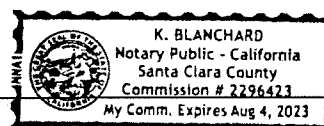
Use additional page(s) if necessary for additional owners.

STATE OF ~~NEVADA~~ **California**
 COUNTY ~~Santa Clara~~ **Santa Clara**

On **12/16/2020**, **Rick Ardizzone**, personally appeared before me, a notary public, personally known (or proved) to me to be the person whose name is subscribed to the foregoing document and who acknowledged to me that he/she executed the foregoing document.

Notary Public: **K. Blanchard**

NOTE: If your project is located within the Historic District or airport area, it may need to be scheduled before the Historic Resources Commission or the Airport Authority in addition to being scheduled for review by the Planning Commission. Planning staff can help you make this determination.

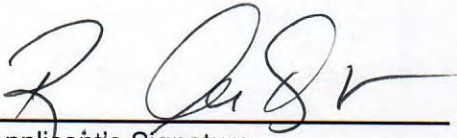


If there is any additional information that would provide a clearer picture of your proposal that you would like to add for presentation to the Planning Commission, please be sure to include it in your detailed description.

Please type and sign the statement on the following page at the end of your findings response.

ACKNOWLEDGMENT OF APPLICANT

I certify that the forgoing statements are true and correct to the best of my knowledge and belief. I agree to fully comply with all conditions as established by the Planning Commission. I am aware that this permit becomes null and void if the use is not initiated within one-year of the date of the Planning Commission's approval; and I understand that this permit may be revoked for violation of any of the conditions of approval. I further understand that approval of this application does not exempt me from all City code requirements.


Applicant's Signature
mgr member LLC

RICHARD ARDIZZONE *5.13.18*
Print Name Date

Master Plan Policy Checklist

Special Use Permits & Major Project Reviews & Administrative Permits

PURPOSE

The purpose of a development checklist is to provide a list of questions that address whether a development proposal is in conformance with the goals and objectives of the 2006 Carson City Master Plan that are related to non-residential and multi-family residential development. This checklist is designed for developers, staff, and decision-makers and is intended to be used as a guide only.

Development Name: Sierra Tahoe RV, Boat & Self Storage

Reviewed By: _____

Date of Review: _____

DEVELOPMENT CHECKLIST

The following five themes are those themes that appear in the Carson City Master Plan and which reflect the community's vision at a broad policy level. Each theme looks at how a proposed development can help achieve the goals of the Carson City Master Plan. A check mark indicates that the proposed development meets the applicable Master Plan policy. The Policy Number is indicated at the end of each policy statement summary. Refer to the Comprehensive Master Plan for complete policy language.

CHAPTER 3: A BALANCED LAND USE PATTERN



The Carson City Master Plan seeks to establish a balance of land uses within the community by providing employment opportunities, a diverse choice of housing, recreational opportunities, and retail services.

Is or does the proposed development:

- ☐ Meet the provisions of the Growth Management Ordinance (1.1d, Municipal Code 18.12)?
- ☒ Use sustainable building materials and construction techniques to promote water and energy conservation (1.1e, f)?
- ☐ Located in a priority infill development area (1.2a)?
- ☒ Provide pathway connections and easements consistent with the adopted Unified Pathways Master Plan and maintain access to adjacent public lands (1.4a)?

- ☐ Protect existing site features, as appropriate, including mature trees or other character-defining features (1.4c)?
- ☒ At adjacent county boundaries or adjacent to public lands, coordinated with the applicable agency with regards to compatibility, access and amenities (1.5a, b)?
- ☐ In identified Mixed-Use areas, promote mixed-use development patterns as appropriate for the surrounding context consistent with the land use descriptions of the applicable Mixed-Use designation, and meet the intent of the Mixed-Use Evaluation Criteria (2.1b, 2.2b, 2.3b, Land Use Districts, Appendix C)?
- ☒ Meet adopted standards (e.g. setbacks) for transitions between non-residential and residential zoning districts (2.1d)?
- ☐ Protect environmentally sensitive areas through proper setbacks, dedication, or other mechanisms (3.1b)?
- ☒ Sited outside the primary floodplain and away from geologic hazard areas or follows the required setbacks or other mitigation measures (3.3d, e)?
- ☒ Provide for levels of services (i.e. water, sewer, road improvements, sidewalks, etc.) consistent with the Land Use designation and adequate for the proposed development (Land Use table descriptions)?
- ☐ If located within an identified Specific Plan Area (SPA), meet the applicable policies of that SPA (Land Use Map, Chapter 8)?

CHAPTER 4: EQUITABLE DISTRIBUTION OF RECREATIONAL OPPORTUNITIES



The Carson City Master Plan seeks to continue providing a diverse range of park and recreational opportunities to include facilities and programming for all ages and varying interests to serve both existing and future neighborhoods.

Is or does the proposed development:

- ☐ Provide park facilities commensurate with the demand created and consistent with the City's adopted standards (4.1b)?
- ☐ Consistent with the Open Space Master Plan and Carson River Master Plan (4.3a)?

CHAPTER 5: ECONOMIC VITALITY



The Carson City Master Plan seeks to maintain its strong diversified economic base by promoting principles which focus on retaining and enhancing the strong employment base, include a broader range of retail services in targeted areas, and include the roles of technology, tourism, recreational amenities, and other economic strengths vital to a successful community.

Is or does the proposed development:

- ☒ Encourage a citywide housing mix consistent with the labor force and non-labor force populations (5.1j)
- ☐ Encourage the development of regional retail centers (5.2a)
- ☐ Encourage reuse or redevelopment of underused retail spaces (5.2b)?
- ☐ Support heritage tourism activities, particularly those associated with historic resources, cultural institutions and the State Capitol (5.4a)?
- ☐ Promote revitalization of the Downtown core (5.6a)?
- ☐ Incorporate additional housing in and around Downtown, including lofts, condominiums, duplexes, live-work units (5.6c)?

CHAPTER 6: LIVABLE NEIGHBORHOODS AND ACTIVITY CENTERS



The Carson City Master Plan seeks to promote safe, attractive and diverse neighborhoods, compact mixed-use activity centers, and a vibrant, pedestrian-friendly Downtown.

Is or does the proposed development:

- ☒ Use durable, long-lasting building materials (6.1a)?
- ☐ Promote variety and visual interest through the incorporation of varied building styles and colors, garage orientation and other features (6.1b)?
- ☒ Provide variety and visual interest through the incorporation of well-articulated building facades, clearly identified entrances and pedestrian connections, landscaping and other features consistent with the Development Standards (6.1c)?
- ☒ Provide appropriate height, density and setback transitions and connectivity to surrounding development to ensure compatibility with surrounding development for infill projects or adjacent to existing rural neighborhoods (6.2a, 9.3b 9.4a)?
- ☐ If located in an identified Mixed-Use Activity Center area, contain the appropriate mix, size and density of land uses consistent with the Mixed-Use district policies (7.1a, b)?
- ☐ If located Downtown:
 - ☐ Integrate an appropriate mix and density of uses (8.1a, e)?
 - ☐ Include buildings at the appropriate scale for the applicable Downtown Character Area (8.1b)?
 - ☐ Incorporate appropriate public spaces, plazas and other amenities (8.1d)?
- ☐ Incorporate a mix of housing models and densities appropriate for the project location and size (9.1a)?

CHAPTER 7: A CONNECTED CITY



The Carson City Master Plan seeks promote a sense of community by linking its many neighborhoods, employment areas, activity centers, parks, recreational

amenities and schools with an extensive system of interconnected roadways, multi-use pathways, bicycle facilities, and sidewalks.

Is or does the proposed development:

- ☒ Promote transit-supportive development patterns (e.g. mixed-use, pedestrian-oriented, higher density) along major travel corridors to facilitate future transit (11.2b)?
- ☐ Maintain and enhance roadway connections and networks consistent with the Transportation Master Plan (11.2c)?
- ☒ Provide appropriate pathways through the development and to surrounding lands, including parks and public lands, consistent with the Unified Pathways Master Plan (12.1a, c)?

Project Information

Location: Airport Road APN: 010-041-76
Site Area: 7.28 ac Developed Area: ±7.0 ac Disturbance: ±7.0 ac
Zoning: PR (±1.43 Ac)/GC (±5.85Ac) Master Plan Designation: Mixed-Use Employment/Open Space
Proposed Use: Covered RV & Boat Storage Facility

Pre-Development Discussion

Existing Conditions & Development:

The subject property is part of the larger Lompa Ranch Development. It is currently undeveloped consisting of moderate vegetation consisting of native grasses and brush. It generally slopes from north to south with the western and southern boundaries of the site lower than the remainder of the property (Reference the attached site plan for location of the onsite flood zones). There is no existing infrastructure extended to the site with the exception of access from Airport Road. Water facilities exist to the north of the property and sanitary sewer services exist to the west. The site is bounded on two sides by large stormwater detention facilities which is where all existing storm flows are discharged.

Surrounding Properties:

- | | |
|---|-------------|
| o North: Undeveloped (Lompa Ranch Residential Site) | Zoning: MFA |
| o South: Open NDOT Detention Pond | Zoning: A/P |
| o East: Municipal Buildings | Zoning: PR |
| o West: Open NDOT Detention Pond | Zoning: A |

Proposed Development Discussion

Proposed Development:

Sierra Tahoe RV, Boat and Self Storage is a facility providing RV and Boat storage spaces and enclosed self-storage buildings to store customers personal belongings. The facility may also store small recreational craft such as jet skis and wave runners. It will offer customers a sales office onsite in order to conduct the rental and business of the facility and offer a small retail sales area within the sales office in order to provide items in connection with RV and boat owner needs such as ice, and motor oil, etc. Also available exclusively to customers will be a sewage dump station and a propane refill /sale station onsite. There will be no onsite mechanic services available and customers will be prohibited from repairing and/or maintaining their recreational vehicles onsite.

Hours of operation are intended from 6:00 am to 10:00 pm with the office open from 8:00 am to 6:00pm daily. Customers will ingress and egress the site during the hours of operation through a security gate controlled by a key pad requiring an individual's code unique to each customer. The site will be monitored through a security camera system and each customer will only have access if they have paid their fees. And for RV and Boat owners, they have provided proof of registration for their RV or Boat and shown proof of insurance with the office and are current with their payments.

Proposed Improvements:

The developed site will include the construction of a ±1,000 s.f. managers office, ±54,400 s.f. (±300 units) of enclosed personal storage, covered storage spaces for ±180 recreational vehicles and a sanitary waste dump station. The manager's office will be constructed with conventional wood framing. The personal storage units will be constructed utilizing premanufactured metal buildings and the canopies will be comprised of steel structural supports and sheet metal roofing. The height of the personal storage buildings will vary as some will be intended for high end RV & Boat Storage. Additionally, a sewer dump station and propane refill tank will be provided. The majority of the site will be paved and fenced with solid view fencing screening as necessary. There is a single point of access to the site. The entrance has been designed to allow for two RVs to be stacked in the inbound lane while a single exit lane has been provided.

Grading of the site has been completed with the goal of exporting material to the adjacent development to the north. Although portions of the development will be within the flood zone, all pavement will be elevated a minimum of 2.0' above the established base flood elevation protecting the improvements from flooding. The site will maintain existing drainage patterns sloping from north to south where flow will be collected and treated in a detention pond before being discharged to the detention facilities to the south. All onsite stormflows are conveyed as surface drainage to ensure viability and lo maintenance.

Water service will be provided by extending a water main from the existing main located near the intersection of Airport Road and Butti Lane. Sanitary sewer service will be provided by connecting to the existing interceptor main with a new lateral connection.

Required Findings for Special Use Permit

1. Will be consistent with the objectives of the Master Plan elements.

Explain how your project will further and be in keeping with, and not contrary to, the goals of the Master Plan elements. Turn to the Master Plan Policy Checklist included with this application. The Master Plan Policy Checklist for Special Use Permits and Major Project Reviews addresses five items that appear in the Carson City Master Plan. Each theme looks at how a proposed development can help achieve the goals of the Carson City Master Plan. Address each theme; a check indicates that the proposed development meets the applicable Master Plan Policy. Provide written support of the policy statement in your own words as a part of these findings. For additional guidance, please refer to the Carson City Master Plan document on our website at www.carson.org/planning or you may contact the Planning Division to review the document in our office or request a copy.

1. A Balanced Land Use Pattern

- **The proposed development will promote a balanced land use pattern by extending public trails & services, incorporating durable and sustainable building materials, providing generous setbacks & allowing for the expansion of drainage detention facilities to the south.**

2. Equitable Distribution of Recreational Opportunities

- **The improvements will include extending a 10' pedestrian path across the frontage of the site extending an existing trail network.**

3. *Economic Vitality*

- **The proposed use will provide for offsite RV storage which will allow for more dense residential communities and expanded RV sales.**

4. *Livable Neighborhoods and Activity Centers*

- **The proposed development will provide a sustainable project with landscape buffers and a screen wall surrounding the storage area.**

5. *A Connected City*

- **The project will extend the 10' pedestrian path from the south across the full project frontage which will eventually lead to a community park.**

2. *Will not be detrimental to the use, peaceful enjoyment, economic value, or development of surrounding properties or the general neighborhood; and is compatible with and preserves the character and integrity of adjacent development and neighborhoods or includes improvements or modifications either on-site or within the public right-of-way to mitigate development related to adverse impacts such as noise, vibrations, fumes, odors, dust, glare or physical activity.*

- a. *Describe the general types of land uses and zoning designations adjoining your property (for example: North: grocery store, Retail Commercial zoning)*

Reference Pre-Development Discussion above.

- b. *Explain why your project is similar to existing development in the neighborhood, and why it will not hurt property values or cause problems, such as noise, dust, odors, vibration, fumes, glare, or physical activity, etc. with neighboring property owners. Have other properties in your area obtained approval of a similar request? How will your project differ in appearance from your neighbors? Your response should consider the proposed physical appearance of your proposal, as well as comparing your use to others in the area.*

The proposed use is consistent with the surrounding development and will not negatively impact property values. To the south and west are open detention ponds and the Carson City Public Works facility is to the East. To the north are a proposed park and residential development. The project will be fully screened and provide landscaping to address visual concerns.

- c. *Provide a statement explaining how your project will not be detrimental to the use, peaceful enjoyment or development of surrounding properties and the general neighborhood.*

The project use will drive minimal traffic increases, generate minimal noise and will have set business hours therefore having no detrimental effect on the surrounding properties.

- d. *If outdoor lighting is to be a part of the project, please indicate how it will be shielded from adjoining property and the type of lighting (wattage/height/placement) provided.*

All required lighting will be provided on the canopies and office building. At final building permit, a photometrics plan will be provided showing compliance with Carson City Development standards.

- e. *Describe the proposed landscaping, including screening and arterial landscape areas (if required by the zoning code). Include a site plan with existing and proposed landscape shown on the plan which complies with City ordinance requirements.*

The proposed landscape improvements will provide evergreen screen buffers on the north and south borders, as requested by the Parks's Dept. review comments. There is a double row of street trees proposed along Airport Road and the project will have a Plant List loaded with pollinator friendly trees, shrubs, groundcovers, vines and perennials.

- f. *Explain any short-range and long-range benefit to the people of Carson City that will occur if your project is approved.*

With the approval of the project, Carson City will see increased tax revenue as well as improved stormwater detention.

3. *Will have little or no detrimental effect on vehicular or pedestrian traffic.*

Consider the pedestrian and vehicular traffic that currently exists on the road serving your project. What impact will your development have to pedestrian and vehicular traffic when it is successfully operating? Will additional walkways and traffic lights be needed? Will you be causing traffic to substantially increase in the area? State how you have arrived at your conclusions.

The Sierra Tahoe RV & Boat Storage will have a minimal impact on traffic generation and will improve pedestrian traffic conditions. RV & BoatSTRV Storage is a low impact use in regards to traffic generation. In accordance with the requirements of the Major Project Review, a 10' wide pedestrian trail has been extended along the project frontage, therefore improving pedestrian traffic.

4. *Will not overburden existing public services and facilities, including schools, police and fire protection, water, sanitary sewer, public roads, storm drainage and other public improvements.*

- a. *How will your project affect the school district? Will your project add to the student population or will it provide a service to the student population?*

The proposed project will have no impact on the school district with the exception of providing tax revenue.

- b. *How will your project affect police and fire protection?*

The proposed project should have a minimal impact on both police and fire protection. The project will include a comprehensive security system and will provide fire hydrants and access in accordance with fire department requirements.

- c. *Is the water supply serving your project adequate to meet your needs without degrading supply and quality to others in the area? Is there adequate water pressure? Are the lines in need of replacement? Is your project served by a well? Contact the Development Engineering Division at (775) 887-2300 for assistance with this item, if applicable.*

Based on discussions with Carson City Public Works, a water main extension will be required and will be extended from the intersection of Airport Road and Butti Lane. Additionally, based in preliminary discussions, there is ample pressure to serve the development.

- d. *If your project will result in the covering of land area with paving or a compacted surface, how will drainage be accommodated? Contact the Development Engineering Division at (775) 887-2300 for assistance with this item, if applicable.*

The proposed development will result in an increase in stormwater runoff due to proposed impervious surfaces, however, based on correspondence with the Carson City Public Works department, no additional stormwater detention will be required due to the project's proximity to the detention ponds to the south and west of the site. In an effort to work with the City, the site will be graded in a manner to allow for additional stormwater detention to the south.

- e. *Is there adequate capacity in the sewage disposal trunk line that you will connect to in order to serve your project, or is your site on a septic system? Contact the Development Engineering Division at (775) 887-2300 for assistance with this item, if applicable.*

The proposed sewer connection is to the City's main interceptor line which based on discussions with Carson City Public works has more than enough capacity to serve the project. The project will have a minimal impact on the sanitary sewer infrastructure and does not meet the thresholds for a full sanitary sewer report.

- f. *What kind of road improvements are proposed or needed to accommodate your project? Contact the Development Engineering Division at (775) 887-2300 for assistance with this item, if applicable.*

No new roadway improvements are required to serve the project.

- g. *Indicate the source of the information that you are providing to support your conclusions and statements made in this application (private engineer, Development Engineering, Public Works, Transportation, title report or other sources).*

The conclusions above were made based on discussions with Carson City Public Works and Engineering Division as well as the Major Project Review comments.

5. Meets the definition and specific standards set forth elsewhere in Carson City Municipal Code, Title 18 for such particular use and meets the purpose statement of that district.

Explain how your project meets the purpose statement of the zoning district in which it is located and how it meets the specific standards that are set forth in that zoning district. In CCMC Section 18.04, Use Districts, find the zoning district where your property is located. Refer to the purpose statement at the beginning of the zoning district section and explain how your project meets the purpose statement of that district. In addition, find the specific Intensity and Dimensional Standards for your zoning district in either CCMC Section 18.04.190 (Residential) or CCMC Section 18.04.195 (Non-Residential) and explain how your project meets these specific standards. To access the Carson City Municipal Code, visit our website at www.carson.org/planning

The zoning is split between General Commercial (GC) and Public Regional (PR). The proposed use is allowed within the GC zoning district however it requires the SUP in the PR zoning district. This is an office and service use which has minimal impact on the surrounding land uses.

6. Will not be detrimental to the public health, safety, convenience and welfare.

Provide a statement explaining how your project will not be detrimental to the public health, safety, convenience and welfare. If applicable, provide information on any benefits that your project will provide to the general public.

The project will be designed and constructed in accordance with all applicable State and City codes. It is low impact in terms of water, sewer and traffic demand and will allow for increased stormwater detention to in the ponds to the south.

7. Will not result in material damage or prejudice to other property in the vicinity, as a result of proposed mitigation measures.

Provide a statement explaining how your project will not result in material damage or prejudice to other property in the vicinity.

The project will provide no material damage to adjacent properties and will serve as a buffer between the residential uses to the north and public works facilities to the east.

DATE: May 22, 2019

APPLICATION: SUP-19-083

APPLICANT/OWNER: Sierra Tahoe RV & Boat Storage LLC

AGENT: Monte Vista Consulting, LTD

PROJECT APN/ADDRESS: 010-041-76/Butti Way and east side of Airport Road

PLANNING COMMISSION MEETING DATE: June 26, 2019

RE: Include text in the submission that the policies of the Specific Plan and Development Agreement have been reviewed and that this proposal has incorporated these requirements and is in compliance.

The policies of the Lompa Ranch North Specific Plan (SPA) and the Lompa Ranch Development Agreement (Ordinance No. 2017-25) have been reviewed and this proposal incorporates those requirements into the plan. The proposed development is in compliance with these governing documents in every respect.

The Development Agreement states that “the development of the plan area will be in compliance with the architectural and design standards incorporated into the Handbook.” (DA Section 1). The SPA along with the phasing plan are referenced as the Handbook.

Section 2.1 of the SPA (i.e. Handbook) deal with commercial uses. The proposed development complies with the Site Planning Standards discussed in section 2.1.1 by placing the lone building in the development in a way to create visual interest along the public street. The other requirements related to grading (section 2.1.2), parking (section 2.1.3), landscaping (2.1.4), lighting (2.1.5), signs (2.1.6), fencing (2.1.7), and trash (2.1.8) have been reviewed and incorporated into the design and is in compliance. Furthermore, the architecture of the building is designed to complement the intended residential design per section 2.4.3 to 2.4.6.



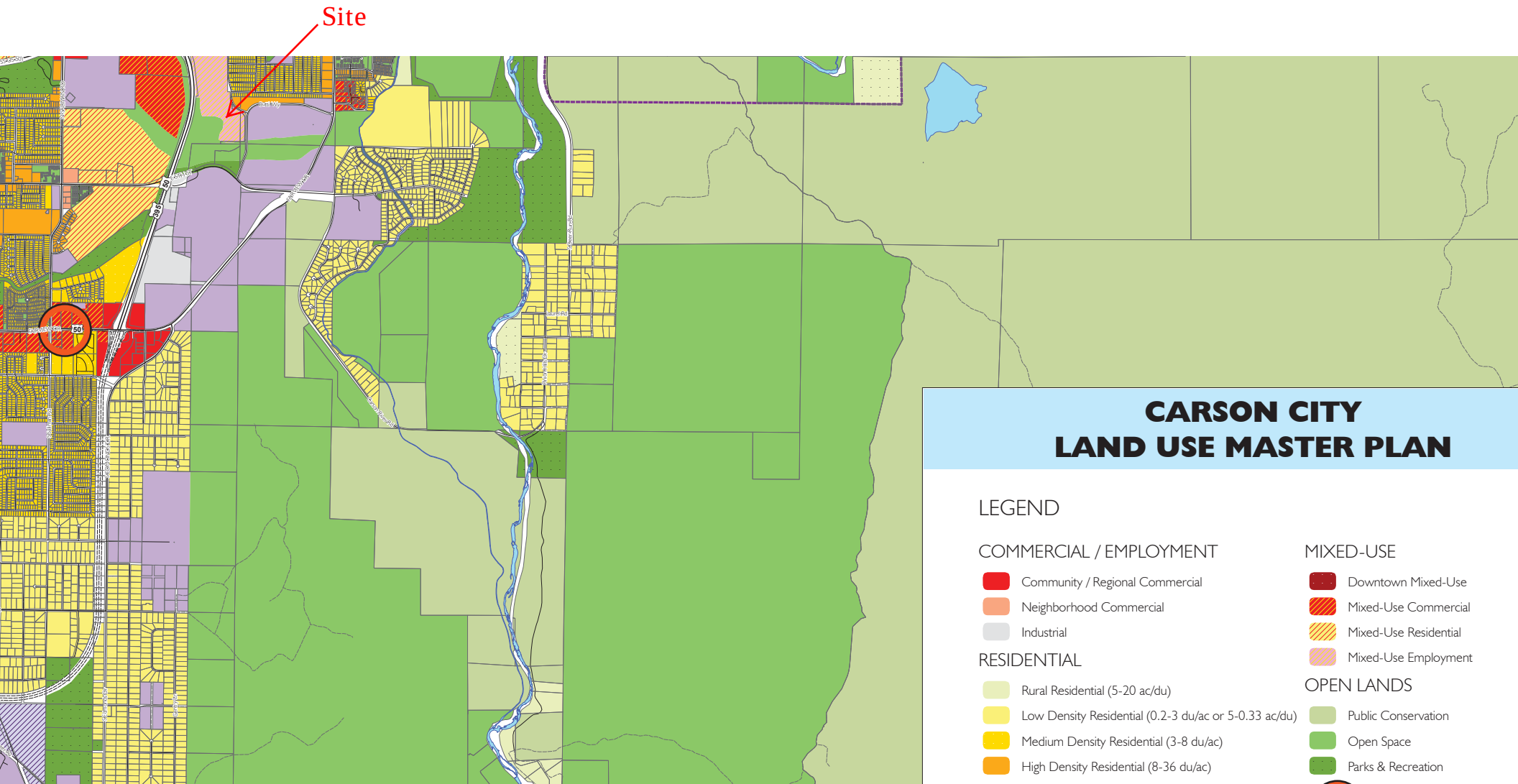
Property Information	
Property ID	01004176
Location	BUTTI WY
Owner	TERRASAS & TRIPP LLC
Acres	7.28



MAP FOR REFERENCE ONLY
NOT A LEGAL DOCUMENT

Carson City , NV makes no claims and no warranties, expressed or implied, concerning the validity or accuracy of the GIS data presented on this map.

Geometry updated 11/17/2018
Data updated 11/17/2018



CARSON CITY LAND USE MASTER PLAN

LEGEND

COMMERCIAL / EMPLOYMENT

- Community / Regional Commercial
- Neighborhood Commercial
- Industrial

RESIDENTIAL

- Rural Residential (5-20 ac/du)
- Low Density Residential (0.2-3 du/ac or 5-0.33 ac/du)
- Medium Density Residential (3-8 du/ac)
- High Density Residential (8-36 du/ac)

MIXED-USE

- Downtown Mixed-Use
- Mixed-Use Commercial
- Mixed-Use Residential
- Mixed-Use Employment

OPEN LANDS

- Public Conservation
- Open Space
- Parks & Recreation



Property Information
Property ID 01004176
Location BUTTI WY
Owner TERRASAS & TRIPP LLC
Acres 7.28

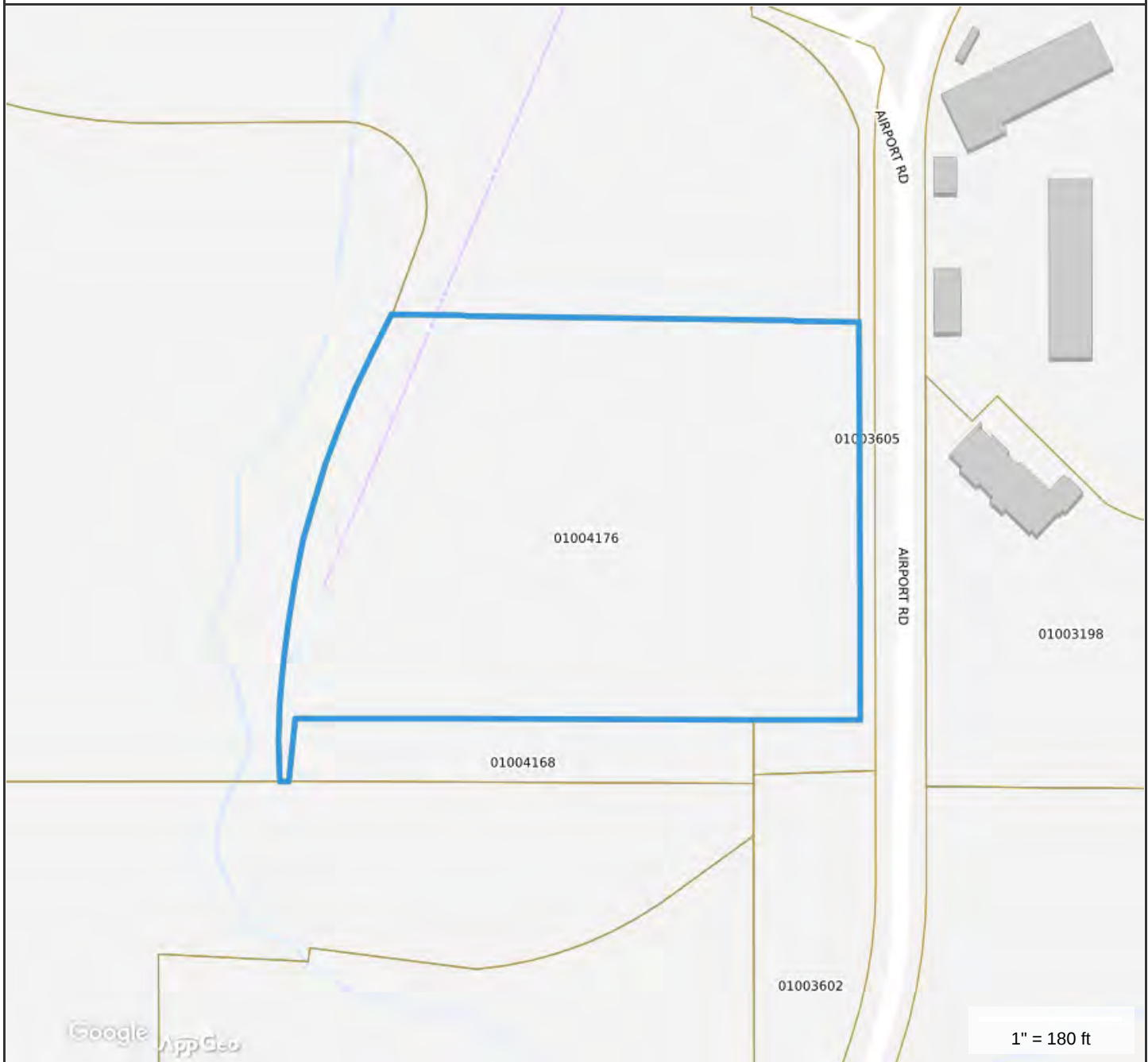


**MAP FOR REFERENCE ONLY
NOT A LEGAL DOCUMENT**

Carson City , NV makes no claims and no warranties, expressed or implied, concerning the validity or accuracy of the GIS data presented on this map.

Geometry updated 11/17/2018
Data updated 11/17/2018

Lompa Fault Line

**Property Information**

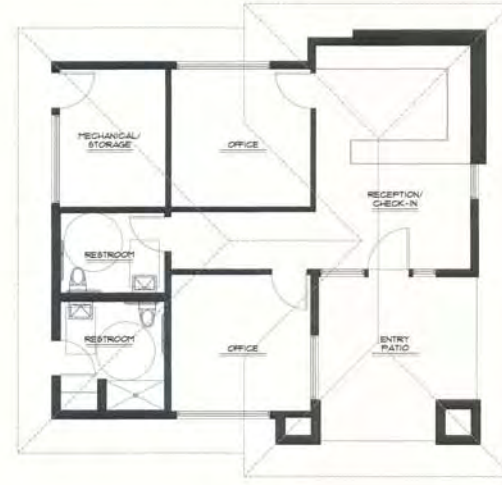
Property ID 01004176
Location BUTTI WY
Owner SIERRA TAHOE RV & BOAT STORAGE
Acres 7.28



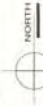
**MAP FOR REFERENCE ONLY
NOT A LEGAL DOCUMENT**

Carson City , NV makes no claims and no warranties, expressed or implied, concerning the validity or accuracy of the GIS data presented on this map.

Geometry updated 11/17/2018
 Data updated 11/17/2018



1 OFFICE FLOOR PLAN



3 WEST ELEVATION



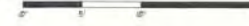
2 SOUTH ELEVATION



5 EAST ELEVATION



4 NORTH ELEVATION



REVISION	DATE



FLOOR PLAN & EXTERIOR BUILDING ELEVATIONS - SUP SUBMITTAL

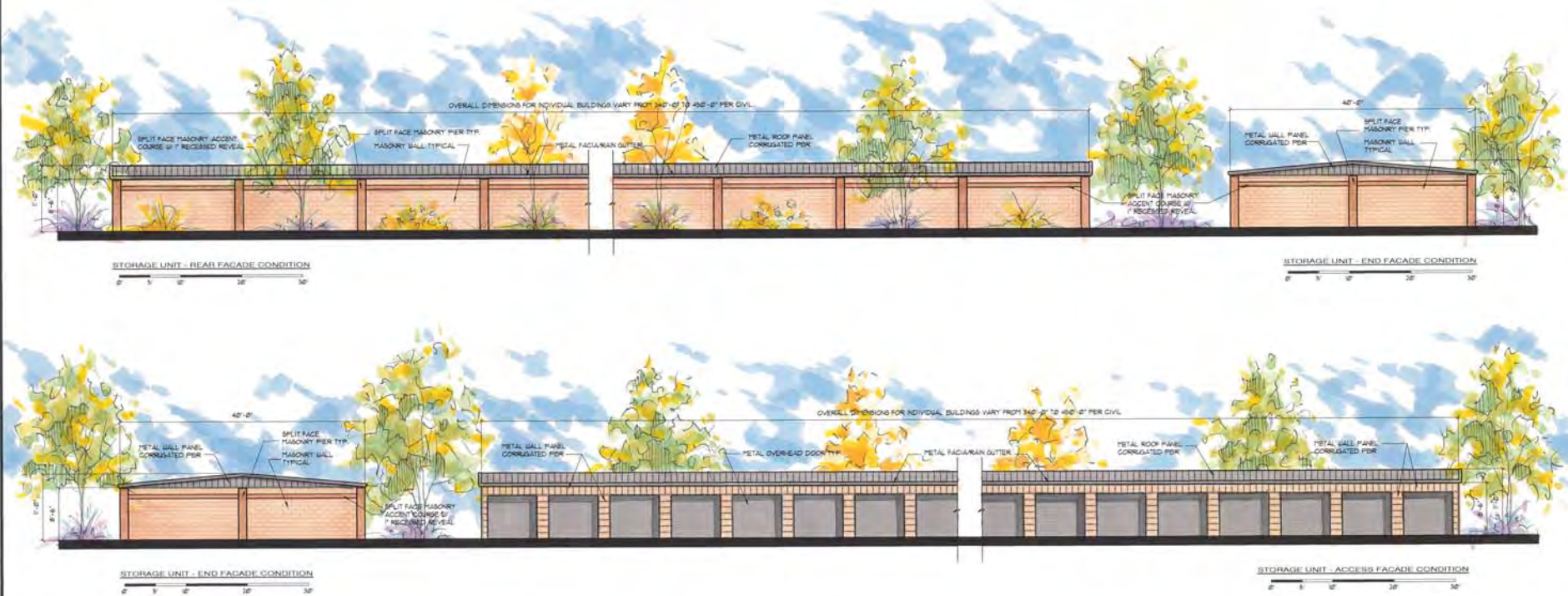
NEW RV & BOAT STORAGE FACILITY FOR:
SIERRA TAHOE RV & BOAT STORAGE
BUTTE WAY | CARSON CITY | NEVADA | APN 010-041-76

DESIGNED BY	ERIC JOHNSON
CHECKED BY	ERIC JOHNSON
DATE	10/25/20
SCALE	AS SHOWN
JOB NUMBER	ST016
SHEET NUMBER	A4.1

NEW RV BOAT & MINI STORAGE FACILITY FOR: SIERRA TAHOE RV BOAT & STORAGE

BUTTI WAY | CARSON CITY | NEVADA | APN 010-041-76

REVISION	DATE



MIN-STORAGE TYPICAL EXTERIOR BUILDING ELEVATIONS - REVISED SUBMITTAL
NEW RV BOAT & MINI STORAGE FACILITY FOR:
SIERRA TAHOE RV BOAT & STORAGE
BUTTI WAY | CARSON CITY | NEVADA | APN 010-041-76

DATE	
BY	
DATE	
BY	
DATE	
BY	
DATE	
BY	

A4.2

PLANT LIST:

LARGE/STREET TREES: (2" Caliper)
IRRIGATE ALL PLANTINGS w/RESOURCE EFFICIENT DRIP IRRIGATION SYSTEM.

BOTANICAL NAME	COMMON NAME
RED MAPLE	ACER RUBRUM
BLACK GUM	NYSSA SYLVATICA
GLADSTON THACANTHOS	SHADEMASTER HONELOCUST
QUERCUS PALUSTRIS	PIN OAK
QUERCUS RUBRA	RED OAK

SMALL/ACCENT TREES:

BOTANICAL NAME	COMMON NAME
ACER GINNALA	AMUR MAPLE
CERCIS CANADENSIS	EASTERN REDBUD
LIQUIDAMBAR STYACIFLUA	SWEETGUM
MALUS PRINERFNE	PRINERFNE CHAMAPPLE
PRUNUS CALLERIANA 'REDSPRE'	REDSPRE PLUM

EVERGREEN TREES: (6' Min. Height)

BOTANICAL NAME	COMMON NAME
JUNIPERUS CHINENSIS 'TORULOSA'	HOLLYWOOD JUMPER
CALOCEDRUS DECURRENS	INCENSE CEDAR
PRUNUS PUNICEA	COLORADO SPRUCE
PRUNUS FLEXILIS 'VANDERHOLP'	VANDERHOLP'S PINE
PRUNUS NERA	AUSTRIAN PINE

SHRUBS/GROUNDCOVERS/VINES: (5 Gal. Min.)

BOTANICAL NAME	COMMON NAME
BUDDLEIA DAVIDA	BUTTERFLY BUSH
CAMPIDUS MAUCANS	RED TULIPET VINE
GENISTA LYDIA	DWARF BROOM
PHOTINIA FRAXES	REDTWIG DOGWOOD
STRONGIA YULGANS	COMMON LILAC

PERENNIALS: (1 Gal.)

BOTANICAL NAME	COMMON NAME
ECONOMICA PURPUREA	PURPLE CORNFLOWER
GALLARDA ARISTATA	INDIAN BLANKET FLOWER
PENTSTEMON STRICTUS	ROCKY MOUNTAIN PENTSTEMON
CERASTHEM TOMENTOSUM	SNOW-IN-JULIUM
PEROVSKIA ATRIPLEXIFOLIA	RUSSIAN SAGE

LANDSCAPE LEGEND:



SHRUB PLANTING AREA w/ROCK MULCH COVER - (36,346 SF)
Shrub along, varieties and quantities to meet code requirements.
Place mulch @ 4" min. depth, over weed barrier fabric

LANDSCAPE CALCULATIONS:

ZONING: GENERAL COMMERCIAL
SITE AREA = 7.28 AC.
IMPERVIOUS AREA = 281,341 S.F.
LANDSCAPE SUMMARY:
LANDSCAPE AREA REQUIRED = 46,288 S.F. (26% of Impervious Area)
LANDSCAPE AREA PROVIDED = 59,525 S.F. (24.6 %)
TREE/SHRUB REQUIREMENTS:
1 TREE/400 S.F. (= 141 TREES)
Includes: 1 TREE/10 PARKING SPACES (7 SPACES = 1 TREE)
and 1 TREE/80 LF STREET FRONTAGE (487 LF. = 17 TREES)
TOTAL TREES REQUIRED = 141 TREES
TREES PROVIDED = 141 TREES
6 SHRUBS per TREE REQUIRED = 846 SHRUBS



AIRPORT ROAD
(PUBLIC)

REVISIONS	BY

TreeHugger Studio, LLC
landscape architecture
and site planning services
140 West Guffey Drive
Washoe Valley, NV 89704
(775) 530-0665



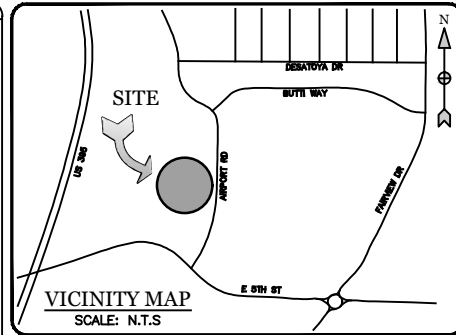
preliminary landscape plan for:
SIERRA TAHOE RV, BOAT and SELF STORAGE
Airport Road, Carson City, NV APN: 00-047-76

PRELIMINARY
LANDSCAPE PLAN

DATE: May 2018, REV Sept 2018
SCALE: 1" = 30'-0"
DRAWN BY: N. SHOCK
CHECKED BY:
PROJECT NUMBER: 228-15-400

L.1

A.C. PAVEMENT AREA
 CONCRETE AREA
 EXISTING PAVING SPACE W. SIGN & PAVEMENT MARKINGS
 PEDESTRIAN ACCESS RAMP
 ACCESSIBLE ROUTE
 PAVING SPACE COUNT
 MANHOLE (W. DESCRIPTION)
 PROPOSED UTILITY LINE W. DESCRIPTION
 EXISTING UTILITY LINE W. DESCRIPTION
 MANHOLE W. DESCRIPTION
 (EXISTING/PROPOSED)
 CLEANSITE
 (EXISTING/PROPOSED)
 CATCH BASIN/DRIP INLET
 DOWN DRAIN
 DIRECTIONAL FLOW LINE
 GRADE BREAK
 PROPOSED CONTOUR LINE
 EXISTING CONTOUR LINE
 SPOT ELEVATION
 (EXISTING) = PROPOSED



SITE ANALYSIS		
AREA	7.28 AC	
ZONING	GENERAL COMMERCIAL(CO)	
SETBACKS (F/R/S)	0/0/0(F)*	
BUILDING FOOTPRINT	±55,255 S.F.	17%
PAVED/IMPERVIOUS	±182,470 S.F.	58%
LANDSCAPED/UNDEVELOPED	±43,453 S.F. MIN.	20%
UNDEVELOPED	±15,868 S.F.	5%

EARTHWORK ANALYSIS	
SITE AREA	7.28 AC
SITE DISTURBANCE	±7.0 AC
PROPOSED CUT	±12,800 YD ³
PROPOSED FILL	±7,300 YD ³
NET EARTHWORK	±5,500 YD ³ CUT

THESE QUANTITIES ARE PRELIMINARY AND ARE BASED ON THE INTENT OF THE DEVELOPER TO WORK WITH THE PROPERTY TO THE NORTH

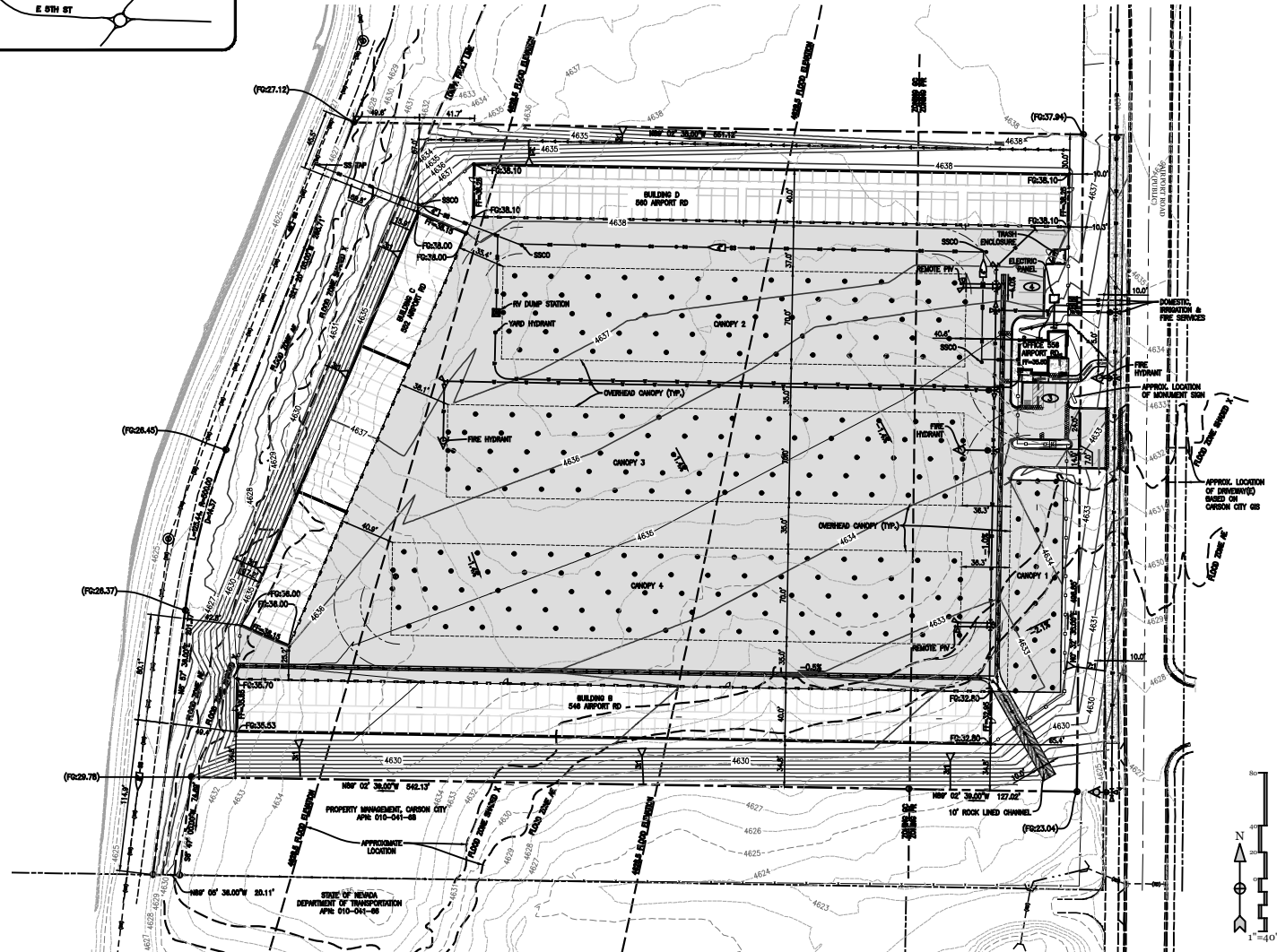
PARKING ANALYSIS			
USE	AREA (FT ²)	CRITERIA	REQUIRED SPACE
OFFICE	805	1/325	2.6
TOTAL REQUIREMENT	SPACES PROVIDED	ADA SPACES	MIN ACCESSIBLE
3 SPACES	7	1	1

NOTES:
 1. ALL BUILDING INFORMATION PROVIDED BY ORIGIN & ARCHITECTURE.
 2. REQUIREMENTS DETERMINED USING CMC 16A2.02.

NOTES:
1. ALL BUILDING INFORMATION PROVIDED BY ORIGIN ARCHITECTURE.
2. REQUIREMENTS DETERMINED USING CMC 18A2.d2.

[illegible]

UTILITY PROVIDERS	
DOMESTIC WATER	CARSON CITY UTILITIES DEPARTMENT
IRRIGATION WATER	CARSON CITY UTILITIES DEPARTMENT
SEWAGE TREATMENT	CARSON CITY UTILITIES DEPARTMENT
STORM DRAIN	CARSON CITY UTILITIES DEPARTMENT
NATURAL GAS	SOUTHWEST GAS CORPORATION
ELECTRICITY	NV ENERGY
TELECOMMUNICATIONS	AT&T / CHARTER COMMUNICATIONS
FIRE	CARSON CITY FIRE DEPARTMENT
POLICE	CARSON CITY SHERIFFS OFFICE



**MONTE VISTA
CONSULTING**

75 E. Plumb Lane, Suite 101
Reno, NV 89502
775.636.7905
montevistaconsulting.com

Sierra Tahoe RV, Boat & Self Storage

556 Airport Road | APN: 010-041-76 | Carson City | Nevada

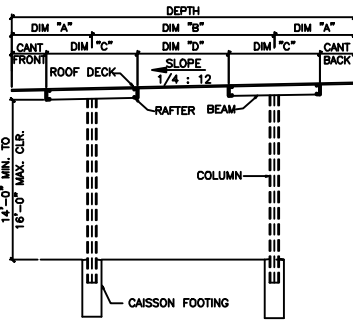
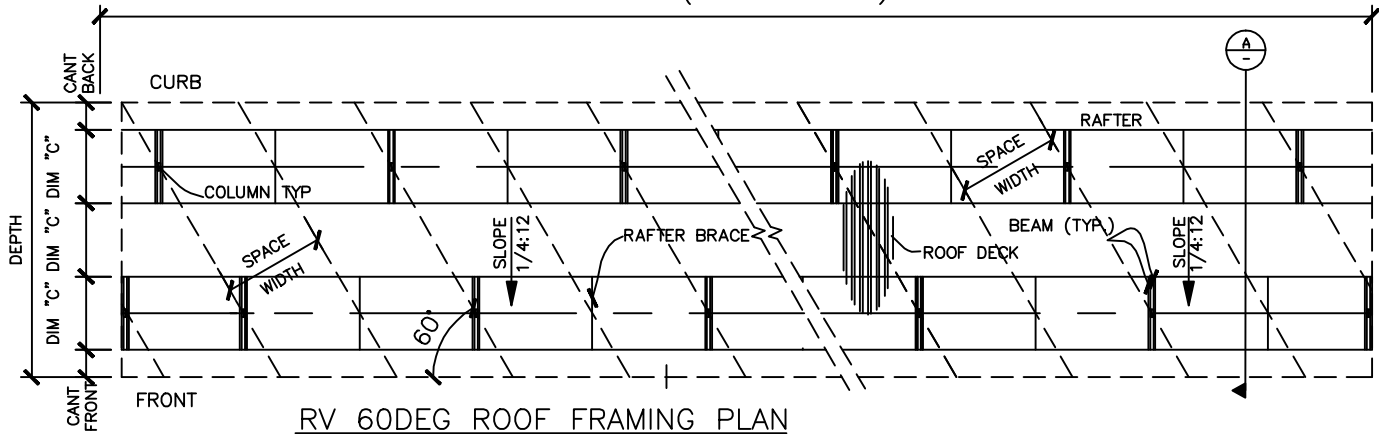


REVISIONS							
Date	12.18.2020						
Drawn	HBA						
Checked	MWV						
Project No.	18.041						

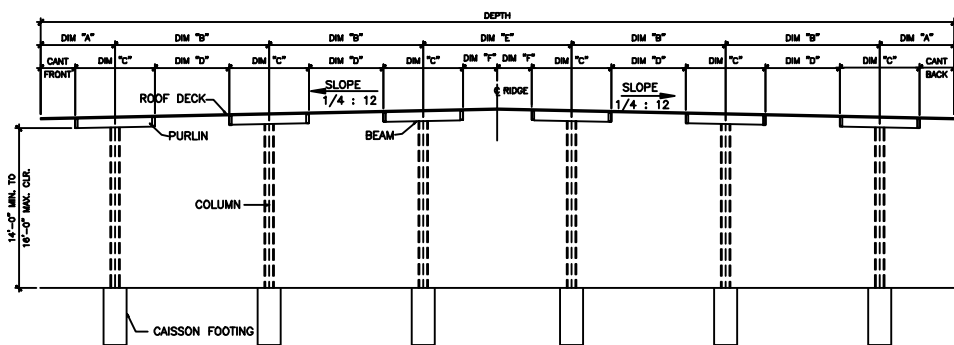
Site Plan

C1.0
of 1

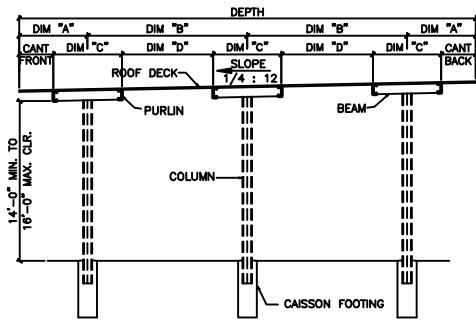
VARIES (SEE SITE PLAN)



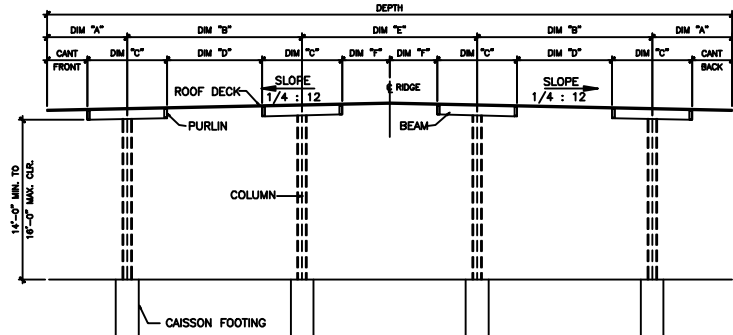
A CROSS SECTION
11x30



A CROSS SECTION
11x80



A CROSS SECTION
11x40



A CROSS SECTION
11x60

SCHEDULE

SIZE	DEPTH	SPACE WIDTH	"A" DIM	"B" DIM	"C" DIM	"D" DIM	"E" DIM	"F" DIM	CANT Front DIM	CANT Back DIM
11'X30'	30'	11'	7'	16'	8'	8'			3'	3'
11'X40'	40'	11'	6'	14'	6'	6'			3'	3'
11'X60'	60'	11'	7'	15'-4"	7'	8'-4"	15'-4"	4'-2"	3'-6"	3'-6"
11'X80'	80'	11'	6'-6"	13'-6"	7'	6'-6"	13'-0"	3'	3'	3'

BAJA STANDARD

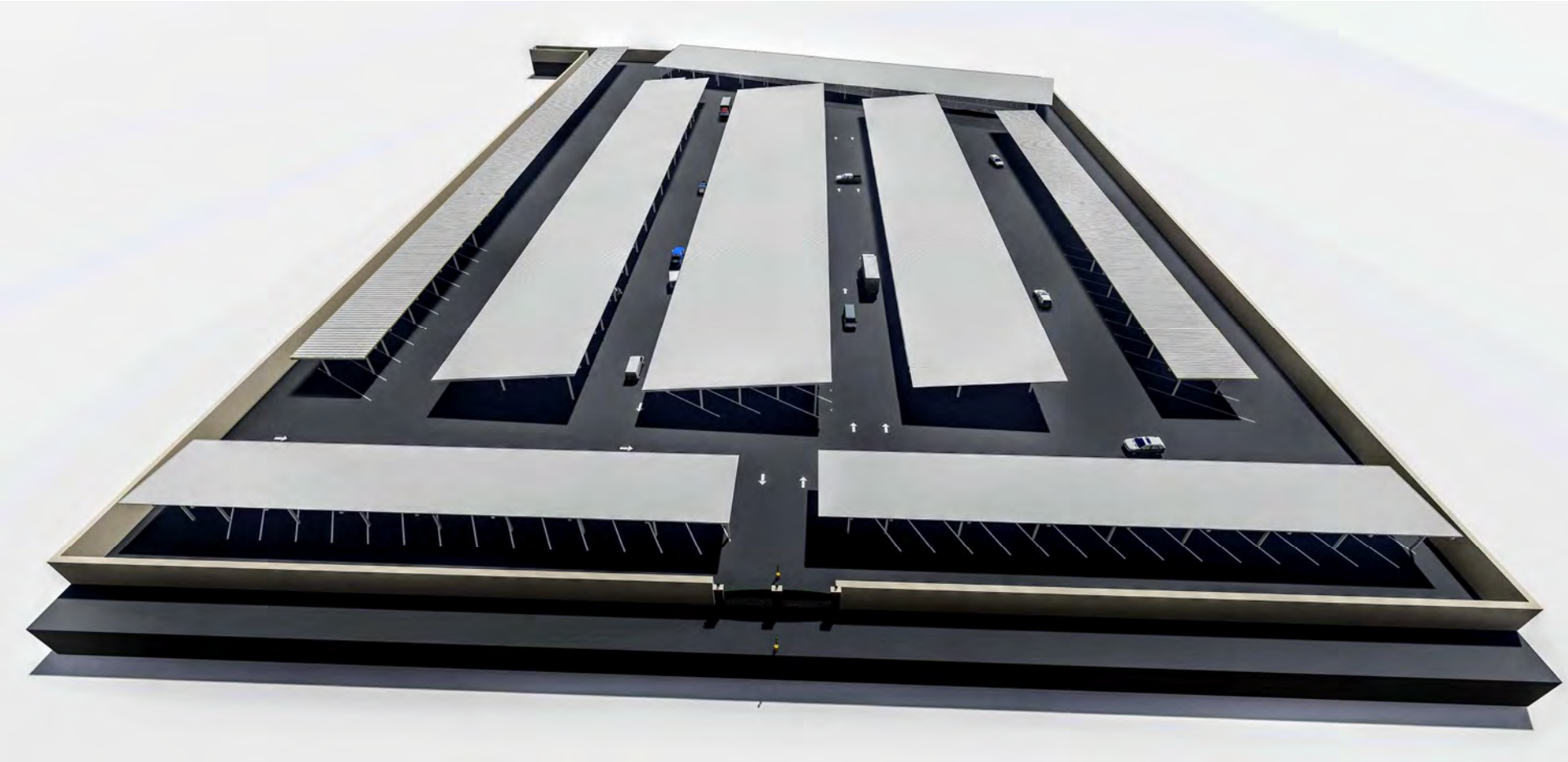
RV 60DEG STANDARD TYPES

BAJA

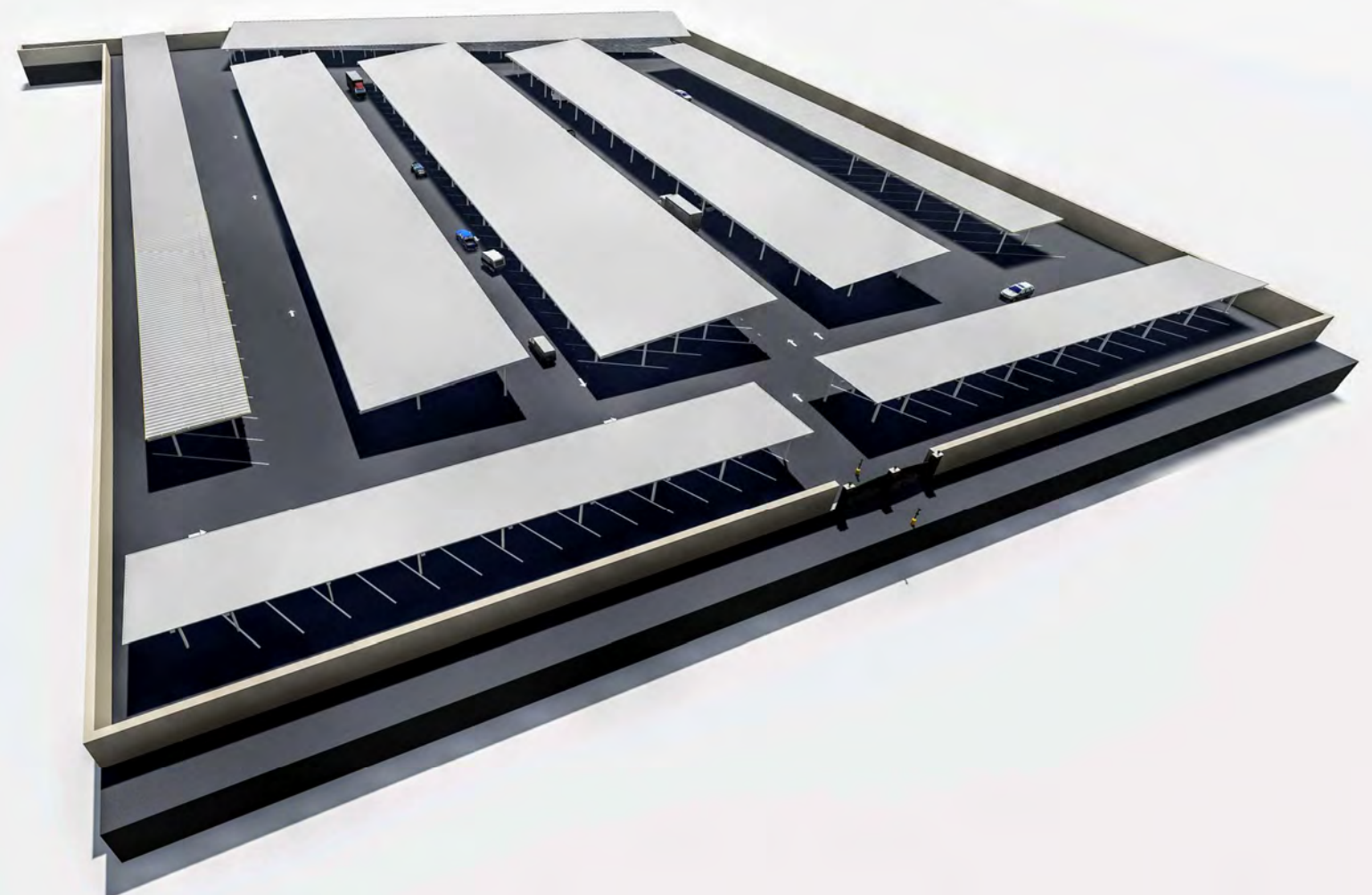
CONSTRUCTION CO., INC.
223 FOSTER ST., MARTINEZ CA 94553
1-800-366-9600 FAX: (925) 229-0161

SHEET

Page 1 of 2



3D RV LAYOUT (60 DEGREES)















ESTABLISHED
1912

NEVADA LICENSE 5493 A
CALIFORNIA LICENSE 199672

800 Glendale Avenue • P.O. Box 855 • Sparks, Nevada 89432 • Fax #: (775)358-7197 • Telephone (775)358-8680

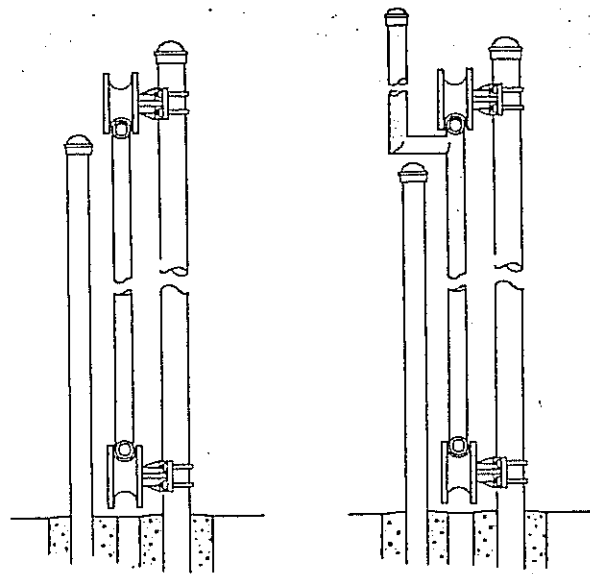
CANTILEVER GATES

Commercial & Industrial

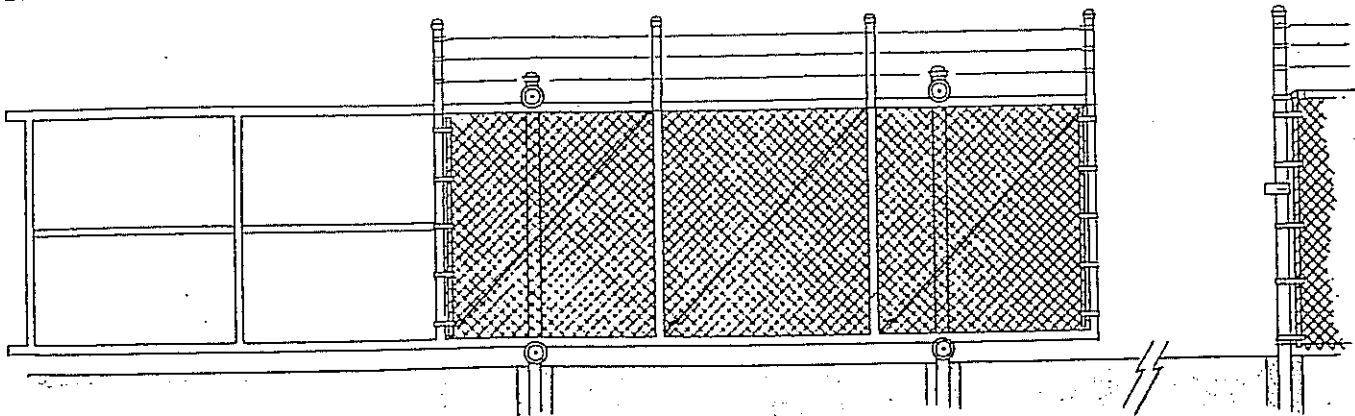
(Made to customer specifications)

CANTILEVER GATE PRICING BASED ON
STYLES SHOWN BELOW AND DOES NOT
INCLUDE HARDWARE.

OPENING	OVERALL HEIGHT
5' - 8'	5' - 8'
8' - 12'	5' - 8'
12' - 15'	5' - 8'
15' - 20'	5' - 8'
20' - 24'	5' - 8'



SINGLE GATES TO 15' OPENING



CONSTRUCTION: 2 3/8" FW/DQ HORIZONTAL RAIL
1 1/8" FW/DQ VERTICAL BRACE
1 3/8" STRUCTURAL/CQ COUNTER BALANCE

TRUSS ROD DIAGONAL BRACE
9 GAUGE FILL
2 3/8" FW/DQ SAFETY POST



VIEWGUARD®

slatted

Viewguard® and Viewguard Plus® are nationally recognized as the premier privacy fencing products on the market today. An attractive, cost effective and extremely durable alternative to wood-slatted fencing, Viewguard® products are perfect for a wide variety of fencing needs. The High Density Polyethylene slats come in a wide range of colors. The colors shown in the brochure are stocked by Security Contractor Services. However, many more colors are available. See your SCS representative for a full line.

All ViewGuard® and ViewGuard Plus® slatted fences include:

- *Comprehensive Manufacturing Process*

Each 2-3/8" slat is inserted vertically and individually secured to the wire with a stainless steel staple. This process ensures the stability and integrity of each individual component. VIEWGUARD® is also available using PVC coated Bonded (fusion adhered) wire to match the plastic slats.

- *Durable Quality Material*

Our standard VIEWGUARD® and VIEWGUARD PLUS® with galvanized wire is woven using 9 ga. weaving wire (.148" diam.) with an average of 1.2 oz. Zinc coating per square foot. The diamond size needed to accommodate the 2-3/8" slats is 3-1/2" x 5-1/2".

- *Weather Resistant*

Made from a high density polyethylene blend, our UV stabilized wide slats (2-3/8") are resistant to chemicals, salt and petroleum products.

- *Multiple Uses*

With the privacy that VIEWGUARD® and VIEWGUARD PLUS® have to offer, in addition to its durability, it is a natural application for commercial, industrial and residential environments.

- *Easy Maintenance*

Because of the VIEWGUARD® and VIEWGUARD PLUS® high density polyethylene finish, its natural, clean look can always be restored by rain or one quick rinse with a garden hose.

- *Made to Order*

8 ga. Vinyl Coated Fused Bonded Wire is available in Black, Green, Brown, Beige and White.

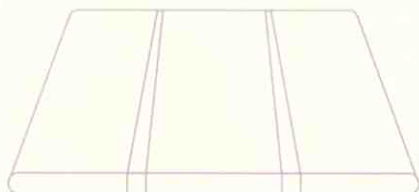
You can order any of our eight standard colors (featured below) or design your own color scheme by using an infinite number of combinations with our various colors.



VIEWGUARD^{plus} Fencing

VIEWGUARD

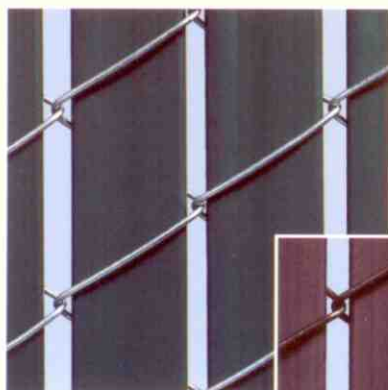
Isometric View



Section View

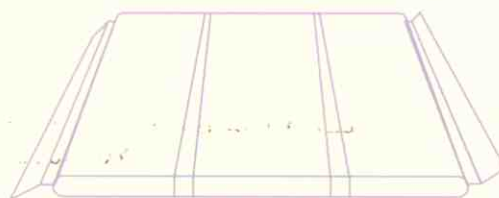


Galvanized Wire

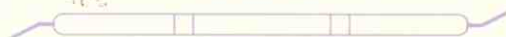


VIEWGUARD^{plus}

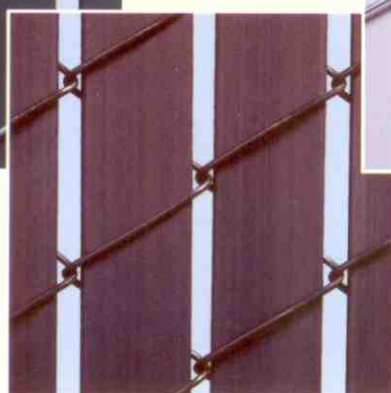
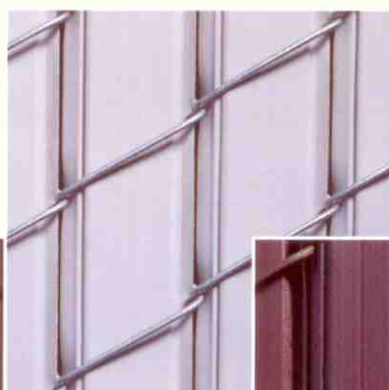
Isometric View



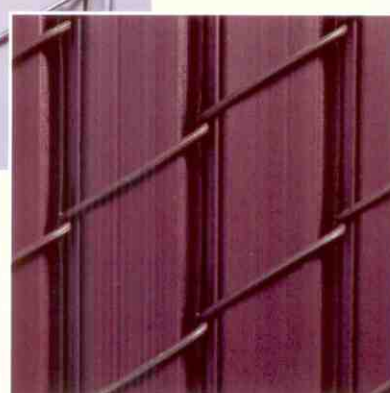
Section View



Galvanized Wire



Bonded Wire



Bonded Wire

Black

White

Beige

Green

ViewGuard Plus[®] slats shown for colors. Standard ViewGuard[®] slats also available in same colors.



CARSON CITY

Capital of Nevada

[Treasurer Home](#)
[Assessor Data Inquiry](#)
[Back to Last Page](#)

Secured Tax Inquiry Detail for Parcel # 010-041-76

Property Location: BUTTI WY
 Billed to: TERRASAS & TRIPP LLC
 250 GREG ST
 SPARKS, NV 89431-0000

Tax Year: 2018-19
 Roll #: 017689
 District: 2.4
 Tax Service:
 Land Use Code: 160

[Code Table](#)

Current Owner: SIERRA TAHOE RV & BOAT STORAGE
 892 SOUTHWOOD BLVD
 INCLINE VILLAGE, NV 89451-0000

Outstanding Taxes:

Prior Year	Tax	Penalty/Interest	Total	Amount Paid	Total Due
------------	-----	------------------	-------	-------------	-----------

Current Year

08/20/18	3,005.29		3,005.29	3,005.29	.00
10/01/18	3,003.00		3,003.00	3,003.00	.00
01/07/19	3,003.00		3,003.00	3,003.00	.00
03/04/19	3,003.00		3,003.00	3,003.00	.00

No Taxes Owing

Totals:	12,014.29	.00	12,014.29	12,014.29	
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[Payment Cart](#)
[History](#)

Additional Information

	2018-19	2017-18	2016-17	2015-16	2014-15
Tax Rate	3.5700	3.5700	3.5200	3.5200	3.5400
Tax Cap Percent	4.2	2.6	.2	3.2	3.0

Reports



Carson City
Community Development Department
108 E. Proctor Street
Carson City, NV 89701

December 18, 2020

RE: Sierra Tahoe RV & Boat Storage – Traffic Letter

The Sierra Tahoe RV & Boat Storage is located on Airport Road (APN: 010-041-76). The site is currently undeveloped and is part of the Lompa Ranch Development. The developed site will include the construction of a $\pm 1,000$ s.f. managers office, $\pm 54,400$ s.f. (± 300 units) of enclosed personal storage, covered storage spaces for ± 180 recreational vehicles and a sanitary waste dump station. The ITE Trip Generation Manual does not have a parking rate for RV & Boat Storage so the Mini-Warehouse (151) rate has been utilized to analyze the full site which is believed to be a conservative estimate. Based on the proposed unit count the ITE Trip Generation Manual (9th Edition) the project will generate 120 daily trips with a peak of 10 trips in both the AM and PM periods falling below Carson City's threshold for a full traffic study. No additional traffic study has been performed.

Please contact Monte Vista Consulting if you have any questions or if there is anything else I can help with.

Sincerely,
Monte Vista Consulting

Michael Vicks, P.E.
Principal



December 18, 2020



Carson City
Community Development Department
108 E. Proctor Street
Carson City, NV 89701

December 18, 2020

RE: Sierra Tahoe RV & Boat Storage – Parking Justification Letter

The Sierra Tahoe RV & Boat Storage is located on Airport Road (APN: 010-041-76). The site is currently undeveloped and is part of the Lompa Ranch Development. The developed site will include the construction of a $\pm 1,000$ s.f. managers office, $\pm 54,400$ s.f. (± 300 units) of enclosed personal storage, covered storage spaces for ± 180 recreational vehicles and a sanitary waste dump station. The ITE Parking Generation Manual does not have a parking rate for RV & Boat Storage so the Mini-Warehouse (151) rate has been utilized to analyze the full site which is believed to be a conservative estimate. Based on the proposed unit count the ITE Parking Generation Manual (4th Edition) the project will require 3 spaces. The proposed project will include 7 parking spaces and will also have oversized drive isles which would allow for additional temporary parking in the drive isles if required. No additional parking study has been performed. Based on similar operations, the proposed site layout provides more than adequate parking.

Please contact Monte Vista Consulting if you have any questions or if there is anything else I can help with.

Sincerely,
Monte Vista Consulting

Michael Vicks, P.E.
Principal



December 18, 2020



Carson City
Community Development Department
108 E. Proctor Street
Carson City, NV 89701

December 18, 2020

RE: Sierra Tahoe RV & Boat Storage – Sanitary Sewer Letter

The Sierra Tahoe RV & Boat Storage is located on Airport Road (APN: 010-041-76). The site is currently undeveloped and is part of the Lompa Ranch Development. The developed site will include the construction of a $\pm 1,000$ s.f. managers office, $\pm 54,400$ s.f. (± 300 units) of enclosed personal storage, covered storage spaces for ± 180 recreational vehicles and a sanitary waste dump station which will be restricted use for tenant use only. No sanitary sewer infrastructure is extended to the site, however, a main interceptor does run along the western boundary of the property. In order to provide sanitary sewer service, a new lateral will be connected to the interceptor main west of the site and extended to the onsite office and RV dump station. The threshold for a full sanitary sewer report is 200 drainage fixture units daily (5,000 gallons). The office will have two bathrooms which will equate to ± 10 fixture units. Based on discussions with Carson City Development Engineering, the average usage over a summer month would be utilized in determining the approximate peak contribution. The owners have reached out to several other facilities with a similar business model and estimated that they would anticipate 5.0 sewer dumps per day and a similar usage for the wash bay. Considering the average RV has a capacity of 100 gallons or less of waste water, it is estimated that the project is likely to contribute less than 500 gallons (25 fixture units) of waste water to the sanitary sewer system daily during summer months. No additional analysis of the existing offsite sanitary sewer system has been completed.

Please contact Monte Vista Consulting if you have any questions or if there is anything else we can help with.

Sincerely,
Monte Vista Consulting

Michael Vicks, P.E.
Principal



December 18, 2020



Carson City
Community Development Department
108 E. Proctor Street
Carson City, NV 89701

December 18, 2020

RE: Sierra Tahoe RV & Boat Storage – Drainage Letter

The Sierra Tahoe RV & Boat Storage is located on Airport Road (APN: 010-041-76). The site is currently undeveloped and is part of the Lompa Ranch Development. The developed site will include the construction of a $\pm 1,000$ s.f. managers office, $\pm 54,400$ s.f. (± 300 units) of enclosed personal storage, covered storage spaces for ± 180 recreational vehicles and a sanitary waste dump station. The site is located adjacent to community detention ponds which are directly to the south and west of the site. Portions of the development are located in FEMA flood Zone AE. Those portions of the project will be elevated in a manner to bring them out of the flood plain and will be officially removed from the flood plain with a LOMA prior to Certificate of Occupancy.

Based on the findings of the Major Project Review and subsequent meetings with Carson City Public Works, it has been determined no additional onsite detention will be required for the development and it can drain directly into the ponds to the south. The development will increase impervious coverage by approximately 237,800 s.f. (5.46 acres). Utilizing the Rational Method the increased impervious area results in anticipated additional site contributions to the adjacent detention facilities of:

5-Year Storm Event = 7.65 cfs
100-Year Storm Event = 14.78 cfs

This flow will be treated through a rock lined swale prior to entering the ponds, however little infiltration is anticipated. In order expand the available storage in the adjacent ponds, the grading of the site has been modified to allow for deeper cuts on the adjacent property which is owned by Carson City.

Please contact Monte Vista Consulting if you have any questions or if there is anything else we can help with.

Sincerely,
Monte Vista Consulting

Michael Vicks, P.E.
Principal



December 18, 2020

March 13, 2017

Feasibility Geotechnical Investigation

LOMPA RANCH BOAT AND RV STORAGE PROJECT Carson City, Nevada



Prepared For:

Terasas and Tripp
250 Greg Street
Reno, Nevada 89431

Prepared By:

Resource Concepts, Inc.
340 N. Minnesota St.
Carson City, Nevada 89703





March 13, 2017

Frank Terasas
Terasas and Tripp
250 Greg Street
Reno, NV 89431

**Subject: Feasibility Geotechnical Investigation for the Lompa Ranch Boat and RV Storage Project,
Butti Way, Carson City, Nevada APN 010-041-76**

Dear Mr. Terasas:

In accordance with your authorization, we have prepared our "Feasibility Site Geotechnical Investigation" report for the subject site titled "Lompa Ranch and RV Storage Project", located on Butti Way, Carson City, Nevada, Nevada APN 010-041-76. The purpose of our investigation was to provide information regarding the constructability of the proposed project on the site from geotechnical and geo-hazards perspectives.

The accompanying report presents our findings and conclusions from our investigation. The project site appears to be generally well suited, except as noted below, to the proposed commercial use as described herein. Liquefaction potential has been indicated on previous geologic mapping reviewed for this investigation and should be further assessed by a site specific geotechnical investigation. Liquefaction potential is common throughout Carson City and is routinely addressed during investigations. This report is not intended to take the place of a site specific geotechnical investigation.

Please contact us if you have any questions concerning the contents of this report. We look forward to reviewing the project plans as they develop further and providing additional geotechnical engineering consultation as needed.

Thank you for the opportunity to be of service.

Resource Concepts, Inc.



Gary C. Luce, PE
Sr. Geotechnical Engineer/Geologist

3/13/2017


David Edgington, EI
Geotechnical Engineering Intern

Enclosures: Figures 1 Through 5
 Appendix A- Field Investigation and Laboratory Tests Results
 Sample Grading Specification

FEASIBILITY GEOTECHNICAL INVESTIGATION LOMPA RANCH BOAT AND RV STORAGE PROJECT

APN 010-041-76

**Butti Way
Carson City, Nevada**

1.0 PURPOSE AND SCOPE

This report presents the results of Resource Concepts Inc. (RCI) Feasibility Site Geotechnical Investigation for the proposed Lompa Ranch and RV Storage Project. The subject parcels are identified by the Carson City County Assessor as APN 010-041-76. The proposed development area totals approximately 7.25 acres located on Butti Way in Carson County, Nevada. It is our understanding that the parcel will have vehicle access from Butti Way as shown on the Vicinity Map, Figure 1. The topography of the site can be generally characterized as flat lying with gentle to moderate slopes to the south and west. The property has an average elevation of approximately 4630 feet. The preliminary layout of the proposed development is presented on Figure 2, the project Site Plan.

For the preparation of this report, we reviewed the following documents:

- *BAJA Construction Company Inc., Preliminary Site Plan, RV Enclosed and Canopy January 17, 2017*
- *Black Eagle Consultants Inc., Geotechnical Investigation, (July 2005), Carson City Freeway, Phase 2A.*
- *Geocon Consultants Inc., (October 2010), Robinson Transmission Main Geotechnical Investigation.*
- *Katzer, T. (1980), Carson City Quadrangle, General Groundwater Map, Nevada Bureau of Mines and Geology, Scale 1:24,000.*
- *Katzer, T. and Schroer, C.V (1981), Carson City Quadrangle, Flood and Related Debris Flow Hazards Map, Nevada Bureau of Mines and Geology, Scale 1:24,000.*
- *Natural Resources Conservation Service Website, Soil Survey of Carson City Area, Nevada, (<http://websoilsurvey.sc.egov.usda.gov/App/HomePage.htm>).*

- *Resource Concepts Inc., Geotechnical Investigation for the Carson City Animal Shelter, Butti Way, Carson City, Nevada, January 20, 2015*
- *Trexler, D.T. (1977), Carson City Folio Geologic Map, Nevada Bureau of Mines and Geology, Carson City 7.5' Minute Quadrangle, Nevada, Scale 1:24,000.*
- *Trexler, D.T. and Bell, J.W., (1979), Carson City Quadrangle, Earthquake Hazards Map, Nevada Bureau of Mines and Geology, Scale 1:24,000.*

RCI's scope of work consisted of a limited field investigation and summarizing published information regarding soil conditions, geologic hazards, and site geology. The published NRCS soils information included in our site review only extends to five feet below the surface. Our limited field investigation included a limited number of exploration test pits accross the site to provide additional confidence in the reliability of the published soil information and to extend the soils information depth to approximately 10 feet for consideration of footing bearing and settlement concerns and utility installation criteria.

Resource Concepts, Inc. (RCI) performed the following scope of geotechnical services:

- Observed the excavation of five exploratory test pits (TP-1 through TP-5) at the site. The test pits were excavated using a Cat Backhoe equipped with a 24-inch bucket to depths of approximately ten feet below the existing ground surface (bgs). The approximate locations of the exploratory test pits are depicted on the Site Plan, Figure 2.
- Logged the test pits in general accordance with the Unified Soil Classification System (USCS).
- Obtained bulk samples from the test pits. Logs of the exploratory test pits and other details of the field investigation are included in Appendix A.
- Submitted selected soil samples for geotechnical laboratory testing.

2.0 REGIONAL GEOLOGY

The geology of the site is referenced from Carson City Folio Geologic Map (Trexler, 1977). Carson City lies within a large fault bounded valley referred to as Eagle Valley. The valley area is typical of the western edge of the Great Basin geomorphic province. The geologic map indicates the project site area is predominantly underlain by Quaternary alluvial plain deposits. These deposits are described by Trexler (1977) as yellowish brown to gray, unstratified to poorly bedded, poorly to moderately sorted fine silty sand, sandy silt, granular clayey coarse sand, and minor sandy gravel. The alluvial plain deposits are on the order of 2,000 feet deep in the Eagle Valley basin based on geophysical data.

Intrusive and extrusive igneous rocks form the mountains that surround the valleys in the Carson City region. These mountains provide the source for sediments that form the alluvial deposits underlying the valley and that form the modern alluvial fans that ring the valleys. The geology of the site and surrounding areas is depicted on the Geology Map, Figure 3.

3.0 SOIL CONDITIONS

The soil conditions observed in the five exploratory test pits excavated at the site are relatively consistent with the Natural Resources Conservation Service (NRCS) mapping and related data. The NRCS data is limited to the top five feet of soil. The soil units mapped by NRCS on the site are shown on the Soil Map, Figure 4. The soils observed in our test pits to a depth of five feet bgs consist of alluvial deposits of loose to medium dense, moist to wet, interbedded poorly graded sand (SP), Silty Sand (SM) and clayey sand (SC). Soils observed and sampled in the five test pits from approximately five to eleven feet below ground surface (bgs) consisted of loose to medium dense, moist to wet, poorly to well graded, Silty Sand with gravel (SM, SM-SW and SP-SM). Test results on representative samples are attached in Appendix A.

4.0 GROUNDWATER CONDITIONS

At the time of our investigation, March, 2017, Carson City has experienced a record amount precipitation. Precipitation records indicate over 25 inches since October 1, 2016 which is almost double the yearly average. Our field measurements are likely to reflect the approximate high groundwater level on the site. During the spring melting period these levels may be anticipated to continue to rise by a foot or more.

Groundwater depth in two of our five test pits (TP-01 and TP-02) ranged from 8 to 10 feet. The groundwater in this interval is contained in medium to coarse grained sand aquifers. Review of the General Groundwater Map of the Carson City Quadrangle indicates permanent groundwater levels of 20 to 30 feet below existing grade for the May to June 1976 period. Variations in rainfall, snowmelt, temperature, and other factors can cause fluctuations in the level of groundwater. Groundwater flow in the project site area is generally to the southeast toward the Carson River.

5.0 GEOLOGIC HAZARDS

Faulting

The project site as all of Carson City is located near active faults which are considered capable of producing significant ground motions due to seismic events. Based on the Nevada Bureau of Mines and Geology and the USGS Quaternary Fault Fold Database, there are no known active faults (Holocene-age, exhibiting displacement within the last 11,000 years) crossing the subject site. Therefore the risk of fault ground rupture at the site is considered low.

A single fault is mapped across the west site of the site. The mapped fault is considered older than would be considered active for the proposed development. Unoccupied projects such as proposed are recommend uses in areas subject to potential ground ruptures and therefore we do not recommend a site specific investigation. The Fault Map, Figure 5 is attached showing the distribution of faults in the vicinity of the site and the Carson City area.

Seismic Hazard Analysis

The Nevada Bureau of Mines has evaluated faulting along the Carson Range and Geology to be capable of producing earthquake Richter Magnitudes of approximately 7.3 with peak ground

accelerations of approximately 2.0g. These values are equivalent to Modified Mercalli Intensities of X (ten) or greater. The seismic risk at the site is not significantly greater than that of the surrounding developments and the Carson City area in general. We recommend that seismic design of the structures be in accordance with the latest version of the International Building Code (IBC) and the American Society of Civil Engineers (ASCE) Standard 7-10. For preliminary design and cost estimations an IBC Site Class of "D" or stiff soil should be employed. Final design should be based on a site specific geotechnical investigation.

Liquefaction

No specific investigation for the possibility of liquefaction at the site was performed. Liquefaction of granular soils can be caused by strong vibratory motion due to earthquakes. Soils that are highly susceptible to liquefaction are loose, granular and saturated. Liquefaction of soils may cause surface distress, loss of bearing capacity, and settlement of structures.

Mapping by the Nevada Bureau of Mines and Geology shows the site to be in an area of moderate severity of shaking where groundwater is generally greater than ten feet below the surface. Areas such as this may be susceptible to liquefaction due to strong shaking caused by large seismic events.

We know of no residential or commercial buildings in this portion of Carson City to have deep foundations due to liquefaction concerns. To our knowledge the buildings surrounding the site are built on conventional shallow foundations.

Flooding and Debris Flow

Review of the FIRM map 320001011G issued on December 22, 2016 indicates that the site is bound by areas within the 0.2 percent annual chance of flooding. The site itself is listed on the FIRM map as an area of minimal flooding. The scope of our work does not include any evaluation or validity of the map. The owner will need to further evaluate the site conditions and determine if the project will need to have additional design or drainage requirements for the project.

5.0 CONCLUSIONS AND RECOMMENDATIONS

General

Based on the results of our preliminary investigation, site soils are generally granular, cohesionless varieties and are well suited for the support of the type of commercial structures we would anticipate being constructed on the site. A thin (< 1 foot) clay layer was identified on the upper portion of the site. This soils should be avoided for direct support of foundations or pavements. Due to the general location and thickness of the clay layer, it will likely be largely removed during clearing and grubbing as well as mass grading of the site. We anticipate that to level the site approximately one to three feet of cut is likely. Deposition of the clay materials in the lower portions of fills or outside of building areas is an effective means of mitigation.

The only potential geologic hazard identified on the site is liquefaction which should be further addressed by a site specific geotechnical investigation. Liquefaction hazards are common in the

Carson City area on low lying sites. Determination of the liquefaction potential and the need for any mitigation can be determined through drilling and subsequent routine geotechnical analyses.

Site Grading

Site grading should be able to be accomplished with moderate size equipment suitable for handling medium dense to dense sandy soils.

Earthwork operations should be observed and compacted fill tested for density. All fill placed within the structural areas should meet the structural fill requirements of the most recent edition of the *Standard Specification for Public Works*.

Attached to this report is a sample grading specification which is intended for planning and cost estimating purposes only. Specific grading recommendations should be developed as part of a site specific geotechnical investigation.

Preliminary Foundation Design Criteria

Conventional foundations are likely to consist of continuous perimeter spread footings and or column footings. Frost depth in Carson City is 24 inches below adjacent exterior grade. For preliminary cost estimates the IBC presumptive bearing capacity of 1,500 psf with lateral bearing of 150 psf/ft below natural grade and a coefficient of friction of 0.25 are recommended. A site specific investigation in our opinion may result in some increase to these values.

Slabs on Grade and Pavements

Site soils, except clay soils as noted above, are well suited to the support of slab on grade and flexible pavement construction. It should be noted that placement conditions of concrete are often adverse due to the dry climate, propensity for winds and hot and cold seasons. Mitigations measures have been developed by local contractors but caution should be used especially if using contractors from outside of the area. Due to freeze thaw extremes at the elevation of Carson City, exterior concrete should be air entrained, as is required in the Standard Specifications for Public Works Construction, with from 4.5% to 7.0% entrained air content. The water cement ratio for all exterior concrete should be 0.45 or less. A mid-range plasticizer is commonly used to facilitate the finishing process while maintaining the desired water cement ratio. Exterior concrete should be placed and finished in accordance with American Concrete Institute (ACI) recommendations for concrete placed in areas subject to freeze-thaw environments.

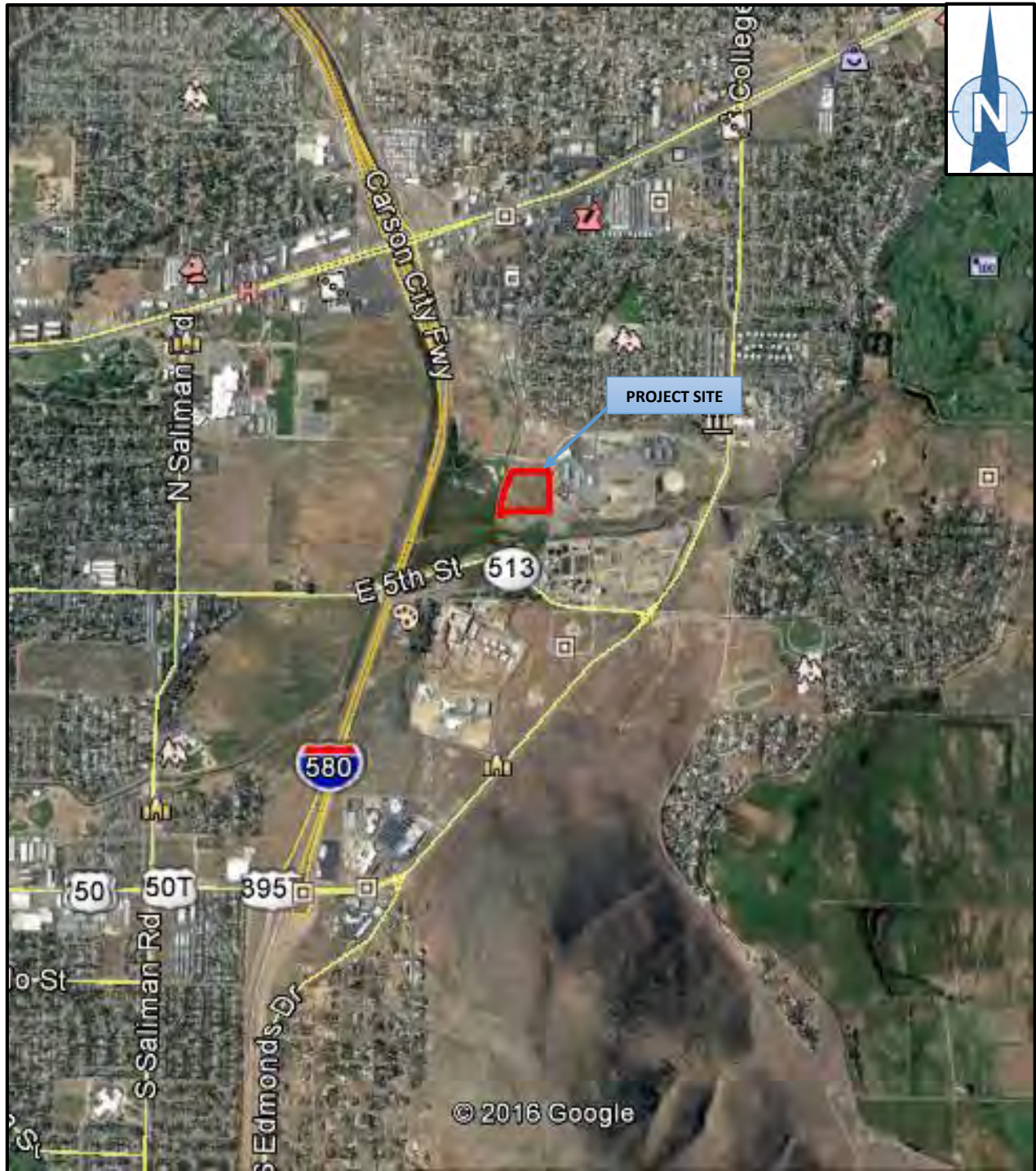
6.0 LIMITATIONS AND UNIFORMITY OF CONDITIONS

The recommendations of this report pertain only to the limits of the site as identified on the Site Plan and are based upon the assumption that the soil conditions do not significantly deviate from those disclosed in the investigation. This report is not intended to take the place of a design specific geotechnical investigation for the project area. The evaluation or identification of the potential presence of hazardous or corrosive materials was not part of the scope of services provided by RCI.

The findings of this report are valid as of the present date. However, changes in the conditions of a property can occur with the passage of time, whether they be due to natural processes or the works

of man on this or adjacent properties. In addition, changes in applicable or appropriate standards may occur, whether they result from legislation or the broadening of knowledge. Accordingly, the findings of this report may be invalidated wholly or partially by changes outside our control. Therefore, this report is subject to review and should not be relied upon after a period of three years or after the acceptance of a newer version of the building code by Carson City.

Our professional services were performed, our findings obtained, and our recommendations prepared in accordance with generally accepted geotechnical engineering principles and practices used in Carson City, Nevada in March of 2017. This warranty is in lieu of all other warranties, either expressed or implied.



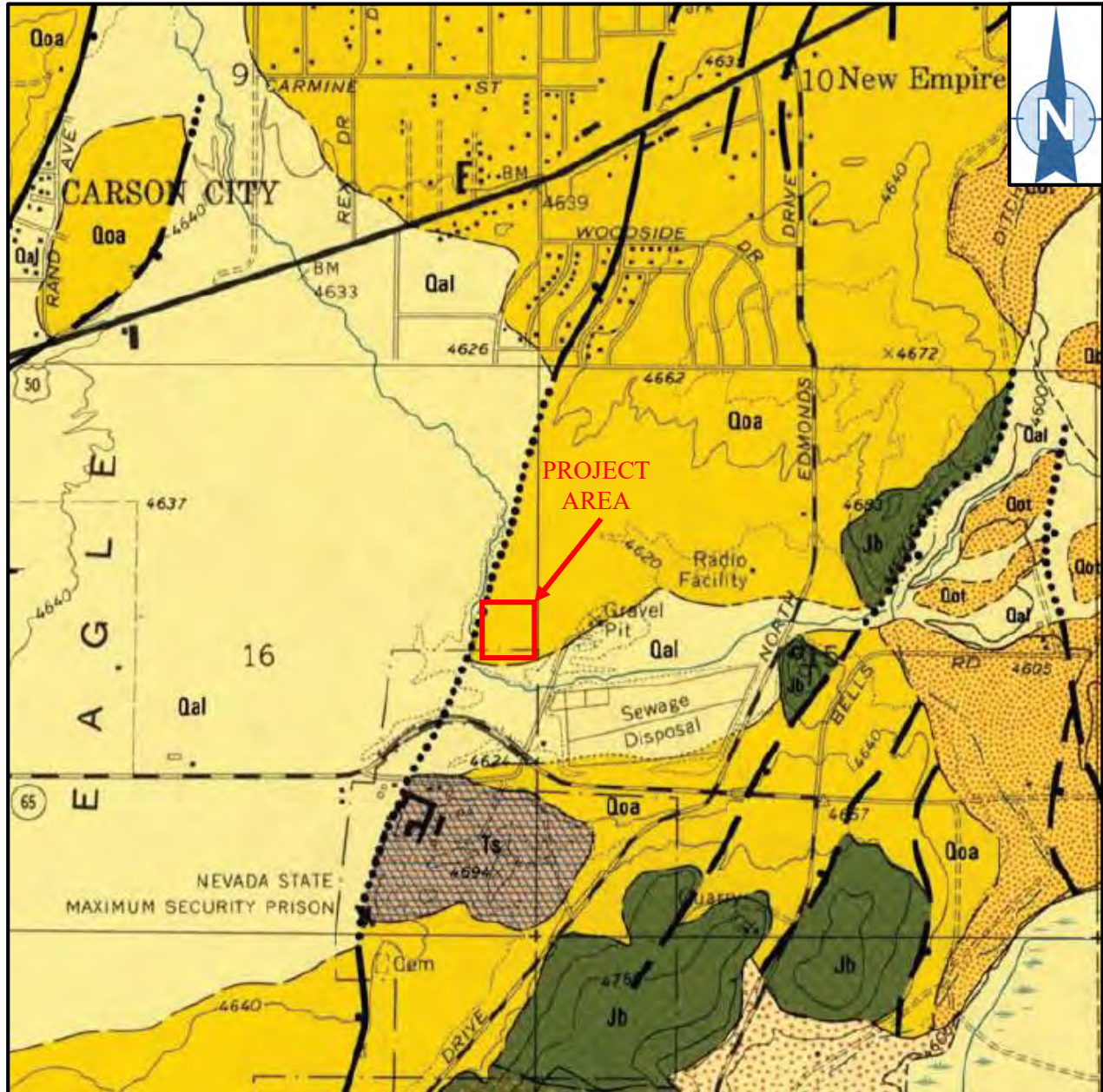
340 N. Minnesota St.
Carson City, NV 89703
775 883-1600

DE

FIGURE 1 VICINITY MAP

LOMPA RANCH BOAT AND RV STORAGE PROJECT CARSON CITY, NV

PROJECT NO. 16-302.1



Not to Scale

E. C. Bingler, 1977

Qoa- Older Alluvial-Plain Deposits
 Qal- Alluvial Plain Deposits
 Ts- Sedimentary Rocks
 Jb- Metavolcanic Breccia

—— FAULT
 - - - - - INFERRED FAULT LOCATION

Map Reference: Nevada Bureau of Mines and Geology, 1977
 U.S. Geological Survey, New Empire 7 1/2' quadrangle



340 N. Minnesota St.
 Carson City, NV 89703
 775 883-1600

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FIGURE 3 GEOLOGIC MAP
LOMPA RANCH
BOAT AND RV STORAGE PROJECT
CARSON CITY, NV

PROJECT NO. 16-302.1



Not to Scale

■ TEST PIT LOCATIONS OBSERVED BY RESOURCE CONCEPTS, INC. ON MARCH 7, 2017



340 N. Minnesota St.
Carson City, NV 89703
775 883-1600

DE

FIGURE 2 SITE PLAN

**LOMPA RANCH
BOAT AND RV STORAGE PROJECT
CARSON CITY, NV**

PROJECT NO. 16-302.1



Map Unit Symbol	Map Unit Name
50	Orizaba Loam, Saline-Alkali
38	Kimmerling Silty Clay Loam
22	Greenbrae Fine Sandy Loam

Map Reference: Natural Resources Conservation Service Website, Soil Survey of Carson City Area



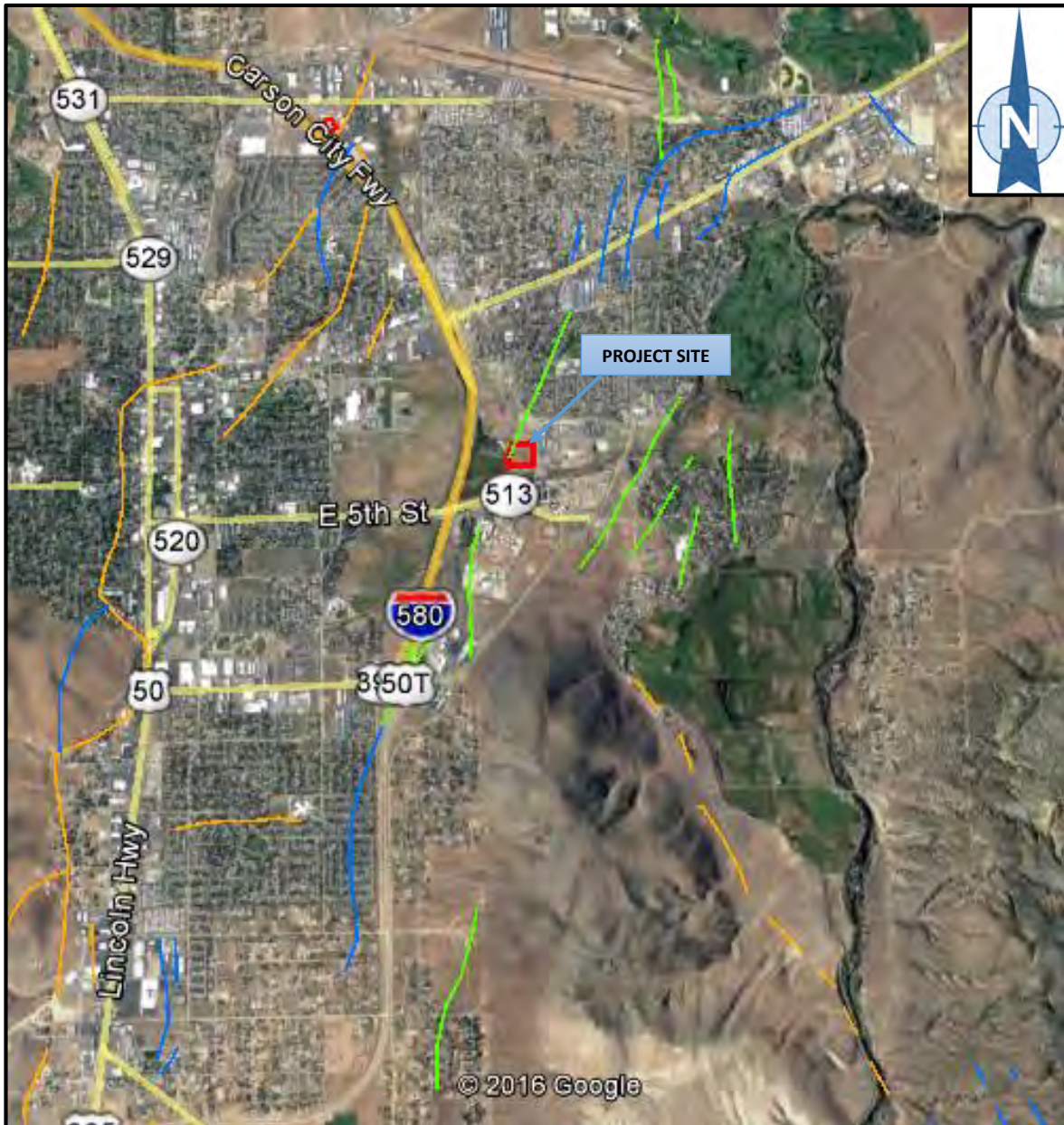
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775 883-1600

DE

FIGURE 4 SOILS MAP

**LOMPA RANCH
BOAT AND RV STORAGE PROJECT
CARSON CITY, NV**

PROJECT NO. 16-302.1



- Quaternary Fault (Inactive)
- Late Quaternary Fault (Inactive)
- Active Holocene Fault



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FIGURE 5 FAULT MAP

LOMPA RANCH BOAT AND RV STORAGE PROJECT CARSON CITY, NV

PROJECT NO. 16-302.1

APPENDIX A

Field Investigation and Laboratory Tests Results



PAGE 1 OF 1

PROJECT NAME Lompa Ranch Mini Storage Project

PROJECT LOCATION Carson City Nevada

COMPLETED 3/7/17

GROUND ELEVATION 4625 ft

TEST PIT SIZE inches

EXCAVATION CONTRACTOR Armac

GROUND WATER LEVELS:

EXCAVATION METHOD Backhoe

▽ AT TIME OF EXCAVATION 8.00 ft / Elev 4617.00 ft

LOGGED BY GL

CHECKED BY GL

AT END OF EXCAVATION ---

NOTES

AFTER EXCAVATION ---

DEPTH (ft)	SAMPLE TYPE NUMBER	TESTS	GRAPHIC LOG	MATERIAL DESCRIPTION
0.0				
				SILTY SAND (SM)- Dark Yellowish Brown, Moist, Loose
2.5		MC = 9% Fines = 22%	2.3	4622.7
		MC = 9% Fines = 13%		SILTY SAND (SM)- Dark Yellowish Brown, Moist, Medium Dense
			4.3	4620.7
5.0				SILTY SAND (SM) Weakly Cemented Caliche Layer- Dark Yellowish Brown, Slightly Moist, Medium Dense, Mottling Apparent
			5.2	4619.8
				POORLY GRADED SAND with Gravel (SP)- Brown, Moist to Saturated, Loose
7.5			▽	
			9.5	4615.5

Bottom of test pit at 9.5 feet.

GENERAL BH / TP / WELL - GINT STD US LAB GDT - 3/10/17 09:07 - N12 DAVE'S WORKING FILES\GEOTECH PROJECTS\16-302.1 LOMPA MINI-STORAGE TERRASS-TRIPPLAMPA RANCH MINI STORAGE PROJECT.GPJ



Resource Concepts, Inc.
4010 Technology Way
Carson City, Nevada 89703
775-883-1600

TEST PIT NUMBER TP-02

PAGE 1 OF 1

CLIENT	Terrasas and Tripp	PROJECT NAME	Lompa Ranch Mini Storage Project
PROJECT NUMBER	16-302.1	PROJECT LOCATION	Carson City Nevada
DATE STARTED	3/7/17	COMPLETED	3/7/17
EXCAVATION CONTRACTOR	Armac	GROUND ELEVATION	4636 ft
EXCAVATION METHOD	Backhoe	TEST PIT SIZE	inches
LOGGED BY	GL	CHECKED BY	GL
NOTES			
		GROUND WATER LEVELS:	
		▽ AT TIME OF EXCAVATION	9.80 ft / Elev 4626.20 ft
		AT END OF EXCAVATION	---
		AFTER EXCAVATION	---

GENERAL BH / TP / WELL - GINT STD US LAB.GDT - 3/10/17 09:07 - N1/2 DAVE'S WORKING FILES\GEOTECH PROJECTS\16-302.1 LOMPA MINI-STORAGE TERRASS-TRIPPI\LOMPA RANCH MINI STORAGE PROJECT.GPJ

DEPTH (ft)	SAMPLE TYPE NUMBER	TESTS	GRAPHIC LOG	MATERIAL DESCRIPTION
0.0				
				SILTY SAND with Fine Sand (SM)- Dark Brown, Moist, Loose
				1.0 4635.0
				SILTY SAND (SM)- Yellowish Brown, Moist, Medium Dense
				2.4 4633.6
2.5				SANDY SILT (ML)- Gray, Moist, Medium Dense
				4.0 4632.0
				SILTY SAND with Fine to Coarse Sand (SM)- Grayish Brown, Moist, Loose to Medium Dense
5.0				
				7.4 4628.6
7.5				SILTY SAND (SM)- Grayish Brown, Moist, Medium Dense
				9.0 4627.0
		MC = 14% Fines = 6%		POORLY GRADED SAND with Fine Sand (SP)- Yellowish Brown, Moist to Saturated, Medium Dense, Strong Mottling Apparent
10.0			▽	
				10.4 4625.6

Bottom of test pit at 10.4 feet.



Resource Concepts, Inc.
4010 Technology Way
Carson City, Nevada 89703
775-883-1600

TEST PIT NUMBER TP-03

PAGE 1 OF 1

CLIENT	Terrasas and Tripp	PROJECT NAME	Lompa Ranch Mini Storage Project
PROJECT NUMBER	16-302.1	PROJECT LOCATION	Carson City Nevada
DATE STARTED	3/7/17	COMPLETED	3/7/17
EXCAVATION CONTRACTOR	Armac	GROUND ELEVATION	4635 ft
EXCAVATION METHOD	Backhoe	TEST PIT SIZE	inches
LOGGED BY	GL	CHECKED BY	GL
NOTES			
GROUND WATER LEVELS:		AT TIME OF EXCAVATION ---	
		AT END OF EXCAVATION ---	
		AFTER EXCAVATION ---	

GENERAL BH / TP / WELL - GINT STD US LAB.GDT - 3/10/17 09:07 - N:\2 DAVE'S WORKING FILES\GEOTECH PROJECTS\16-302.1 LOMPA MINI-STORAGE TERRASS-TRIPPI\LOMPA MINI-STORAGE PROJECT.GPJ

DEPTH (ft)	SAMPLE TYPE NUMBER	TESTS	GRAPHIC LOG	MATERIAL DESCRIPTION	
0.0					
		MC = 18% LL = 35 PL = 21 Fines = 45%		0.4 SILTY SAND with Rootlets (SM)- Dark Brown, Moist, Loose	4634.6
				1.4 CLAYEY SAND (SC)- Dark Yellowish Brown, Wet, Stiff	4633.6
2.5		MC = 4% Fines = 1%		2.5 CLAYEY SAND (SC)- Dark Yellowish Brown, Moist, Medium Dense	4632.5
				3.5 SILTY SAND (SM)- Dark Yellowish Brown, Moist, Medium Dense	4631.5
5.0				POORLY GRADED SAND (SP)- Brown, Moist, Loose, Mottling Apparent	
7.5				6.7 POORLY GRADED SAND (SP)- Brown, Moist, Loose, Strong Mottling Apparent	4628.3
10.0				11.0	4624.0

Bottom of test pit at 11.0 feet.



Resource Concepts, Inc.
4010 Technology Way
Carson City, Nevada 89703
775-883-1600

TEST PIT NUMBER TP-04

PAGE 1 OF 1

CLIENT	Terrasas and Tripp	PROJECT NAME	Lompa Ranch Mini Storage Project
PROJECT NUMBER	16-302.1	PROJECT LOCATION	Carson City Nevada
DATE STARTED	3/7/17	COMPLETED	3/7/17
EXCAVATION CONTRACTOR	Armac	GROUND ELEVATION	4635 ft
EXCAVATION METHOD	Backhoe	TEST PIT SIZE	inches
LOGGED BY	GL	CHECKED BY	GL
NOTES			
		GROUND WATER LEVELS:	
		AT TIME OF EXCAVATION	---
		AT END OF EXCAVATION	---
		AFTER EXCAVATION	---

GENERAL BH / TP / WELL - GINT STD US LAB.GDT - 3/10/17 09:07 - N:1/2 DAVE'S WORKING FILES\GEOTECH PROJECTS\16-302.1 LOMPA MINI-STORAGE TERRASS-TRIPLI LAMPA RANCH MINI STORAGE PROJECT.GPJ

DEPTH (ft)	SAMPLE TYPE NUMBER	GRAPHIC LOG	MATERIAL DESCRIPTION	
0.0				
			0.3 SILTY SAND with Rootlets (SM)- Dark Brown, Moist, Loose	4634.7
			0.8 CLAYEY SAND (SC)- Dark Yellowish Brown, Moist, Loose	4634.2
			SILTY SAND (SM)- Gray, Moist, Medium Dense	
2.5				
			3.1 SILTY SAND with Fine Sand (SM)- Gray, Moist, Medium Dense	4631.9
			4.0 POORLY GRADED SAND with Gravel (SP)- Grayish Brown, Moist, Loose to Medium Dense	4631.0
5.0				
7.5				
10.0				
			10.6	4624.4

Bottom of test pit at 10.6 feet.



PAGE 1 OF 1

PROJECT NAME Lompa Ranch Mini Storage Project

PROJECT LOCATION Carson City Nevada

COMPLETED 3/7/17

GROUND ELEVATION 4635 ft

TEST PIT SIZE inches

EXCAVATION CONTRACTOR Armac

GROUND WATER LEVELS:

EXCAVATION METHOD Backhoe

AT TIME OF EXCAVATION ---

LOGGED BY GL

CHECKED BY GL

AT END OF EXCAVATION ---

NOTES

AFTER EXCAVATION ---

DEPTH (ft)	SAMPLE TYPE NUMBER	TESTS	GRAPHIC LOG	MATERIAL DESCRIPTION
0.0				
		MC = 16% Fines = 38%		SILTY SAND (SM)- Dark Brown, Wet, Loose
			1.6	4633.4
			2.0	4633.0
2.5				
				CLAYEY SAND (SC)- Dark Yellowish Brown, Moist, Medium Dense
			3.8	4631.2
				SILTY SAND with Gravel (SM)- Reddish Brown, Moist, Medium Dense, Mottling Apparent
5.0				
7.5				
10.0				
			10.5	4624.5

Bottom of test pit at 10.5 feet.

GENERAL BH / TP / WELL - GINT STD US LAB.GDT - 3/10/17 09:07 - N:\2 DAVE'S WORKING FILES\GEOTECH PROJECTS\16-302.1 LOMPA MINI-STORAGE TERRASS-TRIP\LAMPA RANCH MINI STORAGE PROJECT.GPJ



Resource Concepts, Inc.
4010 Technology Way
Carson City, Nevada 89703
775-883-1600

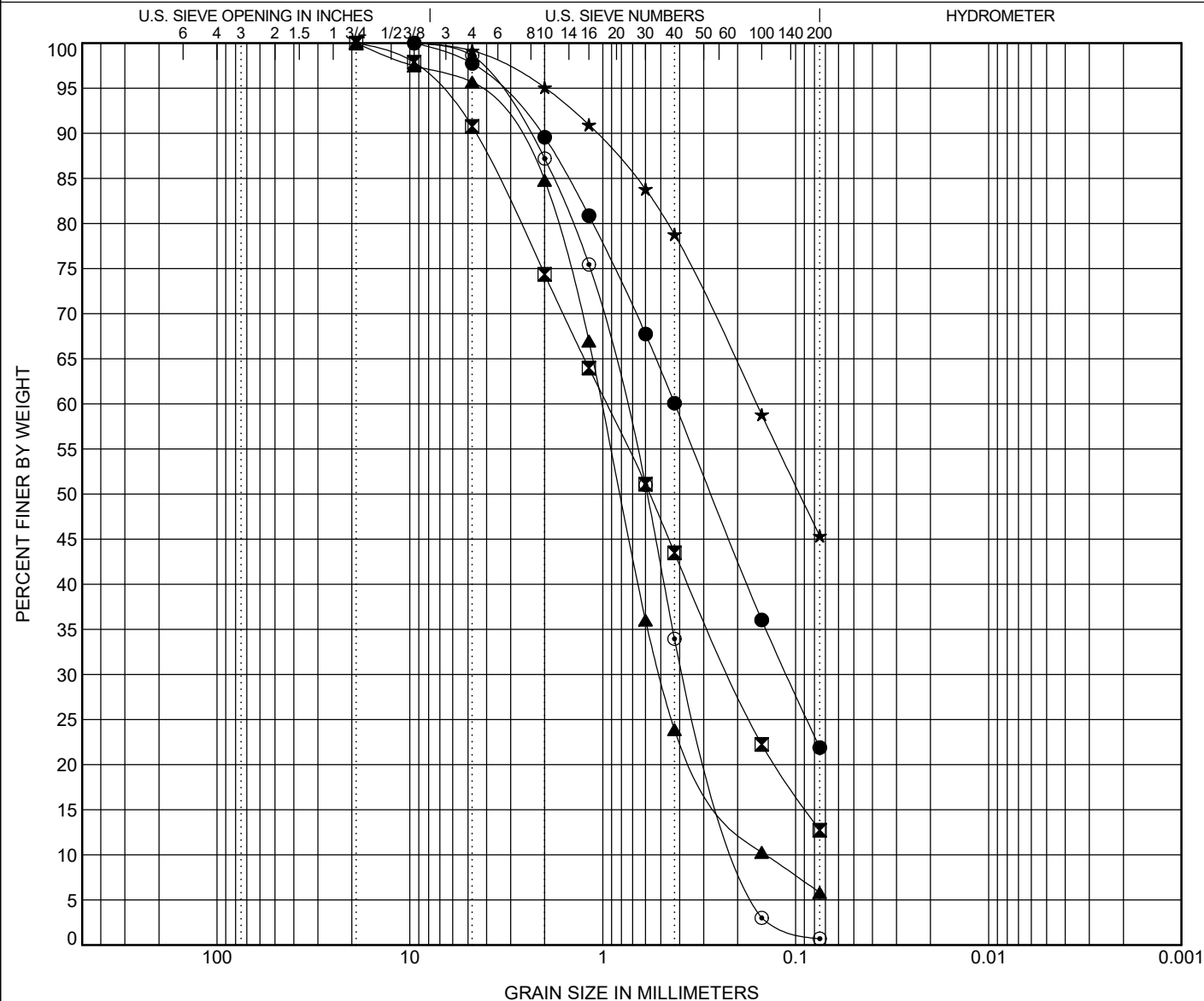
GRAIN SIZE DISTRIBUTION

CLIENT Terrasas and Tripp

PROJECT NAME Lompa Ranch Mini Storage Project

PROJECT NUMBER 16-302.1

PROJECT LOCATION Carson City Nevada



BOREHOLE	DEPTH	Classification					LL	PL	PI	Cc	Cu
●	TP-01	2.0	SILTY SAND (SM)								
☒	TP-01	3.0	SILTY SAND with Gravel (SM)								
▲	TP-02	9.0	POORLY GRADED SILTY SAND (SM-SP)							1.75	7.04
★	TP-03	1.0	CLAYEY SAND (SC)					35	21	14	
⊙	TP-03	3.0	POORLY GRADED SAND (SP)							0.95	4.05
BOREHOLE	DEPTH	D100	D60	D30	D10	%Gravel	%Sand	%Silt		%Clay	
●	TP-01	2.0	9.5	0.424	0.112	2.3	75.9	21.9			
☒	TP-01	3.0	19	0.958	0.219	9.2	78.0	12.7			
▲	TP-02	9.0	19	1.013	0.506	0.144	89.9	5.8			
★	TP-03	1.0	9.5	0.16		0.8	53.8	45.4			
⊙	TP-03	3.0	9.5	0.769	0.372	0.19	98.0	0.7			

GRAIN SIZE - GINT STD US LAB.GDT - 3/10/17 09:09 - N:\2 DAVE'S WORKING FILES\GEOTECH PROJECTS\16-302.1 LOMPA MINI-STORAGE TERRASS-TRIPP\LOMPA RANCH MINI STORAGE PROJECT.GPJ



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4010 Technology Way
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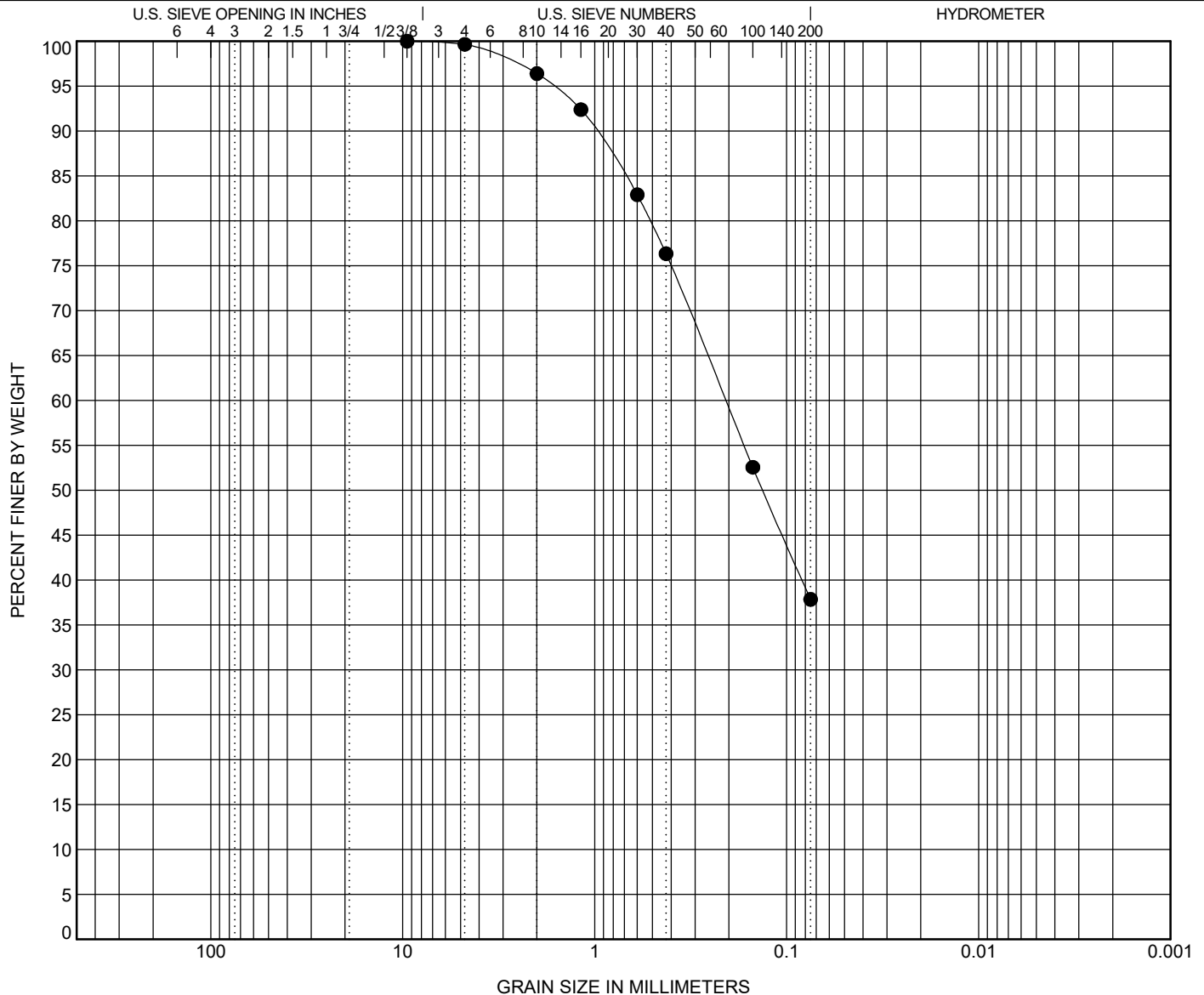
GRAIN SIZE DISTRIBUTION

CLIENT Terrasas and Tripp

PROJECT NAME Lompa Ranch Mini Storage Project

PROJECT NUMBER 16-302.1

PROJECT LOCATION Carson City Nevada



COBBLES	GRAVEL		SAND			SILT OR CLAY
	coarse	fine	coarse	medium	fine	

BOREHOLE	DEPTH	Classification					LL	PL	PI	Cc	Cu
● TP-05	2.0	CLAYEY SAND (SC)									
BOREHOLE	DEPTH	D100	D60	D30	D10	%Gravel	%Sand	%Silt		%Clay	
● TP-05	2.0	9.5	0.208			0.3	61.8	37.8			

GRAIN SIZE - GINT STD US LAB.GDT - 3/10/17 09:09 - N:\2 DAVE'S WORKING FILES\GEOTECH PROJECTS\16-302.1 LOMPA MINI-STORAGE TERRASS-TRIPP\LAMPA RANCH MINI STORAGE PROJECT.GPJ



CLIENT Terrasas and Tripp **PROJECT NAME** Lompa Ranch Mini Storage Project
PROJECT NUMBER 16-302.1 **PROJECT LOCATION** Carson City Nevada

83

SAMPLE EARTHWORK SPECIFICATIONS

General

The geotechnical report is available to bidders as a general guide to project and site conditions. The geotechnical report is not a part of the contract documents and is not a warranty as to site conditions. Bidders are encouraged to pothole the site as changes in site conditions between exploration points or due to weather or man-made influences can impact the site. Access to the site to perform independent evaluations should be provided upon reasonable request. Contract documents should address access and independent evaluations as to site conditions at the time of bidding.

The specifications for earthwork, flatwork and pavements should conform to the Standard Specifications for Public Works Construction (the “Orange Book”) except as modified herein or as specified in the contract documents (including drawings).

1.1 Soil Handling and Excavation Characteristics

- 1.1.1 In our opinion, grading and excavations **may/may not** be accomplished with light to moderate effort with conventional heavy-duty grading/excavation equipment. Excavations in native soils **are/are not** anticipated to generate significant quantities of oversized material (greater than six inches in dimension) that would require special handling or exporting from the site.
- 1.1.2 Excavated native soils generated from cut operations at the site **are/are not** anticipated to be suitable for re-use as engineered fill.
- 1.1.3 Imported structural fill material may be necessary. If needed, imported materials should meet the Standard Specifications for Public Works specifications (304.03). Structural fill is defined herein as all fill placed within two feet of foundations and all fill placed beneath pavement sections. Import structural fill material should be sampled and approved by us prior to its transportation to the site.
- 1.1.4 Temporary excavations, such as utility trench sidewalls excavated within undisturbed, unsaturated native soils or structural fill should remain near-vertical to depths of **XX** feet. Some minor sloughing should be expected within some of cleaner sand lenses or during periods of high precipitation. Native soils within **XX** feet of the existing surface should be considered **Type X** by OSHA Standards. It is the contractor’s responsibility to provide sufficient and safe excavation support per OSHA Standards as well as protecting nearby utilities, structures, and other improvements, which may be damaged by earth movements.

1.2 Grading – General, Site Preparation

- 1.2.1 A preconstruction conference should be held at the site prior to the beginning of grading operations with the owner, contractor, civil engineer and geotechnical engineer in attendance. Soil handling and grading requirements can be discussed at that time. Earthwork operations should be observed and compacted fill tested by our representative.
- 1.2.2 All references to relative compaction and optimum moisture content in this report are based on the newest version ASTM D1557 Test Procedure.
- 1.2.3 Site preparation should begin with the removal of vegetation and debris if any. The depth of removal should be such that material exposed in the cut areas or soils to be used as fill is relatively free of organic matter. This will likely result in removal depths ranging from approximately **XX** inches, depending on location. Material generated during stripping should be disposed of as directed by the Engineer.
- 1.2.4 During or immediately following wet weather, the near-surface soil may deflect or pump under heavy equipment loads. Yielding soil conditions can typically be stabilized using one of the methods listed below. However, soil conditions and mitigation methods should be reviewed and approved by us when encountered.
- **Option 1.** Deeply scarify (10 to 12 inches) allow to air dry to near optimum moisture content and re-compact.
 - **Option 2.** Remove unstable (wet) soils to a firm base and allow the wet subgrade soil to dry to near optimum moisture content and re-compact. Replace the removed soils with drier soil meeting the structural fill specifications.
 - Other stabilization alternatives may be appropriate depending on the situation. Consultation with us is crucial for expedient and appropriate mitigation.

1.3 Grading – Building Pad Preparation

- 1.3.1 For the purposes of this report, structural building pad areas should be considered areas extending a minimum of **XX** feet beyond the outside dimensions of the building or any foundation element.
- 1.3.2 Cut surfaces should be scarified at least **X** inches and compacted to **XX** % of maximum dry density.
- 1.3.3 Areas to receive fill should be prepared as described above for cut surfaces. Fill should be placed in **X** inch lifts, moisture conditioned to within **XX**% of optimum moisture content and compacted to a minimum of **XX**% of maximum dry density.

1.4 Grading – Underground Utilities

- 1.4.1 Underground utility trenches within structural areas (building pads, parking lots, and streets) should be backfilled with properly compacted material. The material excavated from the trenches

will/will not be adequate for on-site use as backfill provided it **does/does not** contain deleterious matter, vegetation or rock larger than six inches in maximum dimension. Trench backfill should be placed in loose lifts not exceeding eight inches. The lifts should be compacted to a minimum of 90% relative compaction at or near optimum moisture content.

- 1.4.2 Site soils **will/will not** be suitable for utility trench pipe bedding. Importation of bedding materials should be planned for accordingly.
- 1.4.3 Bedding and pipe zone backfill should extend from the bottom of the trench excavation to a minimum of 6 inches above the crown of the pipe. Pipe bedding material should consist of Class A Backfill material as defined by the Standard Specifications for Public Works (Orange Book). Bedding and pipe zone material should be compacted in 6 inch maximum lifts.

1.5 Grading – Pavement and Flatwork Areas

- 1.5.1 In pavement and flatwork subgrade areas the subgrade (whether fill or undisturbed native soil) should be scarified to a depth of 8 to 10 inches and moisture conditioned at or near optimum moisture content. The upper 6 inches of pavement subgrade soils should be compacted to a minimum of 90% relative compaction at or near optimum moisture content. If the compacted surface becomes dry and loose after it has been tested for compaction, it should be moisture conditioned and lightly compacted to a firm surface prior to the placement of additional fill or aggregate base.
- 1.5.2 The subgrade soils for pavements should be finished to a compacted smooth unyielding surface. We recommend proof-rolling the subgrade with a loaded water truck (or similar equipment) to verify the stability of the subgrade prior to placing aggregate base.
- 1.5.3 Aggregate base used to support pedestrian and vehicular pavements should be compacted to a minimum of 95% relative compaction.

1.6 Slabs-on-Grade

- 1.6.1 Earthwork below conventional concrete slab-on-grade floors should be prepared as recommended for cuts or fills in building pad areas. A vapor barrier should be provided consisting of a minimum of 10 mil polyethylene material or other approved material. The vapor barrier should be lapped and sealed in accordance with the manufacturer's recommendations. At a minimum, joint laps should be at least six inches in width. In any case, care should be taken to avoid any disturbance or rupture to the vapor barrier measures throughout the construction process.
- 1.6.2 Slabs should be underlain by a minimum of **X** inches of compacted (95% minimum relative density) aggregate base. Slab thickness and reinforcement should be determined by the structural engineer based on the anticipated loading.

- 1.6.3 If a significant amount of time has passed since building pad grading and the soil surface of the building pad has become dry, then it should be re-moistened prior to placing the moisture retarding system. The building pad should be moistened by soaking or sprinkling such that the upper 12 inches of soil is near optimum moisture, as determined by our representative at least 48 hours before concrete placement.
- 1.6.4 Some floor coverings, such as tile or linoleum, are sensitive to moisture that can be transmitted from and through the slab. Slab floors should be moist cured for a minimum of 7 days prior to placing any floor coverings. Floor coverings should be installed in accordance with the manufacturer's recommendations including any moisture transmissivity testing requirements.
- 1.6.5 Crack control spacing should be determined by the project structural engineer based on slab thickness and intended usage.
- 1.6.6 All exterior concrete should be air entrained with from 4.5% to 7.0% air content. The water cement ratio for all exterior concrete should be 0.45 or less. The use of mid-range plasticizer is recommended to facilitate the finishing process while maintaining the desired water cement ratio.
- 1.6.7 Exterior concrete should be placed and finished in accordance with American Concrete Institute (ACI) recommendations for concrete placed in areas subject to freeze-thaw environments.

Recommendations presented herein are intended to reduce the potential for cracking of slabs as a result of differential movement. However, even with the incorporation of the recommendations presented herein, slabs-on-grade will still exhibit some cracking. The occurrence of concrete shrinkage cracks is independent of the soil supporting characteristics. Their occurrence may be reduced and/or controlled by limiting the slump of concrete, the use of crack control joints and proper concrete placing and curing. Adherence to ACI and Portland Concrete Association (PCA) recommendations including those for low humidity and wind, if applicable, should be incorporated into project construction practices.

1.7 Flexible Pavements

- 1.7.1 Earthwork below conventional flexible pavements should be prepared as recommended for cuts or fills in building pad areas. Aggregate base should consist of Type 2 Class B and be compacted to 95% of maximum dry density.
- 1.7.2 Asphalt concrete should conform Section 320.02 of the Standard Specifications for Public Works Construction ("Orange Book"). We recommend that the asphalt consist of polymerized oil type PG64-28NV as it substantially reduces maintenance and extends pavement life in our experience.
- 1.7.3 The asphalt concrete should consist of Type II or Type III of Section 200.02.02 of the Standard Specifications for Public Works Construction ("Orange Book").

1.8 Site Drainage

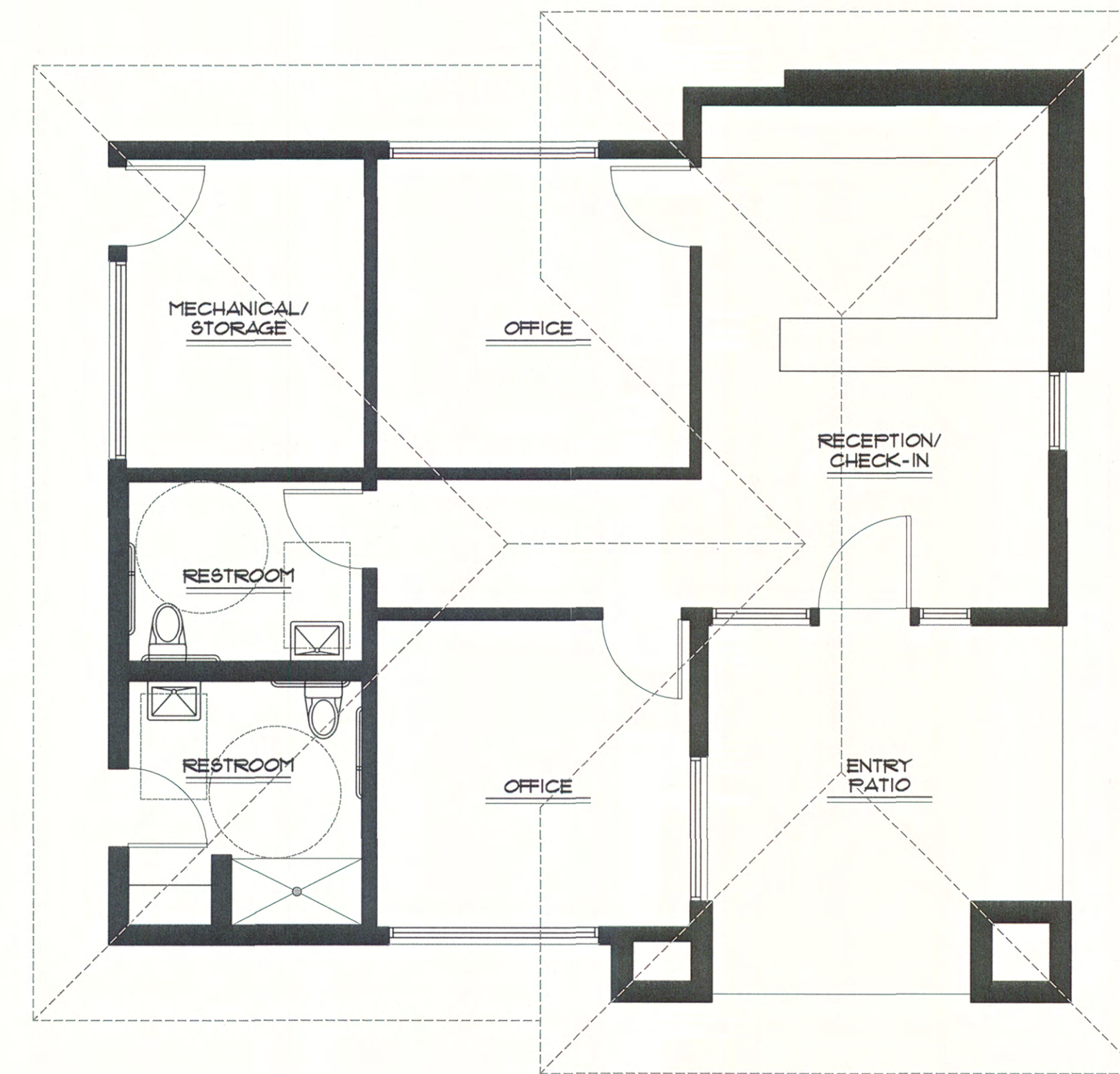
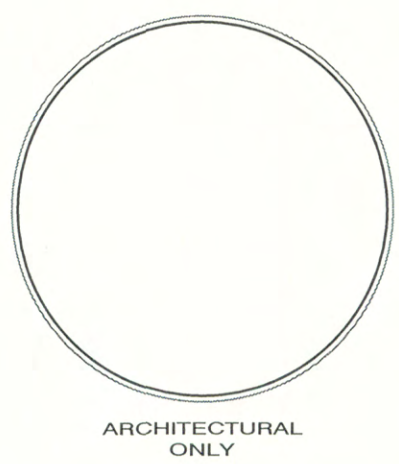
- 1.8.1 Adequate drainage is crucial to reduce the potential for differential soil movement, erosion and subsurface seepage. Under no circumstances should water be allowed to pond adjacent to footings. The site should be graded and maintained such that surface drainage is directed away from structures and the top of slopes into swales or other controlled drainage devices. The percent fall of slopes around structures should be as per the most current version of the IBC as adopted by the local governing agency.
- 1.8.2 Roof and pavement drainage should be directed into conduits to carry runoff away from the structures. Landscape irrigation should be kept at least three feet away from all foundations. We recommended that drip irrigation be installed within six feet of foundations wherever feasible.

Full Size Drawings

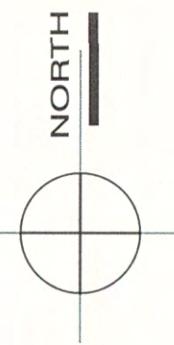
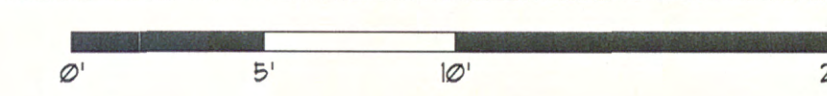
REVISIONS	DATE



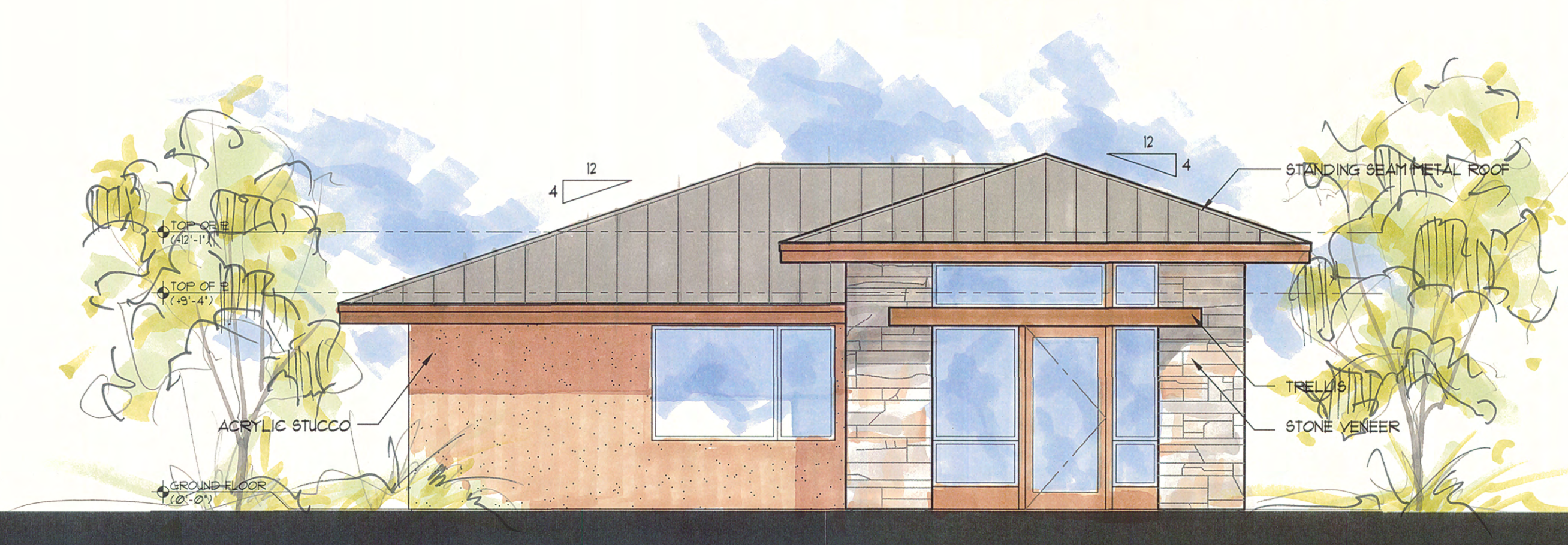
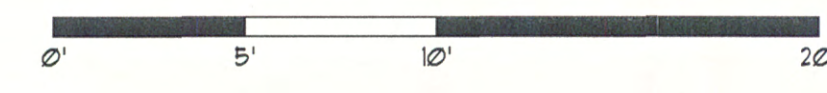
ERIC JOHNSON
 ARCHITECT
 PO BOX 54282
 RENO NEVADA 89523
 775-750-1133
 ECJARCH@CHARTER.NET



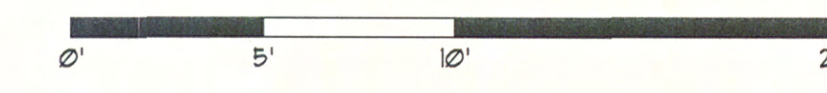
1 OFFICE FLOOR PLAN



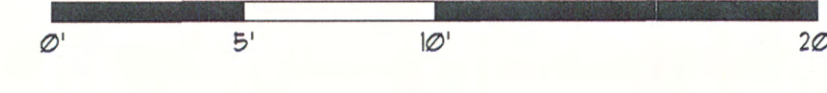
3 WEST ELEVATION



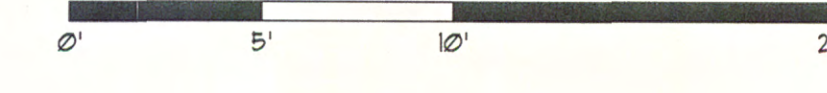
2 SOUTH ELEVATION



5 EAST ELEVATION



4 NORTH ELEVATION



FLOOR PLAN & EXTERIOR BUILDING ELEVATIONS - SUP SUBMITTAL
 NEW RV & BOAT STORAGE FACILITY FOR:
SIERRA TAHOE RV & BOAT STORAGE
 BUTTI WAY | CARSON CITY | NEVADA | APN 010-041-76

DRAWN BY

CHECKED

DATE

12/25/13

SCALE

AS SHOWN

JOB NUMBER

STRVB

SHEET NUMBER

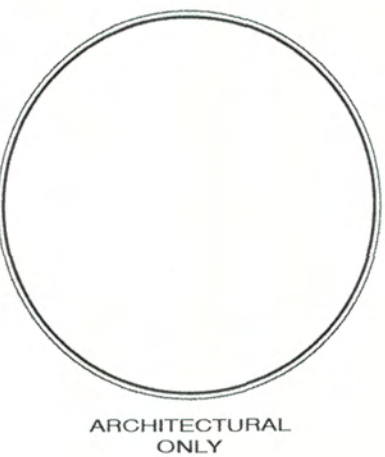
A4.1

REVISIONS	DATE

ECD
ARCHITECT

ERIC JOHNSON
ARCHITECT

PO BOX 34282
RENO NEVADA 89523
775-750-1133
ECJARCH@CHARTER.NET



MIN-STORAGE TYPICAL EXTERIOR BUILDING ELEVATIONS - REVISED SUP SUBMITTAL

NEW RV BOAT & MINI STORAGE FACILITY FOR:

SIERRA TAHOE RV BOAT & STORAGE

BUTTI WAY | CARSON CITY | NEVADA | APN 010-041-76

DRAWN BY

CHECKED

DATE
191003

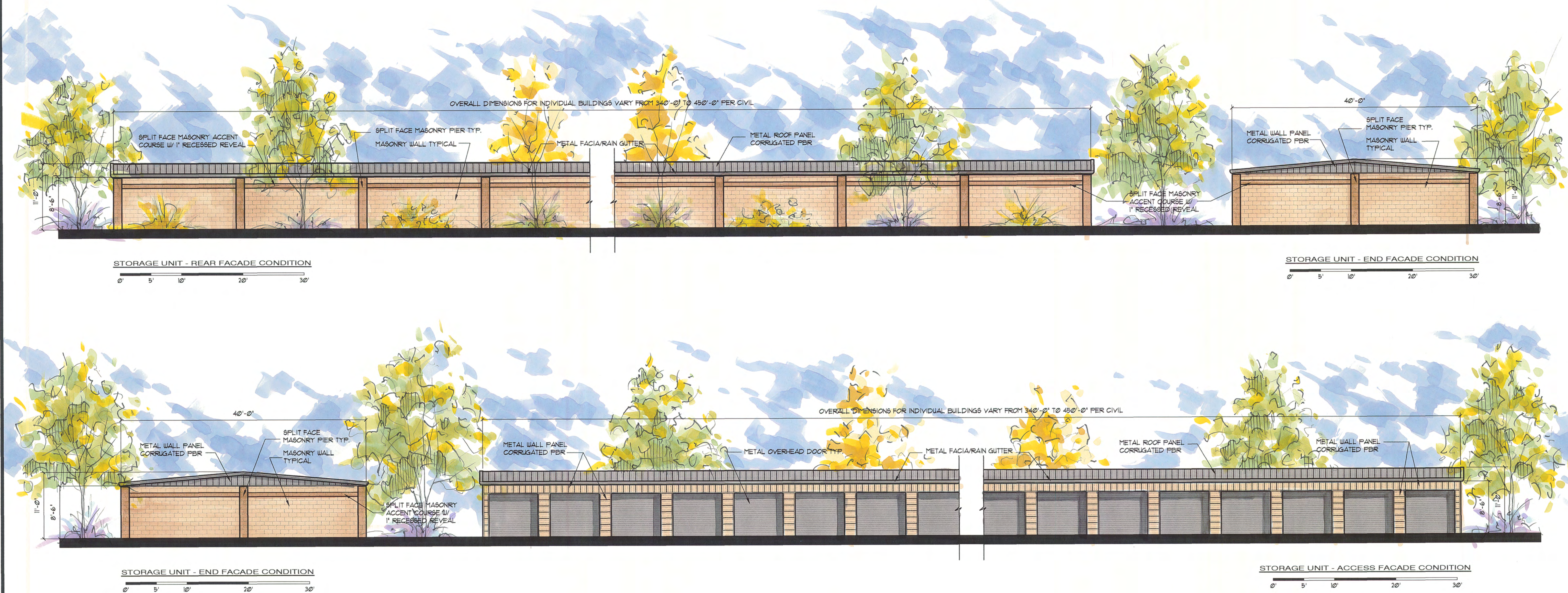
SCALE
AS SHOWN

JOB NUMBER
STRVB

SHEET NUMBER
A4.2

NEW RV BOAT & MINI STORAGE FACILITY FOR: SIERRA TAHOE RV BOAT & STORAGE

BUTTI WAY | CARSON CITY | NEVADA | APN 010-041-76



PLANT LIST:

LARGE/STREET TREES: (2" Caliper)
IRRIGATE ALL PLANTINGS W/RESOURCE EFFICIENT DRIP IRRIGATION SYSTEM.

BOTANICAL NAME	COMMON NAME
RED MAPLE	ACER RUBRUM
BLACK GUM	NYSSA SYLVATICA
GLEDITSIA TRIACANTHOS	SHADEMASTER HONELOCUST
QUERCUS PALUSTRIS	PIN OAK
QUERCUS RUBRA	RED OAK

SMALL/ACCENT TREES:

BOTANICAL NAME	COMMON NAME
ACER GINNALA	AMUR MAPLE
CERCIS CANADENSIS	EASTERN REDBUD
LIQUIDAMBAR STYACIFLUA	SWEETGUM
MALLUS PRARIEFIRE	PRARIEFIRE CRABAPPLE
PYRUS CALLERYANA 'REDSPIRE'	REDSPIRE PLUM

EVERGREEN TREES: (6' Min. Height)

BOTANICAL NAME	COMMON NAME
JUNIPERUS CHINENSIS 'TORULOSA'	HOLLYWOOD JUNIPER
CALOCEDRUS DECURRENS	INCENSE CEDAR
PICEA PUNGENS	COLORADO SPRUCE
PINUS FLEXILIS 'VANDERWOLF'	VANDERWOLF'S PINE
PINUS NIGRA	AUSTRIAN PINE

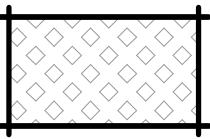
SHRUBS/GROUNDCOVERS/VINES: (5 Gal. Min.)

BOTANICAL NAME	COMMON NAME
BUDDLEIA DAVIDII	BUTTERFLY BUSH
CAMPISIA RADICANS	RED TRUMPET VINE
GENISTA LYDIA	DWARF BROOM
PHOTINIA FRASERI	REDTIG PHOTINIA
SYRINGA VULGARIS	COMMON LILAC

PERENNIALS: (1 Gal.)

BOTANICAL NAME	COMMON NAME
ECHINACEA PURPUREA	PURPLE CONEFLOWER
GAILLARDIA ARISTATA	INDIAN BLANKET FLOWER
PENSTEMON STRICTUS	ROCKY MOUNTAIN PENSTEMON
CERASTEUM TOMENTOSUM	SNOW-IN-SUMMER
PEROVSKIA ATRIPLICIFOLIA	RUSSIAN SAGE

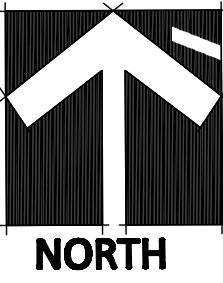
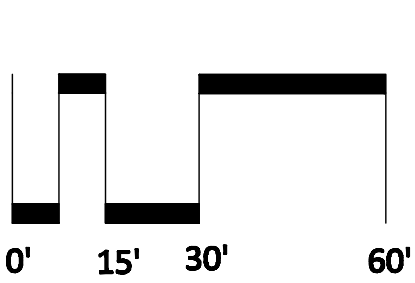
LANDSCAPE LEGEND:



SHRUB PLANTING AREA w/ROCK MULCH COVER - (56,340 SF)
Shrub sizes, varieties and quantities to meet code requirements.
Place mulch @ 4" min. depth, over weed barrier fabric

LANDSCAPE CALCULATIONS:

ZONING:	GENERAL COMMERCIAL
SITE AREA=	7.28 AC.
IMPERVIOUS AREA =	231,341 S.F.
LANDSCAPE SUMMARY:	
LANDSCAPE AREA REQUIRED =	46,268 S.F. (20% of Impervious Area)
LANDSCAPE AREA PROVIDED =	56,925 S.F. (24.6 %)
TREE/SHRUB REQUIREMENTS:	
1 TREE/400 S.F. (= 141 TREES)	
Includes: 1 TREE/10 PARKING SPACES (7 SPACES = 1 TREE)	
and 1 TREE/30 LF STREET FRONTAGE (497 L.F. = 17 TREES)	
TOTAL TREES REQUIRED =	141 TREES
TREES PROVIDED =	141 TREES
6 SHRUBS per TREE REQUIRED =	846 SHRUBS



REVISIONS	BY

TreeHugger Studio, LLC
landscape architecture
and site planning services
140 West Guffey Drive
Washoe Valley, NV 89704
(775) 530-0665



preliminary landscape plan for:
SIERRA TAHOE RV, BOAT and SELF STORAGE
Airport Road, Carson City, NV APN: 010-041-76

PRELIMINARY
LANDSCAPE PLAN

ISSUE DATE:
May 2019, REV Sept 2019
SCALE:
1" = 30'-0"
DRAWN BY:
R. SHOCK
CHECKED BY:

PROJECT NUMBER:
228-19-006

L.1

SITE & GRADING LEGEND

A.C. PAVEMENT AREA

CONCRETE AREA

ACCESSIBLE PARKING SPACE W. SIGN & PAVEMENT MARKINGS

PEDESTRIAN ACCESS RAMP

ACCESSIBLE ROUTE

PARKING SPACE COUNT

KEYNOTE
(REF. CORRESPONDING LEGEND)

UTILITY
PROPOSED UTILITY LINE W. DESCRIPTION
EXISTING UTILITY LINE W. DESCRIPTION

MAHOLE W. DESCRIPTION
(EXISTING/PROPOSED)

CLEANOUT
(EXISTING/PROPOSED)

CATCH BASIN/DROP INLET

YARD DRAIN

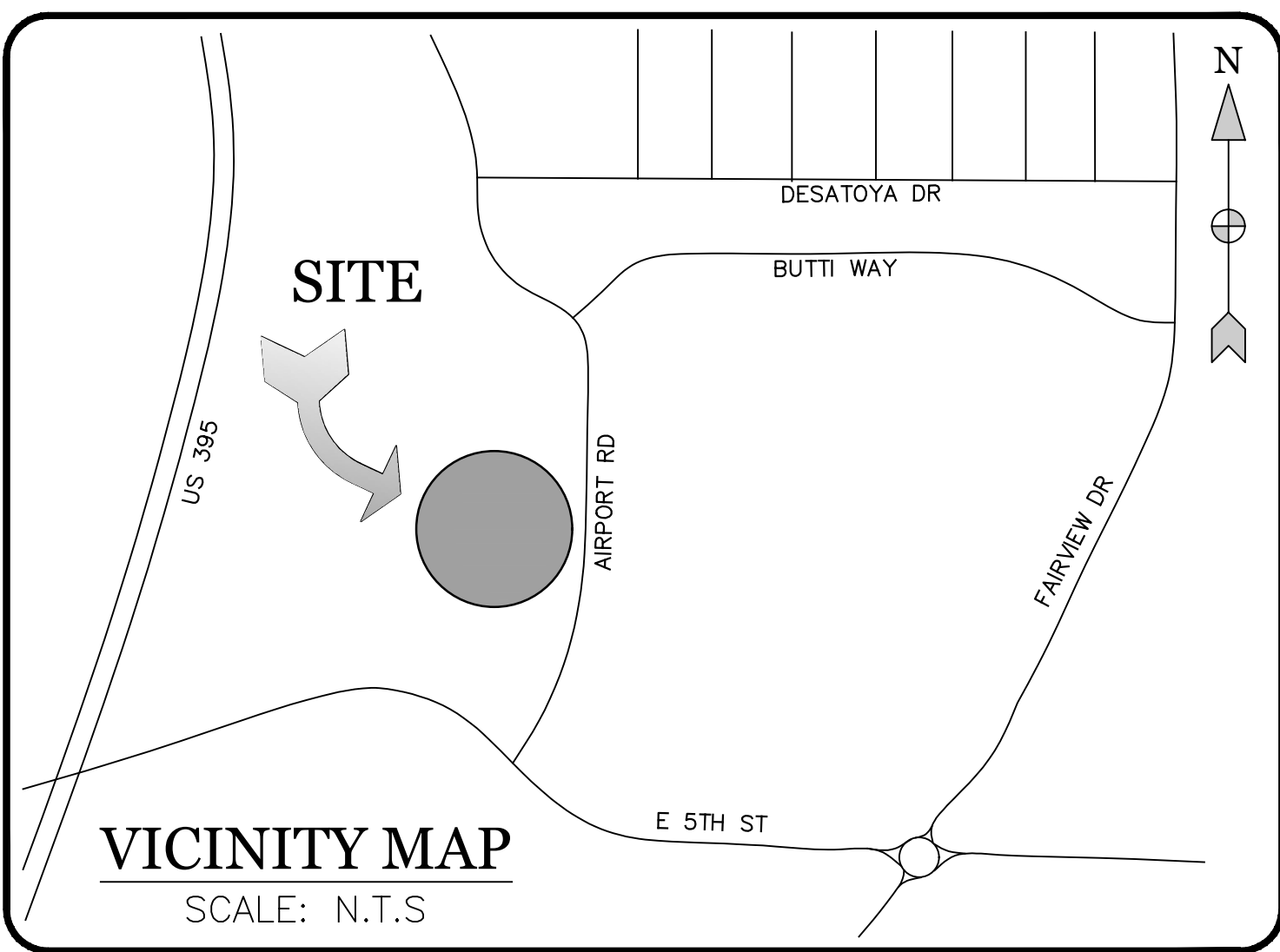
DIRECTIONAL FLOW LINE

GRADE BREAK

PROPOSED CONTOUR LINE

EXISTING CONTOUR LINE

SPOT ELEVATION
(EXISTING) ~ PROPOSED



SITE ANALYSIS		
AREA	7.28 AC	
ZONING	GENERAL COMMERCIAL(GC)	
SETBACKS (F/R/S)	0/0/0(FT)*	
BUILDING FOOTPRINT	±55,255 S.F.	17%
PAVED/IMPERVIOUS	±182,470 S.F.	58%
LANDSCAPED/UNDEVELOPED	±63,423 S.F. MIN.	20%
UNDEVELOPED	±15,986 S.F.	5%

30' MIN. SETBACK FROM RESIDENTIAL & 10' MIN. LANDSCAPE SETBACK ALONG AIRPORT ROAD

EARTHWORK ANALYSIS	
SITE AREA	7.28 AC
SITE DISTURBANCE	±7.0 AC
PROPOSED CUT	±12,600 YD³
PROPOSED FILL	±7,300 YD³
NET EARTHWORK	±5,300 YD³ CUT

THESE QUANTITIES ARE PRELIMINARY AND ARE BASED ON THE INTENT OF THE DEVELOPER TO WORK WITH THE PROPERTY TO THE NORTH WHICH REQUIRES FILL MATERIAL. A MORE BALANCED SITE MAY ULTIMATELY BE PROPOSED.

PARKING ANALYSIS			
USE	AREA (FT²)	CRITERIA	REQUIRED SPACES
OFFICE	855	1/325	2.6
TOTAL REQUIREMENT	SPACES PROVIDED	ADA SPACES	VAN ACCESSIBLE
3 SPACES	7	1	1

NOTES:
1. ALL BUILDING INFORMATION PROVIDED BY ORIGIN 8 ARCHITECTURE.
2. REQUIREMENTS DETERMINED USING CMC 18A2.a2.

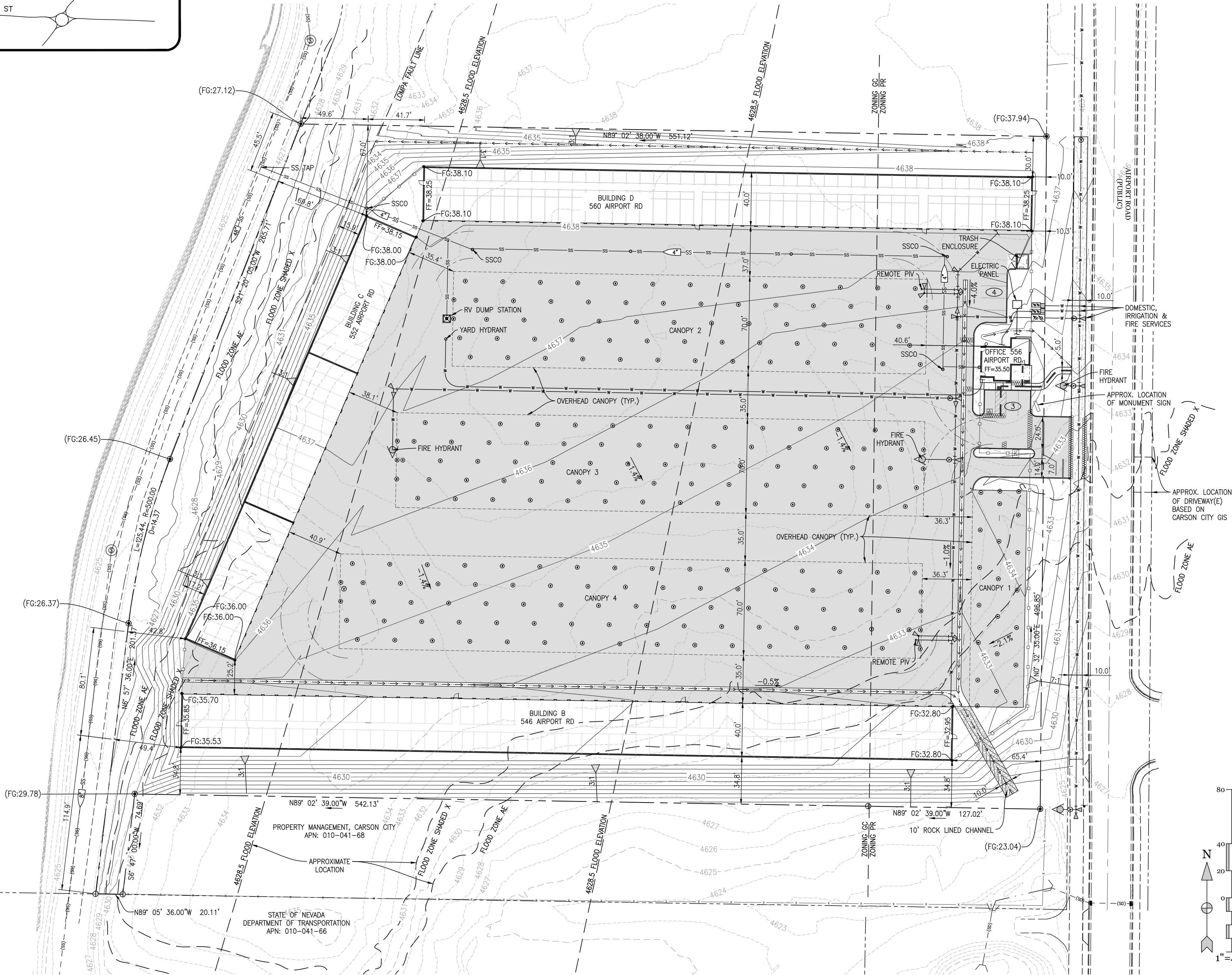
NOTES

- THE FIELD SURVEY PREPARED BY WOOD RODGERS IS THE BASIS OF THIS DESIGN. MVC TAKES NO RESPONSIBILITY FOR THE ACCURACY OF THE SURVEY.
- BASIS OF BEARINGS WAS DERIVED FROM NATIONAL GEODETIC SURVEY MONUMENT "V 357" DESCRIBED AS A DISC SET IN CONCRETE STAMPED "V 357 1953. THE MONUMENT IS LOCATED ON THE SOUTH SIDE OF U. S. HIGHWAY 50, SOUTHEAST OF THE INTERSECTION OF THE HIGHWAY AND AKRON WAY. 101 FEET SOUTHEAST OF THE CENTER LINE OF THE HIGHWAY, 54 FEET NORTHEAST OF THE CENTER OF A WOODEN CATTLE GUARD, PUBLISHED ELEVATION = 4633.40.
- BASIS OF ELEVATION IS THE NEVADA STATE PLANE COORDINATE SYSTEM, WEST ZONE, NORTH AMERICAN DATUM OF 1983/1994. HIGH ACCURACY REFERENCE NETWORK (NAD 83/94-HARN), AS DETERMINED USING REAL TIME KINEMATIC (RTK) GPS OBSERVATIONS OF CARSON CITY CONTROL MONUMENTS "CC038" AND "V357". THE BEARING BETWEEN CARSON CITY CONTROL MONUMENT "CC038" AND "V357" IS TAKEN AS SOUTH 31°48'44" WEST. ALL DIMENSIONS SHOWN ARE GROUND DISTANCES. COMBINED GRID-TO-GROUND FACTOR = 1.00020000.
- ALL WORK WITHIN THE CARSON CITY RIGHT-OF-WAY SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE LATEST CODES, STANDARD SPECIFICATIONS & DETAILS.
- REFERENCE ARCHITECTURAL PLANS FOR ALL BUILDING DIMENSIONS.
- ALL DIMENSIONS ARE TO FRONT FACE OF CURB, FACE OF BUILDING, FACE OF WALL, CENTER OF PIPE, CENTER OF MANHOLE OR PROPERTY LINE UNLESS OTHERWISE NOTED.
- ALL PERMANENT STRIPING, SIGNAGE & TRAFFIC CONTROL IMPROVEMENTS SHALL BE INSTALLED IN ACCORDANCE WITH CURRENT "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) REQUIREMENTS.
- THE ACCESSIBILITY ROUTE SHALL NOT HAVE A RUNNING SLOPE EXCEEDING 5% OR A CROSS SLOPE EXCEEDING 2%. REFERENCE SITE PLAN FOR LOCATION.
- THIS SITE LIES IN FEMA FLOOD ZONE X AND ZONE AE (3200010111G). ZONE AE IS DEFINED AS AREAS WITHIN THE 1-PERCENT ANNUAL CHANCE FLOOD IS ALSO REFERRED TO AS THE BASE FLOOD OR 100-YEAR FLOOD. MODERATE FLOOD HAZARD AREAS, ZONE X (SHADED) IS DEFINED AS A MODERATE FLOOD HAZARD AREA AND IS BETWEEN THE LIMITS OF THE BASE FLOOD AND THE 0.2-PERCENT-ANNUAL-CHANCE (OR 500-YEAR) FLOOD. ZONE X (UNSHADED) IS DEFINED AS AN AREA OF MINIMAL FLOOD HAZARD, WHICH ARE THE AREAS OUTSIDE THE 0.2-PERCENT-ANNUAL-CHANCE FLOODPLAIN.

UTILITY PROVIDERS

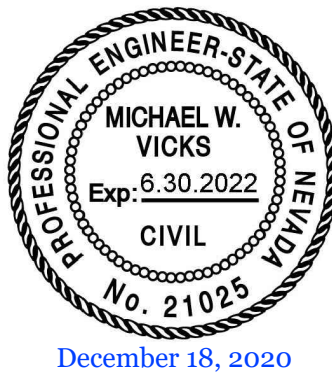
DOMESTIC WATER	CARSON CITY UTILITIES DEPARTMENT
IRRIGATION WATER	CARSON CITY UTILITIES DEPARTMENT
SANITARY SEWER	CARSON CITY UTILITIES DEPARTMENT
STORM DRAIN	CARSON CITY UTILITIES DEPARTMENT
NATURAL GAS	SOUTHWEST GAS CORPORATION
ELECTRICITY	NV ENERGY
TELECOMMUNICATIONS	AT&T / CHARTER COMMUNICATIONS
FIRE	CARSON CITY FIRE DEPARTMENT
POLICE	CARSON CITY SHERIFFS OFFICE

THIS PLAN IS INTENDED FOR SPECIAL USE PERMIT REVIEW BY CARSON CITY ONLY - NOT FOR CONSTRUCTION



Sierra Tahoe RV, Boat & Self Storage

556 Airport Road | APN: 010-041-76 | Carson City | Nevada



Revisions	
Date	12.18.2020
Drawn	HBA
Checked	MWV
Project No.	18.041

Site Plan

C1.0
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